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VICTORIAN RAILWAYS

VICTORIAN RAILWAYS COMMISSIONERS

BY-LAW No. 375

GOODS RATES BOOK

Volume I

Rates and Conditions for the Carriage of GOODS, WOOL AND LIVE STOCK ETC. AND CHARGES FOR OTHER SERVICES.

No. 926.—3266/54.—Price 8/6.

VICTORIAN RAILWAYS

GOODS RATES BOOK

VOLUME 1

Rates and Conditions for the Carriage of

GOODS, WOOL AND LIVE STOCK ETC. AND CHARGES FOR OTHER SERVICES

To Take Effect On And From 1st December, 1954

The Commissioners hereby notify that the Rates and Conditions, Classifications, and Charges shown herein are subject to alteration at any time.

To be read and construed with Goods Rates Book—Volumes II. and III.

All Rates, Conditions, Classifications and Charges for the Carriage of Goods, Wool and Live Stock, etc., issued prior to 1st December, 1954, are cancelled.

Published by the authority of the Government of the State of Victoria :
M. L. G. McKenzie
Manager,
Victorian Railways Printing Works North Melbourne

1954

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VICTORIAN RAILWAYS

VICTORIAN RAILWAYS COMMISSIONERS

BY-LAW No. 375

The Victorian Railways Commissioners, in pursuance of the powers conferred in that behalf by the Railways Acts, do hereby make the following By-Law, and do hereby repeal all previous By-Laws, Conditions, Practices, Instructions, Classifications, Rates and Charges whatsoever as relate thereto or conflict therewith.

The several Rates and Charges which shall be imposed in respect of the carriage of Goods, Wool and Live Stock over the Victorian Railways, and the Classification and Conditions under which such Goods, Wool and Live Stock will be carried and the charges for other services shall be as set forth in Volumes I., II. and III. of this By-Law.

The provisions of this By-Law shall become effective as from 1st December, 1954.

In witness whereof the Common Seal of the Victorian Railways Commissioners was affixed hereto this 29th day of September in the year of our Lord One thousand nine hundred and fifty-four in the presence of—

(SEAL) R. G. Wishart, O. G. Meyer, E. H. Brownbill } Victorian Railways Commissioners

Confirmed by the Governor-in-Council

the fifth day of October, 1954.

A. Mahlstedt
Clerk of the Executive Council.

ALPHABETICAL INDEX

INDEX—continued.

Definitions and Interpretations

Wherever in the Goods Rates Book the following terms are used, such terms shall have the meaning prescribed in the definition or interpretation shown hereunder in respect of each such term :—

Term	Definition or Interpretation
COMMISSIONERS	The Victorian Railways Commissioners.
RAILWAYS	The lines of railway worked by the Victorian Railways Commissioners.
PIER	Any Railway Pier, Wharf, or Jetty, or any Railway Breakwater, if used as a Pier, vested in or under the jurisdiction of the Victorian Railways Commissioners.
PIERMASTER	Any Railway Piermaster, Acting or Assistant Piermaster, Stationmaster, Assistant Stationmaster, or other duly accredited employee in charge of a Pier vested in or under the jurisdiction of The Victorian Railways Commissioners.
EMPLOYEE	Any officer, employee, or other person in the service of the Victorian Railways Commissioners.
CONSIGNOR	The company, firm, person, or persons consigning the goods or live stock for carriage.
CONSIGNEE	The company, firm, person, or persons to whom the goods or live stock are consigned.
CLASSIFICATION	The schedule in the Goods Rates Book specifying the various kinds and descriptions of goods and the classes in which they are placed.
CONSIGNMENT	Any parcels, goods, or live stock consigned by one *bona fide* consignor at the forwarding station to one *bona fide* consignee at the station of destination delivered on one and the same day to the Commissioners for carriage, and entered on one consignment note.
CONSIGNMENT NOTE ...	A form approved by the Commissioners to be used for consigning goods or live stock, and signed by the consignor or by some person authorised by him for the purpose.
LOT	Any quantity of goods of one or more classes consigned by one or more *bona fide* consignors at the same forwarding station to one *bona fide* consignee at the station of destination, and deliverd to the Commissioners on one and the same day. A "Lot" may consist of one or more consignments or parts thereof.
GROUP	Any combination of two or more of the classes of goods which constitute a "Lot" or a consignment.
RATE	The charge fixed by the Commissioners for the carriage of a specified weight, number, quantity, or measurement of a specified kind or class of parcel, goods, or live stock for a specified distance, or between specified places, subject in every case to the "Conditions of Carriage" and the Classification.
TARIFF RATES	The whole of the rates prescribed in the Goods Rates Book or in any authorised amendment thereof or addition thereto.
MILEAGE RATES	Rates based on distance, irrespective of locality.

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Term	Definition or Interpretation
DISTRICT RATES	Rates, other than "mileage rates," applicable only between specified places.
COMMODITY RATES ...	Rates, other than "mileage," "District," or "package" rates, applicable only in respect of specified articles.
PACKAGE RATES	Rates applicable to single packages of specified descriptions.
THROUGH RATES	Rates covering carriage on the Victorian Railways, together with carriage on other railways, or by water or other means of conveyance.
SPECIAL RATES	Non-Tariff Rates, viz., rates other than those prescribed in the Goods Rates Book or any authorised amendment thereof or addition thereto.
FREIGHT CHARGE ...	The amount due to the Victorian Railways Commissioners for the carriage of parcels, goods, or live stock.
RE-CONSIGNMENT ...	A change in the original destination station of truck loads of goods made under the prescribed conditions.

General Conditions for the Carriage of Goods and Live Stock.

Supplementary conditions are also provided for the carriage of exhibits for Agricultural Shows, and Live Stock.

1. CONDITIONS TO BE ADHERED TO.

No employee of the Commissioners has any authority to dispense with or vary any of the provisions of the By-laws of the Commissioners.

All traffic is carried subject to the Railways Acts and to the By-laws of the Commissioners.

2. NON-LIABILITY OF THE COMMISSIONERS.

(*a*) The Commissioners will not be liable for the loss of, or injury to, any article or articles, or property of the descriptions following, that is to say :—Gold or silver coin of the realm or of any foreign State, or any gold or silver in a manufactured or unmanufactured state ; or any precious stones, jewellery, watches, clocks, or timepieces of any description ; trinkets, gold or silver ores, bills, notes of any Bank ; orders, notes, or securities for the payment of money ; English, Colonial, or Foreign stamps ; maps, writings, title-deeds, paintings, engravings, pictures, gold or silver plate or plated articles, glass, china, silks in a manufactured or unmanufactured state, and whether wrought up or not wrought up with other materials ; furs, lace, or opium, or any of them, contained in any parcel or package which shall have been delivered for carriage by or to accompany the person of any passenger on the Victorian Railways, when the value of such article or articles or property aforesaid shall exceed the sum of Ten pounds, unless at the time of delivery thereof at the station or warehouse for the purpose of being carried or of accompanying the person of any passenger as aforesaid, the value and nature of such article or articles, or property, shall have been declared by the person sending or delivering the same, and the increased charges hereunder set forth (which are in addition to the freight charges) shall have been paid to and accepted by the employee duly authorized to receive the same. (See General Condition No. 20 for list of articles which are carried only by passenger train.)

(*b*) The following are the increased charges to be paid upon the said articles (which must be properly protected), the amount to be prepaid upon consignment of the goods.

Distance, in miles.	Percentage on declared value.		Minimum Additional Charge.	
	s.	d.	s.	d.
1 to 101 miles	10	0	2	0
102 to 251 "	20	0	4	0
252 to 500 "	30	0	6	0
501 to 1,000 "	50	0	10	0
1,001 to 1,500 "	60	0	12	0
1,501 to 2,000 "	70	0	14	0
Over 2,000 "	80	0	16	0

The above rates are applicable to Intersystem consignments as well as to local traffic.

3. THE COMMISSIONERS WILL NOT BE RESPONSIBLE FOR.

(*a*) Loss or misdelivery of any goods improperly or insufficiently addressed, marked, directed, or described, nor for loss of, or damage to, any goods which are insufficiently or improperly packed or secured ; nor will they be responsible for any loss or damage occurring to goods consisting of a variety of articles in the same package liable by breakage to damage each other or other articles, or arising from leakage due to bad vessels or bad cooperage, or to fermentation.

(*b*) Nor for damage to fruit, fish, meat, poultry, game, and other perishable commodities arising from the perishable nature of such commodities, or from the same not being taken away forthwith on arrival at the destination station.

(*c*) Nor for any loss arising from delay in the delivery of goods which may have been occasioned by flood, storm, tempest, or other unforseen cause.

(*d*) Nor for any loss of or injury to any articles, goods, or things put into wrappers, boxes, packages, cases, or baskets, marked, described, returned, delivered, or represented as "Empties."

4. ALTERNATIVE RATES FOR CARRIAGE AT COMMISSIONERS' RISK OR OWNER'S RISK.

The Commissioners have two rates for the carriage of the classes and descriptions of goods hereinafter specified at either of which rates the goods may be consigned at the sender's option ; one the ordinary rate when the goods are carried at Commissioners' Risk (hereinafter so-called) whereby the Commissioners take the ordinary liability of a common carrier provided that the goods are properly packed; the other a reduced rate when the goods are carried at Owner's Risk (hereinafter so-called) whereby the sender undertakes or agrees to relieve the Commissioners from all liability for loss injury damage deviation misdelivery delay or detention except upon proof that the same arose from the wilful misconduct of the Commissioners' servants.

The goods to which the said two rates apply are as follows :—Such goods as are classified in the Goods Rates Book under Classes "Firewood," "Firewood plus 20%," "Coal," "Manure," "Special M," "M," "Grain," "AP," "S.A.P.," "Unseasoned Hardwood Timber," "Sand," "Wheat," "Flour, Bran and Pollard," "A," "AA," "B," "BB," and "C," and at all rates less than Class "1," and at the rates for Wool Vehicles and Boats.

The rates for the carriage of goods charged at all rates less than Class "1," and at the rates for Wool Vehicles and Boats specified in this By-law are the reduced rates for carriage at Owner's Risk and such rates will be charged unless it is declared on the Consignment Note that the goods are required to be carried at the Commissioners' Risk. The ordinary rates for the carriage of such goods at Commissioners' Risk shall be the reduced rates for carriage at Owner's Risk published herein with the following additions :—

(i) Goods charged at any rates less than Class "1" ("Grain," "Wheat" or "Flour, Bran and Pollard" rates excepted) and at the rates for Wool.—Add 5 per cent. to the rate per ton (minimum additional charge 1s. 1d. per ton, and minimum additional charge per consignment 3s. 4d.).

(ii) Goods charged at the "Grain," "Wheat" or "Flour, Bran and Pollard" rates—

For distances not exceeding 200 miles—Add 1s. 3d. per ton.
For distances exceeding 200 miles—Add 1s. 5d. per ton.
(Minimum additional charge per consignment 3/4.)

(iii) Vehicles and Boats—Add 10% of the freight charge.

Except as otherwise provided, goods in Classes "1" and "2" shall, if not properly protected by packing, be carried only at Owner's Risk. When so protected, goods so classified will be carried at Commissioners' Risk without any increase in the rates charged for the carriage at Owner's Risk.

Traffic to or from unattended stations, platforms or sidings, stations in charge of caretakers, or private sidings.

Goods consigned to any unattended station platform or siding, station in charge of a caretaker, or private siding shall be carried only at Owner's Risk, and the Commissioners' liability shall absolutely cease when the goods are placed or left thereat.

Goods consigned from any unattended station platform or siding, station in charge of a caretaker, or private siding shall be carried only at Owner's Risk unless the consignor gives notice to the Commissioners that he requires the goods to be carried at Commissioners' Risk and agrees to pay the cost of tallying in addition to the rates prescribed for carriage at Commissioners' Risk and provided always that the goods will not be accepted at Commissioners' Risk if it is impracticable for the Commissioners to have the goods so tallied.

The above conditions do not apply to explosives and goods of a dangerous nature as to which see Conditions for the Carriage of Explosives and Goods of a Dangerous Nature in Volume II.

5. TALLYING CHARGES FOR GOODS CONSIGNED AT COMMISSIONERS' RISK AT UNATTENDED STATIONS, PLATFORMS OR SIDINGS, STATIONS IN CHARGE OF CARETAKERS, OR PRIVATE SIDINGS.

Where the Commissioners agree to carry goods at Commissioners' Risk from any unattended station platform or siding, station in charge of a caretaker, or private siding, the tallying charges referred to in General Condition No. 4 shall be the wages and expenses of a suitable employee computed from the time such employee leaves his home station until his return thereto.

6. CONTENTS OF PACKAGES TO BE SPECIFIED.

The Commissioners will not be bound to receive or forward any packages, whether at their risk or at the risk of the owner, unless the contents thereof be specified if required at the time of delivery for carriage. The Commissioners reserve the right to refuse goods which they or their employees may adjudge to be unsafe or unfit for carriage or of a dangerous nature.

Attention is called to Section 86 of the Railways Act 1928, No. 3759, repeated hereunder:—

"The Commissioners may test, gauge, or open any cask, case, package, parcel, or consignment whatsoever of goods delivered or offered to the Commissioners for transmission by railway, and may examine and inspect the contents thereof in order to ascertain and determine whether the description, value, quantity, number, nature, and character of such contents are correctly described in the consignment note, or invoice, or bill of lading relating to such cask, case, package, parcel, or consignment.

7. TIME OF ARRIVAL OR DELIVERY NOT GUARANTEED.

The Commissioners do not guarantee, under any circumstances, the arrival or delivery of any goods (perishable or otherwise) at any particular time, by any particular train, or for any particular market; neither do they undertake to advise consignees of the arrival of goods, nor to advise consignors or consignees that such goods are unclaimed.

8. ADDRESSING.

All consignments must bear a clear and legible address showing the consignee's full name and destination station or initials over the designated station brand. All interstate consignments must show the full name of the consignee, the destination station and State.

9. CONSIGNMENT NOTES.

(*a*) All goods delivered to the Commissioners for carriage shall be accompanied by a proper consignment note, in such one of the forms prescribed on page 225, Volume 1, and page 109, Volume II., as may be required Provided that if a satisfactory undertaking be given by any consignor on the form approved by the Commissioners, the consignment notes supplied by such consignor may, if considered suitable, be accepted in lieu of the consignment notes in the form prescribed as aforesaid.

(*b*) The number of packages and a full and accurate description of the packages and of the goods contained therein so delivered shall be entered on the consignment note, which must be signed by the consignor, or by the person sending, delivering or bringing such goods as the case may be.

(*c*) The acceptance of a consignment note by or on behalf of the Commissioners shall not be regarded as evidence that the parcels, goods, or live stock specified therein have been received by them, unless the whole of the consignment is delivered to them at the same time with the consignment note, and a receipt therefor has been given by the proper employee of the Commissioners.

Attention is called to Sub-sections (1) and (2) Section 35 of the Railways Act 1928, No. 3759, repeated hereunder :—

"(1) Every person who makes a false or misleading statement as to the description, value, quantity, number, measurement, or weight of live stock or goods delivered or offered for carriage on any railway, or with respect to any consignment note, or invoice, or bill of lading, or who refuses or neglects to give an account of any live stock or goods upon any railway, or to produce his consignment note, or invoice, or bill of lading to any officer or employee of the Commissioners demanding the same or who gives a false account, or removes, interferes with, or conceals, or attempts to remove, interfere with or conceal any part of such live stock or goods, with intent in any such case to avoid the payment, or to conceal the non-payment of any rates payable in respect thereof, shall be liable to a penalty of not more than Twenty pounds.

(2) If any consignment note, or invoice, or bill of lading in connexion with live stock or goods is presented having the value, measurement, quantity, number, or weight of such live stock or goods understated, or the description misstated, to the prejudice of the Commissioners, on such consignment note, or invoice, or bill of lading, the person responsible for the payment of the freight for such live stock or goods shall be liable to the payment of double the ordinary freight charges otherwise payable on such live stock or goods."

10. DETERMINATION OF WEIGHT.

All goods shall, except where otherwise provided or authorised by the Commissioners, be charged at the actual gross weight computed at 2,240 lb. to the ton, and wherever it is prescribed in respect of any goods that the weight thereof shall be computed by measurement or by a specified number of packages to the ton or by any other method, the Commissioners reserve the right to charge at the actual gross weight if considered advisable. (See also General Condition No. 30.)

11. FRACTIONAL PARTS OF DAYS, QUARTERS, PENCE, AND MILES.

The fractional parts of days, quarters, pence, and miles shall, unless otherwise specified, be charged as under :—

(1) DAYS.—Any fractional part of a day as one day.

(2) QUARTERS.—Any fractional part of 1 qr. less than 14 lb. will not be charged for, 14 lb. and over will be charged as 1 qr., provided that if goods in any class weigh less than 1 qr., they shall be charged as 1 qr., subject to the classification and the Tariff for "Smalls."

(3) PENCE.—Fractions of one penny less than one half-penny will not be charged for; half-penny and over will be charged for as one penny.

(4) MILES.—Any fractional part of a mile, in respect of local traffic, shall be charged as one mile; and in respect of Inter-system traffic, fractions of a mile on each of either System's proportion of the through mileage shall be charged as one mile.

12. CONSIGNEES TO GIVE RECEIPT.

A written receipt must be given by consignees for all goods received from the Commissioners. The Commissioners may demand proof of the right of the person applying for goods to receive the same or an indemnity-note signed by two approved sureties, if there be any reasonable doubt as to the person applying for the goods being entitled to receive same.

13. DELIVERY OF GOODS.

The Commissioners reserve the right to deliver any goods at the address of the consignee, and to charge for such delivery, in all cases in which the consignor or the consignee of such goods does not give notice to the proper employee of the Commissioners that delivery will be taken at the railway station by the consignee.

14. FREIGHT AND OTHER CHARGES TO BE PAID ON DEMAND, AND CLAIMS FOR REFUNDS.

(*a*) The authorized charges on all goods must be paid on demand, and in default the Commissioners reserve the right to detain and sell the goods on which such charges are payable, or to detain and sell any other property which they may then or at any future time have in their possession belonging to the person by whom such charges are payable, in order to realize the amount due. If the amount realized be insufficient to cover all charges, the Commissioners may take legal proceedings to recover the deficiency.

(*b*) The Commissioners may require the charges in respect of any goods to be paid at the forwarding station, or they may permit the charges to be collected at the destination station.

(*c*) The Commissioners reserve the right to refuse to receive or forward any consignment the value of which is not considered to be equal to the amount of the freight charge, unless the same be prepaid or unless the consignee has a credit account.

(*d*) Credit for payment of freight and other charges will not be allowed except by special arrangement with the Chief Commercial Manager.

(*e*) Claims for refunds of freight or incidental charges must be lodged with the Chief Commercial Manager within six months of the date on which the service in respect of which the refund is claimed was or was to have been performed.

15. CLASS WEIGHT MINIMUMS.

1. The minimum weights which shall, except where otherwise specified, be charged for the carriage of goods classified at the following Class Rates are as under :—

Class.	In Truck Loads. Minimum Weight per—			In Smaller Quantities.	
	11-ton Truck.	16-ton, 22-ton, or 27-ton Truck.	Bogie Trucks.	Minimum Weight.	Class.
	Tons. cwt.	Tons. cwt.	Tons.		
Firewood plus 20%	10 9	15 4	25	2 tons Actual weight subject to "Smalls" Minimum*	A B
M. Special M	10 9	15 4	25	2 tons Actual weight subject to "Smalls" Minimum*	A B
A.P.	10 9	15 4	25	3 tons 2 tons Actual weight subject to "Smalls" Minimum*	S.A.P. A B
Grain	10 9	15 4	25	3 tons 2 tons Actual weight subject to "Smalls" Minimum*	S.A.P. less 10 per cent. A less 10 per cent. B less 10 per cent.
	Subject to the following Minimum tonnages.				
A.	Minimum 2 tons			Actual weight subject to "Smalls" Minimum*	B
B.	" 1 ton			" " "	C
BB.	" 1 "			" " "	C less 10 per cent.
C.	" 1 "			" " "	1
1	Actual weight subject to "Smalls" Minimum*				
2	Actual weight subject to "Smalls" Minimum*				

* For "Smalls" Minimum Tariff, see page 108.

2. The class rate provided for the minimum tonnages shown, or alternatively for the smaller quantities specified, may be charged (subject to the tonnages stipulated therefor as a minimum) for smaller quantities where it is cheaper than to charge the rate applicable to the actual weight.

3. In no case (except in respect of package rates or where a special minimum charge is otherwise specified) shall the freight charge on any consignment be less than as prescribed in the tariff for "Smalls."

4. The Department does not undertake to supply any particular class of truck, and the several minimums specified will be charged in accordance with the class of truck used.

5. Mixed Truck Loads of Agricultural Produce.

When mixed lots of goods in the "Grain," "Wheat," "Flour, Bran and Pollard" and "A.P." Classes, for which different tonnage minimums are prescribed, are consigned on the same day by one consignor at one station to one consignee at the same destination station in the same truck, the freight shall be computed at the truckload rate, "Grain," "Wheat," "Flour, Bran and Pollard" or "A.P." Class, as the case may be, for the actual weight of each of the commodities so classified comprising the consignment, subject to the highest truckload minimum specified for any of such commodities provided that if cheaper each commodity may be charged separately at its prescribed truckload minimum, under the rate applicable thereto or the prescribed alternative rate for lesser quantities.

Exceptions.

(i) Chaff loaded with other Agricultural Produce.

When other agricultural produce in the "Grain," "Wheat," "Flour, Bran or Pollard" Classes for which a higher tonnage minimum is prescribed than for chaff, is loaded with chaff in the same truck which is consigned on the same day by one consignor at one station to one consignee at the same destination station, the total tonnage of such chaff and other agricultural produce shall be charged pro rata at the appropriate truckload rate for each commodity subject to the tonnage minimum prescribed for chaff being increased by 10 cwt. for each ton or portion thereof of such other agricultural produce contained in the consignment.

(ii) Bran, Pollard, or Sharps loaded with Flour and/or Wheat, Oats, Maize, Wheatmeal, Ground or Gristed Wheat, Barley Meal or Crushed Oats.

Truck loads comprising one consignment and consisting of mixed lots of flour, bran, pollard, sharps, wheat, oats, maize, wheatmeal, ground or gristed wheat, barley meal, or crushed oats, shall be subject to the truck load minimums prescribed for bran, pollard or sharps, **provided that the quantity of flour, and/or wheat, Oats, Maize, Wheatmeal, Ground or Gristed Wheat, Barley Meal, or Crushed Oats so loaded shall not exceed half of the total weight of the consignment.**

If the weight of the flour and/or wheat, oats, maize, wheatmeal, ground or gristed wheat, barley meal or crushed oats exceeds half the total weight of the consignment, the higher truck load minima provided for flour and wheat shall apply.

6. Motor Spirit, Lubricating Oil, and Grease loaded with Kerosene.

When any one or more of the above-named commodities, viz., motor spirit, lubricating oil, and grease, are consigned with kerosene in the same truck on the same day by one consignor at one station to one consignee at the same destination station, the freight shall be computed at the truck load rate for the actual weight of each of the commodities comprising the consignment, subject to a minimum of 8 tons per 11-ton truck and 12 tons per 16, 22, or 27 ton truck.

7. Mixed Truck Loads of Seasoned and Unseasoned Hardwood Timber.

When mixed truck loads of seasoned and unseasoned hardwood timber are consigned in the same truck on the same day by one consignor at one station to one consignee at the same destination station, the freight shall be computed at the truck load rate for the actual weight of each of the commodities, subject to a minimum of 8 tons per 11-ton truck, and 12 tons per 16, 22, or 27 ton truck.

8. Mixed Truck Loads of Softwood and Seasoned Hardwood Timber.

When mixed truck loads of softwood and seasoned hardwood timber are consigned in the same truck on the same day by one consignor at one station to one consignee at the same destination station, the freight shall be computed at the truck load rate for the actual weight of each of the commodities, subject to a minimum of 7 tons per four-wheeled truck.

9. Mixed Truck Loads of Building Lime and Lime for Manure.

When mixed truck loads of building and manurial lime are consigned in the same truck on the same day by one consignor at one station to one consignee at the same destination station, the freight shall be computed at its truck load rate for the actual weight of each of the commodities, subject to a minimum of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27 ton truck.

16. DIVIDED CONSIGNMENTS.

(A) Two or more Consignors to One Consignee.

Goods with the exceptions specified below, consigned on the same day by two or more consignors at one station to the same consignee at the same destination station, shall be treated as one consignment provided that (except in the case of caretaker and unattended stations, where the freight must be prepaid) the freight charge thereon be paid by such consignee.

Exceptions :—

(i) Butter, milk, cream, cheese, eggs, hides, skins, honey, poultry (live or dead), rabbits, hares, vealers, porkers, vegetables and fruit (other than fruit for export).

(ii) Two or more consignments of fruit or vegetables which are not intended for sale on commission sent on the same day by different consignors at the same station to one consignee at any country station shall be charged for as one consignment.

(B) One Consignor to Two or more Consignees.

Consignments comprising goods specified in any one of the following classes, viz. : "Special M," "M," "AP," "Grain," "Wheat," "Flour, Bran and Pollard," "Unseasoned Hardwood Timber," "Firewood plus 20%" or "Manure," in truck loads of not less than the minimum tonnage specified for the particular description of goods, consigned on the same day by one consignor at one station to two or more consignees at the same destination station shall be charged at the truck load rate of the respective class.

(C) Two Consignors to Two or more Consignees.

Manure in full truck loads consigned on the same day by two or more consignors at one station to two or more consignees at the same destination station, shall be treated and charged as one consignment.

(D) Consignments to Two or more Stations on the Same Line.

Each consignment for different stations shall be treated as a separate lot and charged accordingly with the following exceptions :—

1. Flour, Bran, Pollard, Sharps, Wheatmeal and Ground or Gristed Wheat and Small Lots of Wheat not exceeding 25 per cent. of the total weight of the whole consignment in truckloads consigned on the same day by one consignor at one station to two or more consignees at two or more stations on the same line shall be charged as follows :—

(*a*) for the consignment to the first destination station at the truckload rate prescribed for each commodity for the actual weight; and

(*b*) for the consignments to the other destination stations at the alternative rates and conditions applicable to less than truckloads from the original forwarding station to such other destination stations.

(*c*) The term "the same line" shall be interpreted to also apply to consignments—

From country mills to—

(i) Melbourne and a suburban station, or

(ii) two or more suburban stations, or

(iii) Melbourne or a suburban station on the Gippsland line, and stations on the Gippsland line and branches.

From suburban mills to—

A suburban station on the Gippsland line and stations on the Gippsland line and branches.

(*d*) When these mixed truckloads are consigned to two or more country stations, distant not more than 75 miles from the first destination station to the last destination station on the same line, or when consigned to two or more country stations on the same branch line not more than 75 miles from the first destination station to the last destination station (the junction station to be treated as a station on the branch line), the prescribed rate applicable to the commodity concerned shall be charged for the actual weight loaded for each station.

(*e*) The freight must be prepaid on each consignment which shall be consigned direct to its destination station.

(*f*) The tonnage minimum specified for flour, shall be applicable to the truckload rate when the weight of the flour, wheat, wheatmeal, ground or gristed wheat exceeds half of the total weight of the consignment, otherwise the tonnage minimum prescribed for bran, pollard and sharps shall apply.

2. Manure.

Manure in the truckloads specified for manure, when consigned to two or more country stations, distant not more than 75 miles from the first destination station to the last destination station on the same line, or when consigned to two or more country stations on the same branch line not more than 75 miles from the first destination station to the last destination station (the junction station or any country station which is nearer than the junction station to the forwarding station and not more than 75 miles from the ultimate destination station is to be treated as a station on the branch line) shall be charged for at the manure rate for the actual weight loaded for each station.

17. CONSIGNMENTS CONSISTING OF GOODS IN TWO OR MORE CLASSES.

(*a*) Goods classified in two or more classes consigned from one station on the same day to one consignee at one destination station may, if it be cheaper than to charge separately for the goods in each class at the rates and conditions applicable to each class, be aggregated in one or more groups, and the total weight of each such group charged under the rates and conditions applicable to the goods in the highest class in each such group.

(*b*) When several kinds of goods classified under the same class are reduced to the next lower class when in specified large quantities they may all be charged together at the rate of such lower class, if their combined weight is equal to the highest specified weight for any one description of the goods so grouped, and they are of such character that they can be loaded in the same truck.

18. TRUCK LOAD CONSIGNMENTS.

(*a*) Wherever in the classification of goods it is provided in respect of any goods that a lower rate shall be charged if a specified minimum tonnage be consigned and loaded in one truck, the truck may be loaded with such goods up to its full carrying capacity, and the quantity, if any, so loaded in such truck in excess of the specified minimum tonnage shall be charged *pro rata*.

(*b*) Wherever in the conditions of carriage it is specified that goods chargeable in any class shall be carried in truck loads in certain specified minimum quantities which are lower than the full carrying capacity of the trucks, the consignor may be required to, as far as practicable, load the trucks to their full carrying capacity.

19. CONSIGNMENTS TO BE FOR ONE BONA FIDE CONSIGNEE.

Wherever in the Goods Rates Book it is prescribed, in respect of any rate or class, that a specified minimum weight must be consigned, it means that such minimum weight must be forwarded for one *bona fide* consignee only; and if the goods are intended for more than one *bona fide* consignee at destination, then the Commissioners reserve the right to charge for the goods separately as if properly consigned in the first instance.

20. CONSIGNMENTS, SMALL OR SPECIALLY VALUABLE.

The Commissioners do not accept for carriage by goods train any package of—

(i) Gold or silver plate, gold or silver in a manufactured state, watches, jewellery, precious stones, gold or silver coin or bullion, bills, bank notes, orders, notes, or securities for the payment of money, stamps, or title deeds;

(ii) Rabbits, hare, or other furred skins weighing less than 14 lb.;

(iii) Other goods weighing less than 14 lb., excepting small parcels measuring not less than 9 inches by 6 inches by 4 inches irrespective of their weight, unless urgent transport is required, and faster transit is available by goods than by passenger train.

21. MILEAGE RATES TO BE CHARGED UNLESS OTHERWISE PROVIDED.

The mileage rates shall be charged in all cases except where other rates are published in the Goods Rates Book, or in authorized amendments thereof, or additions thereto, or except where otherwise authorized by the Commissioners.

The mileage rates per ton for the carriage of goods in the following classes viz:—"Firewood," "Firewood plus 20 per cent.," "Coal," "Manure," "Special M," "M.," "A.P.," "Grain," "S.A.P.," "Unseasoned Hardwood Timber," "Sand," "Wheat," "Flour, Bran and Pollard," and "A.A." specified hereafter and any rate for the carriage of any goods which is lower than the mileage rates per ton for Class "A" specified hereafter shall apply only where any person firm or body corporate or unincorporate who obtains or seeks to obtain the benefit of any of such rates has used the railways and no other means whatsoever for the transportation of goods in cases where railway facilities were available for such transportation. If at any time the Chief Commercial Manager or such officer as may be acting in the office of Chief Commercial Manager is satisfied upon reasonable inquiry that any goods have been transported by road or by any method of transportation other than by rail by for or on behalf of any such person firm or body corporate or unincorporate when railway facilities were available for such transportation the Chief Commercial Manager or such officer acting as aforesaid may direct that from and after the date of such direction the rates to be charged and paid for the carriage of goods in any of the said classes consigned to or on behalf of or by or on behalf of any such person firm or body corporate or unincorporate shall be the mileage rates per ton for Class "A" specified hereafter.

The Commissioners may make special rates and conditions for the carriage of goods when they are satisfied that it is advisable to do so for the purpose of conserving their traffic or regaining traffic to their railways from any competitive form of transport whether existing or otherwise, and any Freight Contract current at the date of this By-Law at special rates which are lower than the ordinary tariff rates shall continue for the period specified in such Freight Contract.

22. LONG ARTICLES.

(*a*) Any article of such a length as to require more than one four-wheeled truck for its carriage will be subject to a minimum charge of 2 tons for each four-wheeled truck, whether such be used in carrying the load or as a safety truck, when it is necessary to supply additional trucks solely for the carriage of such long articles. Provided that if the trucks so used contain an average of 2 tons each of any kind of goods for the same consignee including such long articles, no extra charge in excess of the usual freight charge shall be imposed for the long articles.

(*b*) The foregoing does not, however, apply to long lengths of Hardwood Timber, Logs, &c., for which the prescribed truck load minimums will be applicable to the truck or trucks carrying the load and a minimum of 2 tons per four-wheeled truck for the use of any safety trucks in connexion therewith. Nor will any extra charge be imposed in respect of any trucks acting as safety trucks to timber or other articles not exceeding 24 feet in length which project over the end of the truck in which they are loaded.

(*c*) Bogie trucks shall be regarded as equal to two four-wheeled trucks, and 50 feet Q., Q.B., S. or E. trucks as equal to four four-wheeled trucks, when carrying this description of traffic.

(*d*) The loading of long articles must be done to the satisfaction of the proper employee of the Commissioners.

(*e*) No long article which requires for its carriage the use of more than four four-wheeled trucks shall be received and carried, except under special agreement.

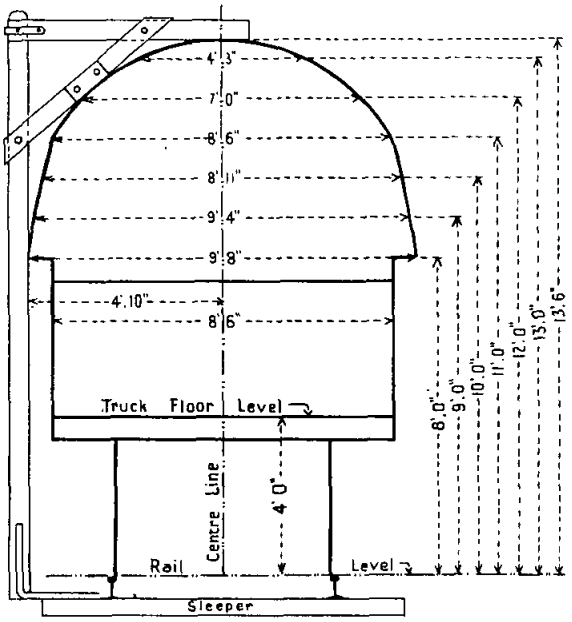
23. BULKY ARTICLES.

No person shall be entitled to have any article of goods carried by rail which, on account of its excessive bulk, would be inconvenient to transport, or which exceeds the measurements prescribed in the Loading Gauge diagram shown hereunder.

LOADING GAUGE LIMITS.

The authorised broad gauge measurements for loads are 13 feet 6 inches from the rail level in the centre, gradually rounded to 11 feet in height at the sides at which height the load must not exceed 8 feet 6 inches across, and from that point should taper to a maximum of 9 feet 8 inches at the level of the side of the truck and must not project more than 6 inches over the truck at each end.

Articles exceeding these measurements shall not be received and carried except under special agreement.



24. HEAVY ARTICLES.

The loading and unloading of heavy articles at stations or sidings unprovided with proper facilities therefor must be undertaken by the owners at their own risk and expense, and such articles will not be received for carriage from or to such stations or sidings unless satisfactory arrangements are made by the owners for loading or unloading, as the case may be.

No article weighing more than ten tons, or the weight of which exceeds the lifting capacity of the cranes at the receiving and forwarding stations, or which requires for its carriage a specially constructed truck, or any alteration in a truck, shall be received and carried except under special agreement. (see page 226)

25. CLAIMS.

Claims for loss, injury, damage, deviation, misdelivery, delay, or detention to any consignment of goods or live stock tendered for conveyance by railway, will not be allowed unless lodged in writing with the Commissioners within fourteen days after the date when delivery of the whole or any part thereof was given, or in the case of non-delivery, within twenty-eight days after the date on which the consignment was tendered for conveyance; and no claim for damage or loss will be allowed in respect of any consignment for which a clear receipt is held if lodged after removal of the consignment or any part thereof from railway premises.

26. SALE OF GOODS.

Any goods, parcel, luggage or thing of any description whatsoever on the railway premises, or stored elsewhere by the Commissioners, which are not claimed and removed by the owner may be sold by the Commissioners after the expiration of the time hereinafter specified, and after deducting any amount which may be due thereon for freight, storage, and other charges, including the expenses of sale, the Commissioners will pay the surplus (if any) to the owner on demand :—

(*a*) Any goods, parcel, luggage or thing likely to deteriorate may be sold forthwith.

(*b*) All empties may be sold after the expiration of seven days.

(*c*) All other goods, parcel, luggage or thing may be sold after the expiration of three months.

27. HOURS FOR RECEIPT AND DELIVERY OF GOODS.

Goods will be received and delivered at all stations open for goods business on days other than Sundays and days declared by the Commissioners as holidays for goods sheds as under :—

	On week days other than Saturdays			On Saturdays		
Melbourne Goods Depot—Inwards	a.m.		p.m.	a.m.		a.m.
(*a*) Perishables	12.01	to	4.45	12.01	to	11.45
(*b*) General	7.30	to	4.45	7.30	to	11.45
				For delivery of goods from No. 1 (Grain) Shed application must be made to the Goods Superintendent's Office.		
				Montague Shipping Shed is open for delivery of truck load consignments only.		
Outwards	7.30	to	2.30	a.m. 7.30 (For truck load consignments only).	to	a.m. 11.45
Suburban and Country stations ...	8.30	to	5.0	8.0	to	12 noon

28. DEMURRAGE.

GENERAL TRAFFIC.—The following provisions, marked respectively from 1 to 6 inclusive shall be applicable to all classes of traffic other than traffic for export, expressly referred to in this By-Law under the heading "Export Traffic" :—

1. A demurrage charge of 41s. 8d. per four-wheeled truck, 83s. 4d. per bogie truck, and 166s. 8d. per 50 ft. Q, QB, S or E truck shall be imposed and levied in respect of each day or part of a day of 24 consecutive hours, in every case in which any truck is detained, either for loading or unloading purposes, beyond the time hereinafter prescribed.

2. In every case in which it is prescribed in any By-Law of the Commissioners for the time being in force that the loading of goods into trucks shall be performed and carried out by the owner or consignor, three working hours will be allowed for the loading of each such truck, and if such loading be not completed and the truck consigned within that time, demurrage at the rates specified shall be imposed and levied.

3. In every case in which it is prescribed in any By-Law of the Commissioners for the time being in force that the unloading of goods from a truck shall be performed by the owner or consignee, the period allowed for unloading after the placing of any truck shall be as specified hereunder, and if the unloading of goods from such truck be not completed within that period, demurrage at the rates specified shall be imposed and levied :—

Days of Placing	Period allowed for unloading after placing
Mondays to Fridays inclusive	8 working hours Exception—Trucks which are placed at or after 1 p.m. on a Friday must be unloaded not later than noon on the following day
Saturday—	
Melbourne Goods Depot ...	4½ working hours
Elsewhere	4 working hours

4. Notwithstanding anything to the contrary contained in the immediately preceding clause 3, demurrage shall be imposed and levied on each and every truck loaded with goods which is detained at any station yard, depot, or other location, owing to accommodation not being available for the placing of such truck:—

   (i) at a private siding at the location to which the goods have been consigned;

   (ii) at a public siding at the location to which the goods have been consigned if the consignee of the goods is at the same time incurring demurrage on a truck already placed at the said siding.

5. Notwithstanding anything to the contrary herein contained (*a*) the proper employee of the Commissioners may direct that certain specified trucks be unloaded within a prescribed period of less than the number of working hours specified in clause (3), and in the event of the refusal, neglect, or failure of the consignee thereof to comply with such direction, demurrage at the rates specified shall immediately become due in respect of such trucks; or (*b*) the Commissioners may at their option load or unload goods into or from trucks (as the case may be) at the risk of the owner, consignor, or consignee, and the goods so loaded or unloaded shall be subject to the cost of such loading or unloading, as the case may be, in addition to the freight and such other costs or charges as may be or as may become due.

6. For the purpose of the foregoing provisions the working hours of each day shall be as shown in General Condition No. 27.

The operation of the foregoing provisions, marked respectively from 1 to 6 inclusive, shall not be applicable to any Sunday, nor to any other day which may be declared by the Commissioners as a public holiday for goods sheds.

EXPORT TRAFFIC.—The following provisions, marked respectively with the letters (*a*), (*b*), and (*c*), shall be applicable to trucks containing goods for the Victoria Dock at Melbourne, Williamstown Pier, Port Melbourne Pier, Geelong Pier, Corio Quay, Warrnambool Pier, Portland

Pier, and the Port Fairy Wharf, delivered at such dock, pier, wharf, or quay, or respectively, or at any siding or yard in connexion with the traffic to the said dock, pier, wharf or quay respectively :—

(*a*) A demurrage charge of 41s. 8d. per four-wheeled truck, 83s. 4d. per bogie truck, and 166s. 8d. per 50 ft. Q, QB, S or E truck shall be imposed in respect of each day or part of a day of 24 consecutive hours in every case in which any truck is detained for unloading purposes beyond the time hereinafter prescribed.

(*b*) Consignees will, subject to the provisions of clause (c), be allowed time to unload trucks as prescribed hereunder :—

The total number of trucks on hand for each of the places herein mentioned for each consignee shall be ascertained and computed at 7.30 a.m. daily, and the total number of trucks unloaded or reconsigned to another station by or on account of each such consignee during the succeeding 24 hours ending at 7.30 a.m. on the following day shall be deducted therefrom, and in respect of the balance, if any, demurrage at the rates specified shall be imposed and levied.

Provided always that in respect of any truck transferred, credit therefor shall be only given to the transferrer and not to the transferree, and only in the event of the unloading of such truck prior to the hour of 7.30 a.m. next following the hour at which such transfer was effected.

Example.

If there are 40 loaded trucks on hand for a consignee at 7.30 a.m. on Monday and 30 trucks containing goods for that consignee are unloaded or reconsigned to another station during the 24 hours ending at 7.30 a.m. Tuesday, demurrage would then be due on ten trucks.

(*c*) Notwithstanding anything to the contrary herein contained (1) the proper employee of the Commissioners may direct, in writing, that certain specified trucks be unloaded within a prescribed period of less than the time herein prescribed, and, in the event of the refusal, neglect, or failure of the consignee thereof to comply with such directions, demurrage at the rates specified shall immediately become due in respect of such trucks; or (2) the Commissioners may, at their option, at the risk of the owner or consignee, unload goods and, at the like option, store the same, and such goods shall be subject to the cost of such unloading and storage in addition to the freight, and such other costs and charges as may be or as may become due.

The operation of so much of the foregoing provisions, marked respectively (*a*), (*b*) and (*c*) as relates to export traffic shall not be applicable to any Sunday.

ALL TRAFFIC.—The following provisions shall be applicable to all classes of traffic, whether for export or otherwise :—

(*a*) Any account rendered for demurrage, loading, unloading, storage, and such other costs and charges as may be imposed in respect of any goods shall be paid to the Commissioners within 24 hours of the time of rendering such account.

(*b*) In default of payment within 24 hours the Commissioners may detain and sell such goods or any part thereof, or, if the same shall have been removed from the premises of the Commissioners, they may detain and sell any goods within such premises belonging to the person liable for the payment of such charges, and out of the moneys arising from such sale may retain such charges, together with all charges and expenses arising out or in consequence of the detention and sale of such goods, and shall render the surplus, if any, of the moneys arising from such sale and such of the goods as shall remain unsold to the person entitled thereto.

(*c*) In the event of delay in the loading or unloading of goods from any truck owing to unfavourable conditions, the Commissioners will, on request, investigate and accord fair consideration to every claim for a refund of the demurrage charges which accrued during the period of such unfavourable conditions.

29. LOADING AND UNLOADING CHARGES.

(*a*) Subject to the provisions of clause (c) of this condition, the rates chargeable for goods in Classes 1 and 2, and also for kerosene (other than in 8 or 12 tons lots), sugar, rice, beer in bulk, galvanized iron in cases, wool, milk, and cream at the package rate, other single packages at the package rate, consignments not exceeding 3 cwt. subject to the tariff for "Smalls," and empties at a fixed rate per package shall include loading and unloading.

(*b*) The loading and unloading at stations other than those provided for in clauses (d) and (e), of goods in Classes "Firewood," "Firewood plus 20%," "Manure," "Coal," "M," "Special M," "A.P.," "Grain," "S.A.P.," "Unseasoned Hardwood Timber," "Sand," "Wheat," "Flour, Bran and Pollard," "A," "AA," "B," "BB" and "C" and all other goods not provided for in clause (*a*) of this condition, shall, unless otherwise specified, be performed by and at the risk of the consignor and consignee respectively. If this work be done by the Commissioners, a charge of

4s. 0d. per ton will with certain exceptions be made for each service, which charge shall include covering and lashing, if required, provided that the tariff rate, plus either or both of the loading and unloading charges, shall not exceed the rate applicable to Class 1.

(c) The loading and unloading of all goods at no-one-in-charge stations or at stations in charge of caretakers must be performed by or at the cost of the consignor or consignee, as the case may be.

(d) MELBOURNE.

(i) Full truck loads, for one consignee, of ashes, asphalt, bark, bones, bone dust, bottles, bricks, firebricks, clay, coal (loose or in bags), compressed fodder, coke, empty returned cases, crates, and jars, felloes, fuller's earth, gravel, gypsum, hay, hides, horns, lime, manure, plaster, potteryware, pyrites, old metal, old rope, road metal, pitchers, quarts, sand, scrap iron, screenings, shell grit, skins, straw, slates, tallow, vegetables (loose), and tiles may be loaded and unloaded by the owners; if done by the Commissioners, a charge of 4s. 0d. per ton for each service shall be made.

(ii) All goods, other than those specified in Clause (a) and Clause (d) Sub-clause (i), will be loaded and unloaded by the Commissioners, and a charge of 4s. 0d. per ton for each service shall be made in respect of goods carried at rates less than Class 1, with the exception that the charge for each service shall be 3s. 4d. per ton for the following commodities :—

Barley
Barley Meal
Beans
Beetroot
Bran
Carrots
Chaff
Ensilage
Flour, white and brown
Fodder, compressed
Fruit, fresh
Grain not otherwise specified
Hay and straw in bundles or bales
Linseed
Lupol
Maize
Malt
Malt combings
Mangel-wursel
Meal, wheaten, for breadmaking
Millet
Molascuit (cattle food)
Molasses and treacle for cattle food, on declaration
Oats
Oil cake, for cattle food
Onions
Parsnips
Peas
Pollard
Potatoes
Pumpkins
Roots and seeds, agricultural, grass, sunflower, and vegetable
Rye
Salt, unrefined
Seaweed
Sharps
Turnips
Wheat
Wheat, poisoned, for rabbit killing

(e) GEELONG, BALLARAT, BALLARAT EAST, BENDIGO AND ECHUCA.

The Commissioners reserve the right to load and unload all goods at these stations, and when the work is performed by the Commissioners a charge of 4s. 0d. per ton for each service will be made in respect of all goods which should be loaded or unloaded by or at the cost of the owners.

Goods in Class C received at Ballarat or Ballarat East from or forwarded from Ballarat or Ballarat East to Melbourne or Geelong will be unloaded or loaded, as the case may be, at Ballarat by the Commissioners without charge.

30. WEIGHING OF GOODS.

(a) All grain in bags, viz., wheat, oats, barley, rye, and maize consigned for delivery at Williamstown, Williamstown Pier, Geelong, Geelong Pier, Corio Quay, Portland, Port Melbourne, Port Melbourne Pier, and the Victoria Dock at Melbourne, shall be weighed on truck weighbridges by sworn weighers appointed under the provisions of section 170 of the *Railways Act* 1928, No. 3759.

A certificate of the total weight of the grain and of the bags containing the same as ascertained by the sworn weighers will on application being made therefor by the consignor or consignee, or both, be supplied by an officer of the Commissioners, who for the purpose of this arrangement shall be entitled "Weights Clerk." The charge for weighing grain (consigned for delivery as above) by sworn weighers and for the certificate of weight shall be 4s. 0d. for each four-wheeled truck, and 8s. 0d. for each bogie truck, including 44-ton E trucks, and shall be paid by the consignor or consignee, or both, as the case may be.

Bulk wheat shall not be weighed except under the conditions shown in clause (b). Freight charges on bulk wheat shall be computed on the weights furnished by the Grain Elevators Board or by Flour Millers.

(b) Subject to the provisions of clause (a) of this condition, all goods for which the Commissioners require the weight in order to determine the freight charges, will be weighed by the Commissioners without charge.

(c) The Commissioners' Weighing Machines and Weighbridges at the forwarding or destination station may at the option of the Commissioners be used to ascertain the weight of any goods if so required by the consignor or consignee at the charges specified hereunder :—

1. WEIGHING MACHINES—

When the labour is supplied by the Commissioners, 2s. 1d. per ton; minimum charge, 1s. 6d.

When the consignor or consignee supplies the labour, 10d. per ton; minimum charge, 1s. 6d.

2. CART WEIGHBRIDGES—

	At Melbourne Goods Depot	At all other Stations
Firewood, coal, or coke, load not exceeding 2 tons ...	1s. 3d.	1s. 3d.
Goods (except firewood, coal and coke), load not exceeding 2 tons	1s. 11d.	1s. 6d.
All goods, load over 2 tons, and not exceeding 5 tons	1s. 11d.	1s. 11d.
All goods, load over 5 tons	4s. 0d.	4s. 0d.

3. TRUCK WEIGHBRIDGES—

(a) If consignors or consignees require to be supplied with the weights of truck load consignments, which have been weighed for freight purposes, weighbridge tickets will be supplied giving the desired information at the following charges, viz. :—

Each four-wheeled truck including UB truck ...	4s. 0d.
Each bogie truck, including 44-ton E truck ...	8s. 0d.

(b) If the consignor requires any goods which are not required to be weighed for freight purposes, to be weighed over a truck weighbridge, such goods may at the option of the Commissioners be weighed at the charges specified hereunder, and weighbridge tickets will be supplied giving the desired information :—

Each four-wheeled truck, including UB truck ...	10s. 3d.
Each bogie truck, including 44-ton E truck ...	20s. 6d.

(c) **Attention is called to Section 170 of the Railways Act 1928, No. 3759, repeated hereunder :—**

(1) The Commissioners may appoint sworn weighers to weigh goods and live stock carried or intended to be carried over the Victorian Railways.

(2) Every person appointed a sworn weigher under this Act shall before entering upon the duties of his office take and subscribe before a Judge of County Courts or a Police Magistrate an oath or if he object to take an oath an affirmation to the following effect :—

"I swear by Almighty God (or affirm) that I will faithfully honestly and truly and to the best of my skill and ability weigh all goods and live stock carried or intended to be carried on the Victorian Railways intrusted to me to weigh and that I will record in every weighbridge book form or certificate kept issued or given by me in respect of such goods and live stock no weight other than the true weights as ascertained by me."

(3) The weight set out in every weighbridge book form or certificate bona fide kept given or issued by a sworn weigher in respect of any goods or live stock intrusted for the purpose aforesaid to such weigher shall for determining the question of weight in all transactions with the railways in respect of such goods or live stock be accepted as the correct weight of such goods or live stock.

(4) Every sworn weigher who is guilty of any nonfeasance misfeasance or malfeasance in the discharge of the duties of his office shall in addition to any liability for damages that he may incur to any person prejudiced by his misconduct or default be guilty of an offence, and shall on conviction before a Court of Petty Sessions be liable to imprisonment for a term of not more than one year and to a penalty of not more than Fifty pounds.

(5) The Commissioners may at any time remove any sworn weigher from office.

(6) The Commissioners shall subject to the payment of such charges as may be fixed by By-Law weigh on a weighbridge all grain in truck loads consigned for delivery at Williamstown Pier, Geelong Pier, Port Melbourne Pier, Portland Pier, or the Victoria Dock at Melbourne, or at any other place the Commissioners may determine, and shall furnish a certificate of the total weight of the grain and of the bags containing the same, as so ascertained for both the consignor and the consignee of the grain.

31. STORAGE CHARGES.

(*a*) At all stations not otherwise provided for, all goods must be removed within three days after unloading, otherwise storage charges, as prescribed hereunder, shall thereafter be imposed in respect of any of such goods stored in the sheds or warehouses, or on the platforms of the Commissioners, provided that no storage charges shall be imposed in respect of agricultural produce and artificial manure stored on the platforms when there is room therefor without interfering with the handling of other goods on such platforms. (Storage of certain artificial manures is not permitted in sheds.)

Weight of Goods	In Sheds or Warehouses	On Platforms
10 cwt. or under	9d. Per Day	8½d. Per Day
Over 10 Cwt.—per ton or part of a ton ...	1s. 5½d. Per Day	1s. 1d. Per Day
Minimum charge	2s. 3d.	1s. 10d.

The Commissioners reserve the right, if deemed advisable, to impose storage charges as under in respect of any goods stored elsewhere on the railway premises; provided that any goods so stored on which the freight has not been paid and delivery taken by the consignee shall be subject to these charges.

Weight of Goods	Per Day	Minimum Charge
10 cwt. or under	5d.	1s. 6d.
Over 10 cwt.—per ton or part of a ton ...	9d.	

(*b*) (i) Any goods (including wool) unloaded at—

Ararat	Colac	Shepparton
Ballarat	Echuca	Stawell
Ballarat East	Geelong	Wahgunyah
Benalla	Hamilton	Wangaratta
Bendigo	Horsham	Warracknabeal
Camperdown	Maryborough	Warrnambool
Castlemaine	Melbourne	Wodonga

shall be allowed free storage on the day of unloading, and, at the convenience of the Commissioners, for one working day thereafter, and if not removed within such time, storage charges as prescribed hereunder shall thereafter be imposed in respect of such goods :—

For consignments of 1 ton or upwards ... 4s. 0d. per ton, or part of a ton exceeding 1 ton, per day or part thereof.

		Not exceeding 2 cwt.	Over 2 cwt., and not exceeding 5 cwt.	Over 5 cwt., and not exceeding 10 cwt.	Over 10 cwt. per ton or part thereof
For consignments of less than 1 ton	First day	1s. 1d.	1s. 5½d.	2s. 0d.	4s. 0d.
	Second day	0s. 9d.	1s. 1d.	1s. 5½d.	2s. 11d.
	Each succeeding day ...	0s. 5d.	0s. 9d.	1s. 1d.	2s. 0d.

Any portion of a day shall be charged for as one day. Minimum charge 1s. 6d.

(ii) Notwithstanding anything hereinbefore contained, if it be inconvenient to store any goods upon the premises of the Commissioners at Melbourne, after the expiration of the period of free storage, the consignee or owner may be required to remove the goods from the railway premises or the goods may be removed by the Commissioners for storage in a warehouse to be nominated by the consignee or owner, and, when so required by the Melbourne Goods Superintendent or other authorized officer of the Commissioners, the consignee or owner shall remove the goods from the premises of the Commissioners, or shall nominate in writing to such officer a store or stores to which the overtime goods may be carted, which store or stores shall not be situated at a greater distance than 9 miles from the Melbourne General Post Office, and the consignee shall pay such charges as may be in force in connexion with, or incidental to, the removal of the goods to the nominated store or stores, as the case may be, and in connexion with, or

incidental to, the storage of the goods thereat; provided that if the consignee or owner, when so required by the Melbourne Goods Superintendent, or other authorized officer of the Commissioners, fail to remove the goods, or to nominate a store or stores at which the goods may be removed, he shall pay to the Commissioners a storage charge in respect of the goods at the rate of 4s. 0d. per ton, or part thereof, per day, or part thereof, during such time as his failure to remove the goods, or to nominate a store or stores, to which the goods may be removed, shall continue.

(c) MONTAGUE SHIPPING SHED.—Any goods discharged from vessels at Port Melbourne or Williamstown Piers, or at the Victoria Dock respectively and conveyed thence by rail to the Montague Shipping Shed or outside platforms there, may at the convenience of the Commissioners, be allowed free storage in the Montague Shipping Shed or on outside platforms there for three working days after the final discharge of cargo from the railway trucks into the Montague Shipping Shed or on to outside platforms there, provided that in no case shall this free storage period be less than six working days after the final discharge of the cargo from the ship into railway trucks, and a charge at the rate of 4s. 0d. per ton per day or part thereof, minimum charge 4s. per day, shall be imposed for any subsequent period the goods remain in the Montague Shipping Shed, or on outside platforms there. The charges on consignments weighing more than one (1) ton shall be computed pro rata to the nearest quarter of a ton subject to any weight up to 2 cwt. 2 qrs. in excess of any quarter of a ton not being charged for.

If in the opinion of the Commissioners the non-removal of goods within the period of free storage hereinbefore prescribed has been due to some cause beyond the control of the consignee of the goods then the Commissioners may if they think fit on the application of the consignee grant a refund of such part of the said storage charges as they consider reasonable.

Notwithstanding anything contained herein the Victorian Railways Commissioners or their authorized officer may at any time after the expiration of the period of free storage without notice remove or order the removal to a warehouse of any such goods and the consignee or owner of the goods shall pay all charges and expenses connected with such removal including the warehouse rent and charges in addition to the said storage charges provided that no goods liable to duty shall be removed unless with the consent of or by arrangement with the Collector of Customs.

(d) CONTRACTOR'S PLANT.—The charges for the storage of Contractor's engines and trucks shall be 4s. 0d. and 2s. 1d. respectively each per month.

(e) FOR STORAGE ON.—Firewood, Stone, Sand, Metal Screenings, etc., at Suburban stations, see page 121.

(f) STORAGE ON WOOL.—See page 193.

No storage charges shall be imposed in respect of Sundays and days declared by the Commissioners as holidays for goods sheds.

32. TARPAULINS.

(a) The Commissioners do not guarantee that any goods consigned at the Owner's Risk Rate will be covered with a tarpaulin or a tarpaulin which is properly water-proof or any other covering and they shall not be liable for any loss, injury, or damage in any way caused to such goods by the absence thereof.

(b) The Commissioners do not undertake to provide tarpaulins to cover any goods which from their character are not considered by the Commissioners or their employees to require such protection, such as :—

Ashes	Ferns	Pipes
Asphalt	Firewood	*Potatoes (old)
Bark (other than Ground Bark)	Fowl Manure	Pyrites
Boats	Gravel	Roofing Slates
Boilers	Gypsum	Sand
Bones	Hardwood (unseasoned)	Scrap Metals
Bottles	Hardwood (unseasoned), cut to approved sizes for case-making	Stable Manure
Bricks	Iron or Ironwork	Stone
Clay	Limestone	Terra Cotta
Coal	Mallee Roots	Tiles (other than Ornamental or Tesselated)
Coke	Mining Timber	Waggons
Circus Material	Ores	Wheelbarrows
Contractor's Plant		

* Potatoes, old, consigned at Commissioners' Risk, must be covered and the charge for tarpaulins imposed in addition to the Commissioners' Risk rate.

(c) If the consignors desire that any goods of the nature of those described above be covered, a charge of 10s. 7d. for each tarpaulin used on the journey shall be made, and a demurrage charge of 10s. 7d. per day shall be imposed for each tarpaulin used in covering the goods if delivery of the consignment be not taken within eight working hours after arrival at the destination station.

(*d*) When the consignor is required to perform the loading of any goods for which tarpaulins are necessary, the covering of the truck must also be done to the satisfaction of the proper employee of the Commissioners, by or at the cost of the consignor, who shall make good any damage to tarpaulins arising from goods insufficiently or negligently covered, secured, or protected. If the covering of the truck be done by the Commissioners, a charge of 2s. 1d. for each tarpaulin will be imposed.

(e) The Commissioners do not provide tarpaulins to cover barbed wire, agricultural implements, machinery, wattle bark, or other goods which from their character are likely to cause damage to the tarpaulins, unless such goods are so packed as to prevent any such damage.

33. RE-CONSIGNMENT OF GOODS.

(*a*) The re-consignment of goods shall be made on the form prescribed by the Commissioners. It shall be limited to truck loads of goods re-consigned to the same consignee at another station, and shall be allowable only—

(1) When the truck is in transit.

(2) To not more than one re-consignment.

(*b*) A re-consigning charge of 3s. 11d. per ton shall be imposed in respect of such re-consignment in addition to the freight charges.

(*c*) The freight charges in respect of re-consigned goods shall be computed at the Tariff Rates for the actual continuous mileage the truck is hauled between the original despatching station and the station to which the truck is re-consigned.

(*d*) The full Tariff Rates shall be charged when the goods are re-consigned after the truck has arrived at the destination station to which it was originally consigned.

(e) If more than one re-consignment be made in respect of any goods, the ordinary Tariff Rates shall be imposed for other than the first re-consigning.

(*f*) Goods unloaded at the Melbourne sheds and afterwards consigned to another station shall, if hauled from the inwards shed to the outwards shed by the Commissioners be charged 5s. 7d. per ton, minimum 3s. 4d.

34. LIMIT OF SIZE OF BAGS OF GRAIN, ETC.

Attention is called to Section 87 of the Railways Act 1928, No. 3759, which prescribes that :—

"The Commissioners may decline to carry wheat, maize, barley, and peas if contained in a bag having a greater capacity than a bag measuring 44 inches long by 26½ inches wide."

35. COMPUTATION OF MILEAGE.

The freight charges on all goods and live stock shall be computed as for the shortest mileage by which such goods or live stock may be hauled by goods or mixed trains irrespective of the route actually used, and the mileage which shall be charged in respect of any goods or live stock passing from one line to another shall be computed *via* the junction station.

36. QUOTATION OF RATES AND FREIGHT CHARGES.

(*a*) The Commissioners will not be responsible for quotations of rates, or conditions of carriage, or particulars of freight charges given by telephone unless confirmed in writing.

(*b*) Quotations of rates or of the conditions of carriage, either verbal or by telephone, or in writing, are not binding upon the Commissioners beyond the currency of the By-Law under which such quotations are given.

37. USE OF RAILWAY CRANES.

(*a*) Consignors or consignees or their servants or agents may be permitted to use a crane or other loading or unloading facility provided by the Commissioners at any railway station yard depot or other location up to the extent of the maximum load indicated on such crane or other facility.

(*b*) The Commissioners shall not be liable for any injury or damage to any person or property caused by a breakdown in any part of such crane or other loading or unloading facility at a time when the maximum load indicated thereon is being exceeded unless a servant of the Commissioners is operating the crane or other facility at such time.

(c) When a departmental crane is used for handling material, not carried by rail, a charge of 7s. 11d. per ton (minimum charge 30s. 10d.) shall be imposed. The use of a crane for handling such material shall only be permitted when it is not required at the time for handling consignments railed or to be railed.

Conditions in Respect of the Carriage of Exhibits for Shows, etc.

1. The undermentioned conditions shall be applicable in respect of agricultural machines and implements, machinery, produce, live stock (including dogs, poultry and other birds), buggies, trotting sulkies* accompanying trotting horses, motor-cars, drays, wagons, and other goods of a like nature sent for exhibition at Shows.

They may also, by special arrangement with the Chief Commercial Manager, be made operative in respect of :—

Agricultural machinery sent for field trials in which a number of machines participate.
Horses sent for horse parades.

2. The exhibits will be carried at the convenience of the Commissioners by Passenger, Mixed, or Goods trains as may be desired by the consignors. Valuable animals may be sent by passenger trains in horse-boxes if this class of vehicle be available, and charged the same rate as if loaded in cattle trucks and forwarded by Goods trains. Each 3 stalls in "F" or "FF" horse-box to be regarded as equivalent to one full cattle truck.

3. When the exhibits are consigned on the forward journey, the consignors will require to declare, on the consignment note therefor, that they are for exhibition, specifying the Show for which they are intended, and the way-bill must be endorsed accordingly.

The ordinary freight charges shall be imposed and prepaid in respect of the forward journey in all cases.

4. On the return journey any exhibits, or portion thereof, which have been sent by rail as paying freight on the forward journey, exhibited, and not sold, will, subject to the conditions in clause 7, and on production of the approved certificate signed by the Secretary of the Show, be carried "free" to the station whence they were forwarded, provided the journey is completed at any time within one month from the date of despatch on the forward journey. The certificate must be handed in when the exhibits are consigned, and attached to the way-bill.

If on the backward journey a greater number of vehicles be used for the carriage of exhibits than was used on the forward journey, ordinary freight charges will be imposed in respect of exhibits carried in the additional vehicles.

Tarpaulins, if used at request of owner to cover live stock trucks, shall be charged for at the rate of 10s. 7d. per tarpaulin.

All exhibits, or part thereof, which have been sold, or are not accompanied by the aforesaid certificate, will be subject to the ordinary freight charges and conditions on the backward journey.

5. Exhibits, accompanied by the aforesaid certificate, may be forwarded on from one Show to another, subject to the freight charges and conditions, as prescribed in clause 3 of this division, which freight charges must be prepaid, and when the final destination is reached, they may be returned "free" to the original despatching station, subject to the conditions in clause 7. If, however, in proceeding to another Show, such exhibits be returned over any portion of the line in respect of which the full freight charges have been paid on the forward journey, the exhibits may be sent "free," subject to the conditions in clauses 4 and 7, over such portion of the line.

6. Exhibits must be loaded and unloaded by or at the expense of the consignors or consignees. In all cases where Show exhibits are loaded or unloaded by the department the charges prescribed for the service shall be imposed.

7. Exhibits, other than live stock, may, where alternative rates apply (see General Condition No. 4), be consigned on the forward or the backward journey at the sender's option either at the rates and conditions prescribed in the Goods Rates Book for carriage at Commissioners' Risk or at Owner's Risk. On the backward journey, when consigned at Commissioners' Risk, the full Commissioners' Risk rate shall be imposed, but if such exhibits be consigned at Owner's Risk they will be carried "free."

Live stock exhibits shall be carried at the prescribed rates for live stock on the forward journey, and shall be carried on the backward journey "free," but whether on the forward or the backward journey, such carriage shall be subject to the conditions prescribed in the Goods Rates Book in respect to the carriage of live stock.

8. Owners of stock going to and returning from Shows may be allowed free carriage each way of fodder, as specified hereunder, if it be loaded in the trucks with the animals :—

1 cwt. for each horse or head of cattle.
20 lbs. for each pig or sheep.

* *Wherever practicable trotting sulkies shall be afforded similar transport on the return journey as was given on the forward journey, so that, as far as possible, the sulkies may accompany the horses.*

9. Exhibits from the Department of Agriculture, viz., wheat, straw in bulk, olive oil, wine, honey, &c., fruit (preserved), charts, &c., fibres, including flax, hemp (dressed and undressed), willows, manures, vegetable oil, eggs, also the necessary marquees, poles and pegs, tables, trestles, and fittings, forwarded to the various Agricultural Shows in the State, shall be carried "free" both on the forward and backward journeys at owner's risk; when consigned at Commissioners' risk on the forward or backward journey the full Commissioners' Risk Rate shall be imposed.

10. District exhibits forwarded by local Agricultural Societies to the Royal Agricultural Show shall be carried on the forward journey at the Grain Rate subject to the tariff for "Smalls" and returned "free" under the usual Show conditions.

11. Consignors should give not less than four (4) days' notice to the stationmaster of the number and description of live stock trucks, and 48 hours for other trucks, required to load exhibits.

12. For concessions in fares to Stock Attendants, see Passenger Fares Book.

DEFINITION OF PACKING FOR CARRIAGE OF GOODS AT COMMISSIONERS' RISK.

(A) BAGS.—Bags new or in a condition equal to new shall be made of jute, canvas or calico of sufficient strength to carry the contents. The openings of the bags must be securely sewn to prevent the escape of the contents.

(B) BALES.—Bales shall consist of packages wholly covered with hessian or canvas of sufficient strength and thickness to carry the contents and shall be completely sewn up or secured by metal clips or staples.

(C) FIBREBOARD CONTAINERS.

"Solid fibreboard containers and corrugated fibreboard containers must be new and of such composition and strength to comply with the following specification."

(*i*) Solid fibreboard containers must be made of three or more plies of basic fibreboard of good bending quality with all plies firmly glued together and the outer facing water resistant to the degree specified in Section 8 (E).

(*ii*) Corrugated fibreboard containers must be made of double-walled or triple-walled corrugated board of good bending quality with each facing firmly glued to the corrugated sheet and the outer facing water resistant to the degree specified in Section 8 (E).

(*iii*) For any fibreboard container and its contents to be accepted for carriage by rail at Commissioners' Risk, the following requirements must also be complied with :—

TABLE A.

Maximum Gross Weight of Contents :	Minimum Average Bursting Test in Lbs. per sq. inch Mullen or Cady	Solid Fibre Containers Board Thickness	Corrugated Fibre Containers Thickness of Facings				
			Double Wall Single Corrugation		Triple Wall Double Corrugation		
			Outer facing	Inner facing	Outer facing	Centre facing	Inner facing
40 lbs.	175	.080″	.016″	.014″	.016″	.014″	.014″
60 lbs.	200	.080″	.016″	.016″	.016″	.014″	.016″
90 lbs.	275	.095″	—	—	.016″	.014″	.016″
112 lbs.	350	.115″	—	—	—	—	—

(*iv*) Contents of Fibreboard Containers.

(*a*) The contents of any container must not exceed the maximum gross weight set out in the above Table "A."

(*b*) Liquids or solids—in glass or earthenware containers must be packed with internal liners and/or partitions to afford adequate protection against breakage or damage. Partitions which must be used with all glass or earthenware containers, must be the full height of such containers.

The capacity of any glass or earthenware container must not exceed 5 pints.

(c) Liquids or solids in tins must be packed so as to provide adequate protection against breakage or damage. The quantity of liquid in any one fibreboard container must not exceed 6 gallons, with the maximum capacity of any one tin not exceeding one gallon.

(d) Articles other than substances contained in glass or earthenware bottles or jars, or in tins, must be packed as necessary to provide adequate protection against breakage or damage.

(v) Manufacturers' Joint of Containers.

(a) Stitched joint—of flat wire at least 3/32 inch x 0.019 inch formed into staples about 7/16" wide, with maximum spacing between stitches of 2 inches.

(b) Tape joint—cloth tape or sisal reinforced kraft tape minimum width 2 inches.

(c) Glued laminated joint—constructed by the removal of corrugated sheet and centre liner, if one is used, and splicing inner and outer facings over the end of abutting double faced corrugated board.
The overlap must extend not less than 1" and both facings be securely glued.

(d) Glued lap joint. The sides of box forming joint must lap not less than 1¼" and be firmly glued throughout the entire area of contact.

(vi) Manufacturer's Certificate.
Each container must bear the manufacturer's name and certificate showing the maximum gross weight of contents which the container is designed to carry.

(vii) Sealing of Containers.

(a) The four flaps on the tops and bottoms of containers must be securely fastened with suitable adhesive covering at least 50% of contacting flap area.

or

(b) The flaps stitched with wire or staples of sufficient strength and number to securely fasten the containers.

(viii) Testing of Fibreboard.

(a) **Sampling.**
All tests to be made on samples taken from containers in good condition.

(b) **Conditioning.**
All tests shall be made after the board has reached equilibrium at a relative humidity of 65% and temperature of 20°C.

(c) **Thickness Test.**
The thickness of solid boards and facings shall be determined by measuring the thickness of each of 10 samples of the material and taking an average of the readings obtained. A dial type micrometer, having a circular foot between 0.56" and 0.65" in diameter, and a pressure of 7 to 9 lbs. per sq. inch shall be used for this purpose. A manufacturing tolerance on thickness of -5% shall be permitted.

(d) **Bursting Strength.**
The bursting strength test shall be carried out in the manner described in Aust. Standards specification N. 21. Six tests shall be made, 3 from each side of the board. If the board fails to pass this test, 24 tests shall be made, 12 from each side of the board. If the average of these tests does not fall below the value as specified in Table "A," the board shall be considered to pass the test. All two-pop tests shall be considered void.

(e) **Water Resistant.**
The outer facing of the container shall be water resistant so that the water absorption (Cobb/30 mim.), when determined by the method described in Aust. Standard specification N. 21, shall not exceed 150 grammes per sq. metre.

(D) CASES.—Cases must be constructed of wood and have solid or closely fitting sides, ends, tops and bottoms, securely fastened without projecting nails, screws or bolts, and be of sufficient strength and rigidity to safely carry the contents during handling and transit. Cases must hold the contents securely in place and be so constructed that the contents may be taken into and out of the vehicle within the case. Cases of unusual size and carrying heavy weights must, unless otherwise provided be strapped with metal or wire binding, or be re-inforced by cleats. When the contents of a case include a tin or tins without cushioning material, the internal dimensions of the case must be such that the tin or tins are a close fit and unable to move about within the case after closing.

(E) WOODEN BARRELS OR CASKS (wet).—A barrel or cask is a round bulging coopered wooden container made of staves, heads and hoops, and constructed to hold liquids without leaking. Staves and heads shall be constructed of the best quality of barrel making woods well seasoned, free from knots, worm holes, patches and other defects. The barrels must be new or in condition equal to new and the bilges must be well cambered. The heads must be without signs of "rippling," and made of one piece or of two or more pieces closely fitted and held securely in crozes (grooves) by hoops.

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The timber must conform to the following sizes :—

Capacity of barrel	Thickness of staves and heads not less than
Up to 20 gallons	¾"
21 to 36 gallons	1"
37 to 65 gallons	1¼"

Barrels of any capacity for the carriage of beer must be constructed of staves and heads at least one and one half inches thick.

(F) WOODEN BARRELS OR CASKS (dry).—A barrel or cask to carry dry commodities is a round coopered wooden container made of staves, heads and hoops. Staves and heads must be constructed of the best quality of cask making woods well seasoned, free from knots, worm holes, patches and other defects. The hoops must be so fitted that the heads will be held securely in place, in order that there will be no possibility of leakage or sifting. If the heads are one half inch or more in thickness they may be countersunk or set below the crozes (grooves), but must be secured by two or more battens extending completely across the heads, and the ends of the battens must be nailed to the sides of the container or the heads held in place by a head liner. The barrels and casks must be new or in condition equal to new and the staves and heads must be constructed of timber at least one half inch in thickness. The maximum weight of a (dry) barrel and contents must not exceed 5 cwt.

(G) CRATES.—Crates must be constructed of wood and be of strength and sufficient rigidity to safely carry the contents during handling and transit. The crates must protect the contents on the sides, ends, tops and bottoms, and no part of the contents shall protrude. The crates must hold the contents securely in place and be so constructed that the contents may be taken into and out of the vehicle within the crates. Surfaces liable to be damaged must be fully protected. The sides of the crates must be diagonally braced and the battens must be of sound timber free from defects that materially lessen their strength, and the space between the battens must not exceed three inches. Crates of unusual size or carrying heavy weights must be strapped with metal or wire bound. Crates in cylindrical form must be re-inforced at the ends by metal or wire binding.

(H) FURNITURE.—

1. All removable handles, rails, castors and similar projections should be dismantled, or protected as shown in clause 2.
2. Unremovable projections and corners and edges should be well padded with either woodwool, corrugated cardboard, or felt strips, and the packing tied into position. Tables, chairs or other items of furniture with legs not otherwise braced must be suitably battened.
3. All polished or upholstered surfaces to be covered with :—
   (a) one layer of under carpet felt, or
   (b) two layers of glossy paper, or
   (c) four layers of newspaper.
4. The final protection to be a layer of heavy sisalkraft or heavy standard wrapping paper (not ordinary brown paper) hessian, cloth or similar covering, securely tied.
5. If flock or felt lined calico or canvas covers are used, the internal paper wrapping is not necessary.
6. If standard felt lined calico or canvas covers are used as external covering, the location of the glass portions of the furniture must be marked on the outside of the covers "glass with care," otherwise glass portions of furniture must not be covered by packing, but must be left exposed.
   Movable mirrors are to be dismantled, packed, crated and marked "Glass with care," except those which can be secured inside of wardrobes, etc., unremovable mirrors should be cross tied with rope to avoid breakage should the metal clips fail to hold.
7. **Furniture with laminex, formica or similar surfaces.** Pack as in Clauses 3 and 4 and mark outside wrapping "Laminex, etc."

SPECIAL SPECIFICATIONS FOR CERTAIN ARTICLES.

8. **Pianos, Piano-players and Organs.**—To be packed in cases with the bottoms and the backs of the instruments securely screwed to the cases.
9. **China, crystal and cocktail cabinets.**—To be packed in wooden cases or wooden crates with suitable internal wrappings. The outsides of the cases or crates should be marked "Glass with care." Glass shelves should be removed and crated separately.
10. RADIO SETS.—

Console, radiogram, and car radios.—In triple walled double corrugated fibre containers.

Mantle type.—In double walled single corrugated fibre containers with internal liners. Knobs and dials must be so protected that they will not contact the outer walls of the containers. The outsides of all containers should be marked "Radio—fragile."

11. **Chairs not upholstered or polished.**—Pad corners and wrap in one layer of heavy standard wrapping paper.

12. REFRIGERATORS (**Domestic type**) :—

(*a*) Cover enamel surfaces with four layers of newspaper or one layer of waterproof paper, and completely enclose in strong wooden cases or diagonally braced wooden crates suitably cleated with felt faced cleats to prevent abrasion and internal movement. The spaces between the battens of the crates must not exceed 3 inches, and the sides and ends of each crate must be diagonally braced. The refrigerators should be bolted to the bases and the backs of the cases and crates.

(*b*) Brand the cases or the crates with the words, "Refrigerator, This End Up."

Commercial Type.—Packing as for the household type, but when crates are used the spaces between the battens must not exceed 6 inches.

13. **Clocks.**—Pack in cases and protect against internal movement. Mark "Fragile—Handle With Care."

14. **Electric radiators.**—Pack in cases or solid fibreboard containers and protect against internal movement.

15. **Sewing machines.**—

(*a*) Treadle—In strong cases or crates sufficiently packed to prevent internal movement.

(*b*) Electric—In strong cases sufficiently packed to prevent internal movement.

16. **Mattresses.**—

(*a*) Wire, with wooden rails—Cover side and end rails with one layer of heavy standard wrapping paper.

(*b*) Steel, Spring or Flock—Wrap in two layers of heavy quality sisalkraft or heavy standard wrapping paper (not ordinary brown paper).

17. **Ornaments.**—In cases or crates with suitable internal protection and marked "Fragile—Handle With Care."

18. **Pictures.**—To be packed in cases or crates and the outsides marked "Fragile—Handle With Care."

(I.) DRUMS FOR LIQUIDS.—Drums of 4 and 5 gallon capacity must be constructed of the best quality steel, 24 Birmingham Gauge.

Every drum must be clearly marked by embossing as follows :—

(*a*) The nominal capacity.

(*b*) Gauge of steel.

(*c*) Year of manufacture,
i.e., 4—24—54.

Drums must be either new, or in condition equal to new. The construction and marking must be in accordance with the Australian Standard Specification (E)K.503.

Drums of 10 gallon capacity and over must be made of good quality thoroughly annealed low carbon, mild steel sheets, in accordance with the following specifications :—

Nominal capacity Imperial gallons	Minimum thickness of body and head B. G.
10	18
12½	18
25	18
44	16

Every drum must be clearly marked as follows :—

(*a*) The gauge of the steel used (Birmingham Gauge).

(*b*) Nominal capacity (Imperial gallons).

(*c*) Year of manufacture,
i.e., 16—44—54.

The drums must be either new or in condition equal to new.

The construction and marking must be in accordance with Australian Standard Specification K.87, in all details.

(J.) DRUMS MADE OF MILD STEEL WITH LEVER OR CRIMPED LIDS, OR OTHER SUITABLE LARGE APERTURE CLOSURES FOR POWDERS AND SOLIDS.

Side seams to be electrically butt-welded. Ends double seamed to the body. Drums to be made liquid tight by suitable resilient gaskets under the lids.

Drums must conform to the following specifications :—

Nominal capacity Imperial gallons	Minimum thickness of body and head B. G.
44	18
25	20
12½	20
5	24
4	24

(K.) JARS, earthenware or glass. 1 to 3 gallon capacity, in cases or crates, protected by cushioning material, maximum 2 jars to a case or crate; jars must be separated by a wooden partition.

Earthenware jars may be packed in wickerwork.

(.L) CARBOYS.—Glass, in wooden cases with an efficient protective device or bridge covering the projecting neck of the carboy. Straw, woodwool or similar material must completely fill the space between the carboy and the wooden case, except when rubber cushioning pads are used.

Internal Packing.

When articles are packed in a container they must be protected by or with partitions, wrappers, straw, sawdust, woodwool or other cushioning material that will afford adequate internal protection against injury and damage.

Marking and labelling.

Cases, crates, fibreboard containers or packages which require to be so carried must be marked "TOP, THIS SIDE UP."

When such packages contain fragile articles or articles in glass or earthenware, they must be so consigned and marked or labelled "FRAGILE, HANDLE WITH CARE" or "GLASS HANDLE WITH CARE."

Addressing.

All consignments must bear a clear and legible address showing the consignee's full name and destination station or initials over the designated station brand. All interstate consignments must show the full name of the consignee, the destination station and State.

Alphabetical Classification of Goods.

Abbreviations and References.

Abbreviations :—

N.O.S. Not otherwise specified.
C.R. Commissioners' Risk.
O.R. Owner's Risk.
P.C. Per Centum.

References :—

* For concessions to Victorian Country Industries see pages 139 to 148.

Packing References :—

Articles in the following classification referenced by the symbols a, b, c, d, e, f, g, h, i, j, k, or l when in good order and condition and packed to the satisfaction of the Commissioners, in the manner prescribed hereunder opposite the respective symbols, and when the containers used conform to the specifications appearing under the heading "Definitions of Packing" page 26 hereof may, at the sender's option, unless expressly stipulated as being carried at the owner's risk only (see General Condition No. 4) be consigned for carriage at Commissioners' risk rate and conditions.

a In bags.
b In bales.
c In fibreboard containers.
d In cases.
e In barrels (wet).
f In barrels (dry).
g In crates.
h Packed in accordance with specifications for furniture.
i In drums (wet).
j In drums (dry).
k In jars.
l In carboys.

Articles not referenced by one of the above symbols, which by their nature do not require packing, may, except otherwise herein provided (see General Condition No. 4) be consigned for carriage at Commissioners' risk rate and conditions.

Tonnage Minimum References :—

Except where otherwise provided the minimums prescribed in General Condition No. 15, page 12 shall be applicable.

Special tonnage minimums indicated by one of the reference marks shown hereunder shall apply where provided for in the Alphabetical Classification of Goods.

Reference	Tonnage Minimum applicable							
	Per 11 ton truck		Per 16, 22 or 27 ton truck		Per "QR" Truck		Per "Q," "QB," "E" or "S" Truck	
	Tons	Cwt.	Tons	Cwt.	Tons	Cwt.	Tons	Cwt.
aa	6	0	6	0	—	—	—	—
ab	6	0	8	0	—	—	—	—
ac	5	0	7	10	10	0	17	0
ad	7	0	10	0	—	—	—	—
ae	7	0	10	10	—	—	—	—
af	8	0	12	0	16	0	24	0
ag	6	0	8	0	12	0	16	0
			High sided 16 ton truck 7	0				
ah	5	0	5	0	10	0	—	—
ai	10	9	15	4	25	0	25	0
aj	9	10	14	5	—	—	—	—
ak	7	0	10	0	14	0	20	0
			High sided 16 ton truck 8	10				

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NOTES.

1. All goods not specified in the Classification of Goods in Volumes 1 and 11 shall be charged Class 2 rates Owner's Risk.

2. Wherever in the Classification of Goods in Volumes 1 and 11 Up journey rates are specified, they shall apply only for the mileage on the direct Up journey from the forwarding station, but shall not be operative in respect of goods carried by water to ports and railed thence on the Up journey, provided that commodities consigned direct for export from country stations to Port Melbourne pier or Williamstown pier shall be charged Up journey rates where specified for the actual mileage to such piers.

3. Up journey rates are not applicable in respect of consignments forwarded from suburban stations within 20 miles of Melbourne (Flinders Street or Spencer Street).

4. Raw materials for Victorian Country Industries, consigned at suburban stations situated within 20 miles of Melbourne and forwarded on Up journey to Melbourne or to the junction station and thence on Down journey to destination, shall be charged the reduced Down journey rate where specified for the total mileage from the forwarding station to the destination station.

5. For goods despatched between stations involving an Up and Down journey rate, the charges shall be computed on the sum of the Up and Down journey rates if cheaper than the through Down journey rate. Where the route embraces two or more Up and Down journeys, the total Up mileage is to be charged at Up journey rate, and the total Down mileage at Down journey rate, if cheaper than the through Down journey rate. Consignments forwarded from country to suburban stations within 20 miles of Melbourne, and involving an Up and Down journey rate, shall be subject to the District rates provisions.

6. When goods which are charged at a reduced rate on the Up journey are forwarded over a route embracing both an Up and Down journey, and a District rate operates in respect of the latter, such goods shall be given the benefit of the District rates on the Down portion of the journey.

7. Wherever in the Classification of Goods in Volumes 1 and 11 an entry is made after any commodity thus, "Minimum 2s. 0d.," it means that the actual weight of such commodity shall be charged for at the rate shown, subject only to the minimum charge stated, and irrespective of the tariff for "Smalls."

8. Wherever in the Classification of Goods in Volumes 1 and 11 an entry is made after any commodity "Smalls Minimum" it means that such commodity shall be charged at the Classification rate shown subject to the tariff for "Smalls" and shall not be charged at the higher alternative rate for smaller quantities.

9. Class S.A.P. less 10 per cent. shall not be less than class "Grain."

10. Class "A," less 10 per cent. shall not be less than class "Grain."

11. Class "B," less 10 per cent. shall not be less than class "A."

12. Class "B," plus 25 per cent. shall not be more than class "C."

13. Class "C," less 20 per cent. shall not be less than class "B."

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
A.		
(For Explosives and Goods of a Dangerous Nature see Volume 11)		
Acetate of Lime "Smalls" Minimum	B	a, c, d, f
" " (see aa)	A	
Acids Non dangerous		c, d, e, j, k, l
" n.o.s.	2	
" Citric	2	
" Lactic	A	
" Tartaric	*2	
Advertising Materials (see page 136)		
Aerated Water (see soft drinks)		
Aeroplanes or Gliders—Aeroplanes or Gliders or sections of aeroplanes or gliders such as wings, tails or fusilages		d, g
Per truck Per mile s.d. 4 wheeled 3.4; "QR" bogie 6.8; "Q" "E" or "S" 10.0 — Minimum charge £5.7.6		
Agricultural Implements and Machines n.o.s. (Vehicles excepted)		
Packed	*1	d, g.
Set up, Loose C.R.	*1 plus 10 p.c.	
O.R.	*1	
In pieces, in cases or in bundles (see ae)	*C	d, g.
Parts of, including castings and mouldings—		
Finished	*1	d, g.
In the rough and not machined or polished	*C	d, g.
" " " " (see ae)	*C less 20 p.c.	d, g.
Secondhand—(see page 56)		
Ale, Beer and Stout		d, e, g, i.
Bulk—		
n.o.s.	C	
Produce of Commonwealth on "up" journey	*B	
Bottled—		
n.o.s.	2	
" (see ae)	1	
Produce of Commonwealth	*1	
" " " (see ae)	*C	
Bulk and bottled, mixed lots, Produce of Commonwealth, minimum quantity of bulk, 1 ton (see ac)	*C	
Certified by Customs Department to be unfit for consumption (see ae)	M	
Alum (Alumina Sulphate) and Alum Cake	C	a, c, d, f, i, j.
Alumina, Sulphate of (Water purifier)	*A	
Aluminium, n.o.s.	2	d, g.
" Ingots	C	
" Roofing Sheets, corrugated	C	d, g.
" Scrap	B	a.
Alumite	M	
Ammonia—		
Liquor. In Owner's tank truck, minimum 35 tons per tank truck. (Empty tank trucks shall be returned free).	C less 33⅓ p.c.	
Sulphate of (see Manures)	A	a, c, d
Ammonium Chloride	C	a.
" " (see ai)	B	
Anchors	1	
Animals (See Live Stock)		
Ant exterminators	A	c, d.
Antimony—		
Oxide	A	a,

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Anvils	1	
Aqueous Liquid (see ae)	M	d, g, i, k, l.
Architraves, Hardwood and Softwood. (See Timber).		
Arms and Raisers, hardwood (for telegraph or electric light poles) with or without attached iron braces, bolts or caps	A	
" " " " " (see ad)	A less 25 p.c.	
Arrowroot, manufactured, n.o.s.	2	c, d.
" Produce of Commonwealth in bulk	B	a.
Arts, Works of, Packed	2 plus 50 p.c.	d.
" " Loose, at O.R. only	Double 2	
Artificial Manures (see Manures)		
Asphalt—		
In bags	M	a.
In blocks	A	
Boilers (see Bitumen Kettles)		
Asphaltum	C	e, i.
Asbestos—		
Flake	A	a.
Fire lumps	2	d.
Sheets (see Plaster)		
For covering steam pipes	C	b.
Ashes, n.o.s. (see af)	M	
Ashes, Wood (see Manures)		
Augers, Wheat, set up and consigned either separately or affixed to Grain Bin Trailers on Wheels	Vehicle rates	
Augers, Wheat, dismantled i.e. length of tube reduced by 50%, support from axle to tube lowered, engine protected by packing and the whole consignment reduced to one compact package and protected by side boards	2	
Axes	1	d, g.
" loose, at O.R. only	2	
Axle Arms and Boxes, Iron	*1	d, g.
B.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
§Bacon	*B, less 10 p.c.	a.
" for export (see pages 155 and 188)		
" Single Package Rate (see page 133)		
Bagging, teased	C	b.
Bagging, old, for paper-making	A	b.
Bags and Cornsacks, new, "Smalls" minimum	A	b.
" including Calico bags and Cornsacks, second-hand minimum, 2s. 0d.	A	
" Calico (new)	1	b.
" Jute, Wax-paper lined	B	b.
" Paper (see Paper)		
Baking Powder—n.o.s.	2	c, d.
" " Produce of Vic. country industry, on Up journey	*1	c, d.
Ballast (see Stone)		
Balls, steel, for grinding (see Castings and Forgings)		
Balsa Wood	2	
Bank Notes, Bills or Orders (see General Condition No. 20)		
Bark, bundled, or in sheets—n.o.s.	B	b, g.
" " " " see (ab)	A	b, g.
" " " " Consigned to country bark mills or country tanneries in Victoria (see (ab))	*M	b, g.

§ Bacon in refrigerator trucks shall be charged Tariff Rates plus 25% subject to a minimum of the full weight carrying capacity of the truck used.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Bark, extract of, n.o.s.	A	a, c, d, f, i.
" " Consigned to country tanneries in Victoria— Minimum 10 cwt.	*A, less 25 p.c.	a, c, d, f, i.
" " " see (ab)	*M	a, c, d, f, i.
Bark—ground or chopped, n.o.s.	B	a.
(Minimum 9 tons per 11 ton truck and 13½ tons per 16, 22 or 27 ton truck) ...	A	a.
" Consigned by Victorian country bark mills to tanneries situated within a radius of 20 miles of Melbourne or to the seaboard for immediate export beyond the State (minimum 9 tons per 11 ton truck 13½ tons per 16, 22 or 27 ton truck)	M, plus 5 p.c.	a.
" Consigned to country tanneries in Victoria (minimum 9 tons per 11 ton truck 13½ tons per 16, 22 or 27 ton truck)	*M	a.
Bark Fibre	A	b.
Barley (see page 151)	Grain	a.
" for seed (see page 160)		
" Pearl—		a, c, d.
n.o.s.	1	
Produce of Commonwealth, on Up journey	B	
" Crushed or ground "without addition of other ingredients for stock food" and so consigned	Grain	a.
" Husks, (see (ab))	Grain	a.
Barriers, comprising posts and rails	1	
Basils (see Leather)		
Baskets and Basketware—		
Packed	2	d, g.
Loose, at O.R. only	2, plus 50 p.c.	
Baths, enamel, cast iron, pressed steel, and galvanized iron, and basins, enamel and cast iron—		d, g.
n.o.s.	2	
Produce of Commonwealth	*1	
Bath Heaters	*2	d, g.
Bath Screens	2	d, g.
Batteries, Wet, charged, (see Volume 11)		
n.o.s.	2	c, d, g.
" Old (liquid contents to be emptied), declared on consignment note to be converted into scrap	A	
Beans—		
" Cocoa or Coffee	2	a.
" Haricot, n.o.s.	1	a, c, d.
" " Produce of Commonwealth, on Up journey (see (af)) ...	Grain	a.
" Fresh, French, and Broad (same as vegetables)		
" n.o.s. (see (af))	Grain	a.
Bearers, iron	2	
Beaver Board (as Wallboards)		
Bedplates, iron	2	
Bedsteads*, (same as Furniture)		
Bedding*, (same as Furniture)		
Beehives—		
" with bees, in truck loads	* *	
" in lesser quantities, at O.R. only	2	
" without bees—		
" set up	C	g.
" in pieces, "Smalls" minimum	A	g.
Beer (see Ale)		

* * Full sheep truck rate plus 33⅓p.c.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Beeswax	*1	a, d.
Beet-root (see af)	Grain	a.
„ „ small quantities, "Smalls" minimum	A, less 10 p.c.	a
Bellows—n.o.s.	2	d, g.
„ Produce of Commonwealth on Up journey	C	d, g
Bells	2	d., g.
Belting, leather and rubber	2	b, d. g.
Benches—carpenters' and Sloyd	1	d, g.
„ Saw	*1	d, g.
Bench Gauge	1	d, g.
Bicarbonate of Soda (see Soda)		
Bicycles packed	2 plus 25 p.c.	c, d, g.
„ loose, at O.R. only	Double 2	
Bicycle parts	2	d, g.
Biege* (same as drapery)		d, g
Billiard and Bagatelle Tables, (same as Furniture)		
Biscuits minimum, 2 tons	*1	d, g.
„ smaller quantities	*2	d, g.
„ Dog	A	d, g.
Bitumen	C	i.
„ in Owners tank truck, fully loaded (empty tank truck returned free)	A	
„ Emulsions such as Colas, Colbion, Colfix, Emoleum, Mexphalt, Texaco Cold Patch, Vialit	C	i.
„ Kettles	1	
„ „ on statutory declaration that same have been in use for 3 years or more	C	
Blacking in bulk	1	c, d, f, j.
Blacklead	2	c, d, f, j.
Blankets, Flannels and Woollens, including Woollen Rugs, and* Woollen Cloth n.o.s.	2	b, c, d.
„ Produce of Commonwealth	1	b, c, d.
„ „ „ on Up journey, "Smalls" minimum ...	*C	b, c, d.
Blinds	2	c, d, g.
Blood, dried, used in the manufacture of stockfood	AP	a.
Blue	2	c, d.
Boats (see page 131)		
Boilers and Coppers, Kitchen, Washing, &c., n.o.s. (including iron frames for boilers)	*2	d, g.
Boilers, or boiler parts, steam (all projecting parts crated or removed and cased. Second-hand to be also subject to inspection by Departmental Engineer at cost of sender, **C.R.**)	*1	
(see (af))	*C	
Boilers Secondhand (see Machinery secondhand and mining)		
„ Tubes (see page 77)		
Bolts, iron	*1	a, d.
Bone Dust (see Manures)		
„ Flour for poultry food)	AP	a.
„ Grit	AP	a.
„ Meal	AP	a.
Bones (see (ab))	M	a.
„ Bottles and Scrap Iron, mixed lots (see (ab))	M	a.
Books (see page 137)	2	c, d.
Boot Polish, Cream and Gloss	2	c, d.
Boots and Shoes	*2	c, d.
Borax and Borax Glass	1	a, c, d.
„ crude	A	a, d.
Bottles, Glass		d, g.
„ „ for Aerated Water and Temperance drinks minimum 2s. 0d.	A	
n.o.s. "Smalls" minimum	A	
(see (ab))	M	
„ Earthenware product of Victorian Country Industry (see ai) ...	M, less 21 p.c.	d, g.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Bottle caps tin for covering bottles and pickle jars, minimum 2s. 0d. ...	C	c, d, g.
Boxes, Empty, (see Empties page 122)		
Brackets, iron, for telegraph poles	1	a, d, g.
Braids	*2	a, c, d.
Bran (see pages 115, 150 and 153)	Flour, Bran and Pollard	a.
Brands, iron	2	b, d, g.
Brandy, same as Spirits		
Brass—		
„ Burnished, Rods, Sheet or Tubing	2	d, g.
„ Ingots to Victorian Country Foundries	*C	g.
„ „ Minimum 5 tons per four-wheeled truck	*B	
„ Scrap	A	a, d.
Bread	1	d.
„ Improvers, including Nymalt, Arcady, Effrontine, Bakerine, Doughrine, Dry Gluten, Gluten flour, Malt flour, Trusol flour	1	a, c, d.
Breakfast Foods, such as— All Bran, Corn Flakes, Crispies, *Granose Biscuits, Grape Nuts, Kornies, Vita Brits, and Weeties in packets	1	c, d.
Bren Gun Carriers, (same as Vehicles)		
„ „ „ reconstructed for use as tractors and so consigned ...	B	
Bricks—		d, g.
„ Air, metal	2	
„ Glass	C	
„ Enamelled and camber	A	
„ Cindcrete or Concrete	M	
„ Fire, Fire Clay, and Fire Lumps—n.o.s.	A	
„ „ „ „ (see ai)	A, less 25 p.c.	
„ „ „ Produce of Commonwealth, on Up journey ...	*M	
„ Ordinary building (see page 137)	M	
„ Vitrified Acid Proof, and Special Building such as Bullnose and		
„ Facing	A	
„ „ „ „ „ (see (ai))	A, less 25 p.c.	
Brine	A	e, i.
Briquettes, Minimum 8 tons per 11 ton truck 14 tons per 16, 22 or 27 ton truck 16 tons per "QR" truck	Fire-wood plus 20%	a.
Brooms—n.o.s.	2	b, c, d.
„ Produce of Commonwealth on Up journey	B	b, c, d.
Brushware—n.o.s.	2	b, c, d, g.
„ Produce of Commonwealth, on "Up" journey	B	b, c, d, g.
Buckets, metal or wooden—n.o.s.	2	d, g.
Buckets, fruit, (see Empties, page 123)		
Buckets, mining, iron	1	
Bulldozers, new, ...	2	
„ Secondhand, (as Machinery, secondhand)		
Bungs, wooden	B	a, d.
„ Steel, "Smalls" minimum	C	a, d.
„ (sausage skins)	1	e.
Burry Waste, same as Wool		
Butchers Small Goods, viz., Sausages, Saveloys, Frankfurts, Germans, Black and White Puddings, Heads and Feet, Corned Beef, "Smalls" minimum ...	*C	d.
Butter	BB	d.
„ in refrigerator trucks under special conditions shown on page 137		
„ *Single Package Rate, see page 133*		
„ for export (see pages 188 and 189)		

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Butter on declaration from the Customs Department, returned from Melbourne to Country factories to be re-worked on account of being below standard	**	
Buttermilk (desiccated) Produce of Commonwealth	BB	c, d.
„ „ „ minimum 4 tons per four-wheeled truck...	A less 10 p.c.	c, d.
„ condensed or powdered (see page 157)	BB	c, d.
Buttons	*2	c, d.
Butts, empty (see Empties, page 127)		
C.		
(For Explosives and Goods of a Dangerous Nature see Volume 11)		
Cabinets, telephone	B	
Cable boxes, wooden	B	
„ „ or drums, iron	1	
Cables Chain	1	
„ Electric	2	
„ „ minimum 3 tons per four-wheeled truck	1	
„ iron or steel, second-hand	C	
„ „ (see (aj))	B	
Cages, bird, packed	2 plus 50 p.c.	d, g.
„ „ loose, at O.R. only	Double 2	
„ mining	1	
Cake	2	c, d.
Cakemix	1	c, d.
Calcium Chloride—n.o.s.	1	i.
„ „ (see (af))	C	i.
„ „ forwarded to Butter Factories for refrigerating purposes and so declared on the consignment note	A	
Calico	*2	b, c, d.
Camp Equipment	2	b, d.
Candles	1	c, d.
Canes (in bundles, **C.R.**)	*2	
Caneite (see Wallboards)		
Canoes (see page 131)		
Cantilevers, iron	2	
Canvas	2	b, c, d.
„ cut to approved sizes for agricultural machines and so consigned ...	1	b, c, d.
Caps, Tin, for covering bottles and pickle jars. Minimum 2s. 0d. ...	C	c, d, g.
Capsules	2	c, d, g.
Carbolized Sanitary Sawdust	A	a.
Cardboard, n.o.s. (same as pulpboard)		
„ strips for packing fruit so declared on consignment note ...	B	b.
Card Flyings }	A	a, b.
„ Fettlings }		
Carpeting	2	b, d.
Carrots	Grain	a, d.
„ small quantities, "Smalls" minimum	A, less 10 p.c.	a, d.
Cartons	2	d. g.
„ Cardboard, collapsed, "Smalls" minimum	C	d, g.
„ Cardboard, containing small cylindrical cardboard containers, with metal top and base	2	d, g.
Cartridge Cases (empty)	C	

** Half classification rates on the backward journey.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Carts, Farm, Water, same as agriculture implements and machines		
Casein—		
„ n.o.s.	BB	a.
„ Product of Victorian Country Industry consigned direct from the place of manufacture to the seaboard for immediate export beyond the Commonwealth (see (ad))	BB, less 25 p.c.	a.
„ Sheets	2	d, g.
Castings and Forgings, rough and not machined or polished or light and fragile, including stamper heads, shoes and cams, hammered shafts, stamper boxes and fire blocks, bars and bearers, cast-iron balls for grinding purposes, sash weights	*C	d, g.
„ „ as above (see (ai))	*C, less 20 p.c.	d, g.
„ „ machined and polished, or light and fragile ...	*2	d, g.
„ Scrap	*A	d, g.
Ceilings, Plaster (see Plaster)		
„ and Walls, metal, packed	*1	d, g.
„ „ loose, at O.R. only...	*2	
Celery, same as vegetables (see page 138)		
Cement	*B	a.
„ (see (ai))	*A	a.
„ (see page 138) ...		
„ Ex Fyansford (see page 152)		
„ Pipes (see Pipes)		
Cementone	1	a.
Cement and Concrete Articles as under (see note page 40)		d, g.
All cement and concrete articles must be declared on consignment note to have been manufactured at least 5 weeks.		
‡Air Ventilators	C	
‡Bases for Bowser Pumps	A	
‡Baths and Basins, Terrazzo		
n.o.s.	2	
Produce of Commonwealth	*1	
‡Blocks for building purposes	*M	
‡Cell concrete	B	
‡Columns concrete and Terrazzo, including caps and bases	A	
‡Conduit Blocks	A	
‡Fireplaces	A	
‡Garden Seat Ends	A	
‡Inverts, Channelling, Guttering, Plates, Slabs, Curbs, Manhole covers...	M	
‡Kerbings, Terrazzo	A	
‡Panels, posts, grooved, struts, with detached caps and stumps ...	A	
„ „ „ „ (see (af)) ...	A, less 25 p.c.	
Pedestals, Terrazzo cement	*2	
‡Posts, bridge	1	
‡ „ mile	A	
‡ „ fencing	A	
„ „ (see (ai))	A, less 25 p.c.	
‡Posts, Verandah	A	
‡Telegraph Poles (see (ai))	A	
‡Rings, Gully	A	
‡Rollers, Garden	2	
‡Sluice Gates and Inspection Shafts	2	
‡Copper Boiler Stands	B	
‡Box Culverts and Gutter Crossings	*A	
„ „ „ (see (aa))	*A, less 25 p.c.	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Cement and Concrete Articles—*continued*		d. g.
‡Chimneys in sections	A	
„ „ (see (ai))	A, less 25 p.c.	
‡Survey Cover Box Lids and Tablet Base Blocks	A	
Sinks, Terrazzo	*2	
‡Slabs, Terrazzo	A	
Troughs—		
„ Washing—		
„ n.o.s.	*B	
„ Terrazzo top	*B	
„ All Terrazzo	*2	
„ Sheep, pig, cattle or horse	*A	
Pipes, concrete, and collars (see pipes page 62)		
Tanks	*B	
„ (see (ae))	*A	
Window Frames	A	
Chaff (see (ak))	Grain	a.
Chains—n.o.s.	2	d.
„ used in the manufacture of Agricultural Implements	1	d.
„ and Hames (see (ai))	1	d.
Chairs on wheels—Invalid, Barbers', or Dentists'	2	d, g.
„ Invalid (motorised)	2, plus 25 p.c.	d, g.
„ rail	C	a.
„ „ (see (ai))	B	a.
„ n.o.s., (same as furniture)		
Chalk	*A	a, c, d.
Channels, Iron (see Iron and Steel)		
Charcoal, animal (charred bones)		
„ „ „ n.o.s.	1	a.
„ „ „ (see (ai))	B	a.
Cheese—in cases or crates—		
„ minimum 1 ton	BB	d, g.
„ "Smalls" minimum	B plus ⅛	Maximum rate per ton 144/4
„ loose at O.R. only "Smalls" minimum	B plus ⅛	
„ in refrigerator trucks under Special Conditions (see page 137)		
„ for export (see pages 188 and 189)		
„ Single Package Rate, see page 133		
„ in jars or tins, n.o.s.	2	c, d.
„ „ „ Produce of Commonwealth, "Smalls" minimum ...	B plus ⅛	
„ Binding	2	b, d.
Chemicals, Non dangerous	2	c, d, i, j, k.
Chicory Roots (see (af))	Grain	a.
„ „ dried	A	a.
„ manufactured	2	c, d.
Chimneys, Concrete (see Cement and Concrete articles)		
„ Pots, Earthenware	C	d, g.
„ Tops, iron	2	d, g.
Chinaware—n.o.s., subject to General Condition No. 2	2	d, f, g.
Chloride of Lime (same as Lime)		
„ Potassium	A	a.

‡ These articles may be accepted at Commissioners' risk when not less than 3 inches thick (kerbings not less than 6 inches in breadth and depth, columns and bases to be separate, and not less than 6 inches in thickness)

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Chocolate	2	c, d.
Churns	1	d, g.
Chutney (in bottles)—		c, d.
„ n.o.s.	2	
„ Produce of Commonwealth—on "UP" journey	*A	
„ „ consigned direct to seaboard for immediate export beyond the State (see page 148)	*AP	
Cider & Perry, n.o.s.	C	d, e, i.
„ Produce of Commonwealth, same as Wine		
„ Single Package Rate, (see page 133)		
Cigars and Cigarettes	2	c, d.
Cinnamon	2	c, d.
Circus Material (including Wild Animals, such as Elephants, Lions and Tigers), Merry-go-rounds, and Switchback Railways (see page 138)		
Per Truck — Rate Per mile s. d.		
4-wheeled ... ... 3 4 } Minimum charge £5-7-6		
QR ... ... 6 8 }		
Q, E, or S ... ... 10 0 }		
Citrus Health Saline Powder (in bottles, tins or jars)		c, d.
„ „ „ n.o.s.	2	
„ „ „ Produce of Commonwealth on "Up" journey	*A	
Clamps, iron	1	a.
Clay	M	a.
„ Fire (see Bricks)		
„ Pipe, crude	M	a.
Cleansers, Non dangerous		
„ Household	1	c, d.
„ Dairy	B	c, d.
Clippings, Waste fron Knitting Mills (see Waste Wool)		
Clocks, same as furniture and subject to General Condition No. 2		
Cloth, Woollen (see Blankets)		
Clothes Lines, Hempen or Wire	1	b, d.
„ Pegs, Wooden or Wire	*1	c, d.
Clothing, manufactured		b, c, d.
„ n.o.s.	2	
„ Produce of Commonwealth	1	
„ „ „ „ on "Up" journey, "Smalls" minimum	*C	
„ Oiled	2	
Cloves	2	c, d.
Coal, black, brown, and lignite (see page 116)	Coal	a.
Cocoa	2	c, d.
„ Bean Husks (see (ab))	AP	a.
„ and Milk		
„ „ n.o.s.	1	c, d.
„ „ Produce of Commonwealth	BB	c, d.
Cocoanut, desiccated	1	c, d, f.
Cocoanuts (same as Fresh Fruit)		
Coffee, ...	2	c, d.
„ Essence		
„ n.o.s.	2	c, d.
„ Produce of Commonwealth, on "Up" journey	*1	c, d.
Coffins in felt lined hessian or canvas covers, C.R.	*2	d, g.
Coin, Gold or Silver (see General Condition No. 20)		

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Coke minimum 6½ tons per 11 ton truck and 10½ tons per 16, 22 or 27 truck	M	a
Colas, Colbion, Colfix (See Bitumen Emulsions)		
Collars and fittings, iron or steel, for concrete-lined steel pipes or concrete pipes	B	
" " " " " (see (ah)) ...	A	
Columns, Iron (see Iron and Steel)		
Commercial Travellers' Samples (see Passenger Fares Book)		
Concrete Articles (see Cement and Concrete Articles)		
Conduit metal for electric wires not exceeding 17 ft. 6 in. in length ...	1	d, g
Conduits, four-way earthenware, with bitumen lined ends	C	g
" " " minimum 5 tons per 4 wheeled truck ...	B	g
Confectionery Minimum 2 tons	*1	c, d, g
" Smaller lots	*2	c, d, g
Construction Material (see page 149)		
Containers (waxed paper)	2	c, d
Contractors' plant, secondhand, used in building and road construction—not including vehicles, but including iron tubular scaffolding	C	
" " " " (see (ab))	B	
Copha Butter, (same as Margarine)		
Copi—		
" n.o.s.	AP	a
" for manure and so declared on consignment note (see page 111) ...	Manure	a
Copper—		
" Nails, Rivets, Plate or *Sheet	2	d, f, g, i
" Rod	2	b, d, g
" Regulus	M	a.
" Ingot to Victorian Country Foundries	*C	d, f, g
" " " " minimum 5 tons per 4 wheeled truck	*B	d, f, g
" " n.o.s. and Scrap	1	d, f, g
" " " " " (see (ai))	C	d, f, g
" Tubing and piping	2	b, d, g
" Carbonate, consigned for treatment of seed wheat or vines ...	A	a, d, f
Copperas	1	a, d, f
Coppers (see Boilers)		
Copra	B	b
Corbital	C	c, d, i
" (see (ai))	B	c, d, i
Cordials—		
" Fruit Juice, see page 47		
" Other than Fruit Juice		
" " " In bottles n.o.s.	2	c, d, e, g, k
" " " Produce of Commonwealth on "Up" journey ...	*1	c, d, e, g, k
" " " In barrels or in jars	1	d, e, g, k
Core Gum, (same as Dextrine)		
Coring (see Stone)		
Cork—Granulated—		
" " n.o.s., "Smalls" minimum	C	a, b, d, g
" " In hydraulically-pressed bales	A	
" " Pressed into sheets made in Commonwealth ...	C	d, g
" Sheets, n.o.s.	2	d, g
" Shavings, "Smalls" minimum	C	a, b, d
Corks, cut	2	a, d
Corn Flour and Cornina—		
" " n.o.s.	2	a, c, d
" " In bags	A	a, c, d
Cornsacks (see Bags)		
Cornices, Plaster (see Plaster)		
Cornish Stone, ground, and ground Flint, for Pottery making	A	a

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Corzol	C	c, d, i
„ See (ai)	B	c, d, i
Cotton—		
„ Clippings or waste	A	a, b
„ Raw, n.o.s.	C	a, b
„ „ consigned to Victorian country knitting or woollen mills or cotton factories	*B	a, b
„ Wool	2	c, d
„ Yarn, n.o.s.	*C	b, d
„ „ consigned to Victorian country knitting or woollen *mills, cotton factories, or rope and twine works*	*B	*b, d*
„ Sewing	*2	c, d
Cotton Goods—		
„ n.o.s.	2	b, c, d
„ Produce of Commonwealth	1	b, c, d
„ Produce of Commonwealth on "Up" journey, "Smalls" minimum	*C	b, c, d
Crab Winches (see Machinery)		
Cranes, Derrick	C	
„ *Gantry*	2	
Crates (see Empties, page 124)		
Cream (see page 129)		
Cream of Tartar	2	c, d, f
Creosote	A	i
Crockeryware	2	d, f, g
Crowbars in bundles, C.R.	1	
Crucibles (see Retorts)		
Culverts, Corrugated Iron, —		
„ „ „ *n.o.s.*	1	g
„ „ „ consigned to the country for Victorian Government Bodies, Municipalities and Public and Water Sewerage Trusts for Water Supply and Sewerage	B	g
Curds—		
„ Minimum one ton	BB	c, d, f,
„ smaller quantities "Smalls" Minimum	B plus ½	
Currants, dried (see Dried Fruit)		
Curry Powder *n.o.s.*	2	c, d, g
„ „ Produce of Commonwealth on "Up" journey	*1	c, d, g
Cutlery	2	c, d
Cylinders, engine	*2	d, g
„ empty (see Empties, page 125)		

D.

(For Explosives and Goods of a Dangerous Nature see Vol. II.)

Dags, sheep (see af)	M	a
Dampcourse	B	a
Dates	2	c, d
Dextrine—		
„ in bottles, jars or tins	2	c, d
„ in bulk in bags	B	a
„ „ (see ai)	A	a
Disc plates	*1	g
Disinfectant Fluids, Non dangerous—		d, i, k, l
„ „ n.o.s.	1	
„ „ Produce of Commonwealth on "Up" journey, "Smalls" Minimum	*C	
Dolomite (see Manures)	M	a
Door Frames	2	g

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Doors—		
„ Wire (when protected by timber, **C.R.**)	2	
„ „ not protected, **O.R.** only	2	
„ Furnace	2	d, g
„ Glazed (when glass is protected by wood not less than ½ inch thick, **C.R.**)	2	
„ Wooden, unglazed (when raised mouldings are protected, **C.R.**)	2	
„ Hardwood (see page 149.)		
Dowels	C	a, b
Drapery	2	b, c, d
Dregg or Draff dried (barley refuse) (see (aj))	Grain	a
Drills, Diamond and Rock (see Machinery)		
Drinks, Soft, Aerated and Temperance	*B	d, e, g, k
„ (see (aa))...	*A	d, e, g, k
Dripping	B, less 10 p.c.	d, e, g, j
Drugs, non-poisonous, n.o.s.	2	c, d, g
Duck	*2	b, c, d
Drums (see Empties, page 127)		
Durabestos Sheets (see Plaster)		
Durite Sheets (see Plaster)		
Dust (Foundry Sweepings)	M	a
Dyes	*C	d, g
Dynamos	2	d, g
E.		
(For Explosives and Goods of a Dangerous Nature see Vol. II.)		
Earth	M	a
„ Diatomaceous	M	a
Earthenware (see page 158)		
Earthen and Stone Ware	*2	d, f, g
Egg Fillers and Carriers, minimum 2s. 0d.	C	c, d
„ Preservatives Liquid		
„ „ „ n.o.s.	C	c, d, e, i
„ „ Produce of Commonwealth on "Up" journey	*A	c, d, e, i
„ § Pulp, "Smalls", Minimum	BB	d, e
§ Eggs, in cases packed in fillers and (cups) flats equal in strength to .035 leather-board, tops and bottoms to be buffered with straw or wood wool—"Smalls" minimum	BB	
„ (Single Package Rate, see page 133.)		
„ Preserved, "Smalls" minimum	BB	
„ for export, loaded in Refrigerator "T" trucks—		
„ Minimum, 4 tons per 12-ton "T" truck or 6 tons per 15-ton "T" truck (see pages 188 and 189)...	BB	
Egg Fruit, same as vegetables		
Elevators	2	
Emoleum (see Bitumen Emulsions)		
Empty Containers n.o.s. (see Empties, page 122)		
Engines, New and Second-hand (Second-hand to be subject to inspection by Departmental Engineer at cost of sender)		d, g
„ n.o.s.	2	
„ Gas, Oil or Steam, stationary	2	
„ Motor (see pages 157 and 158)	*2	
„ Engines as above in truck loads (see (af))	1	
„ Donkey	C	
„ Locomotives	C	
„ Oil or Steam, portable	C	
„ Traction	C	
„ Second-hand (see Machinery)		

§ Eggs, other than for export and Egg Pulp in Refrigerator trucks shall be charged Tariff Rates plus 25% subject to a minimum of the full weight carrying capacity of the truck used.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Engravings, Paintings, and Pictures, subject to General Condition No. 2		
„ „ „ Packed	2	d
„ „ „ *Loose at* **O.R.** *only* ...	*Double* 2	
Envelopes, straw, for bottles n.o.s.	1	b
„ „ „ Produce of Commonwealth on "Up" journey	*1	b
„ „ „ „ *(see (ac)*	*B	b
„ „ „ Returned empty "Smalls" minimum ...	A	b
Essence, Culinary in bottles or jars	2	c, d, g, k
Excavators (see Machinery)		
F.		
(For Explosives and Goods of a Dangerous Nature, see Vol. II.)		
Fasteners, steel	1	c, d
Fat, n.o.s.	B	d, e, g, j
„ Copha, n.o.s.	C	d, e, g, j
„ „ Produce of Commonwealth	B less 10 p.c.	d, e, g, j
Feathers	2	a, b, c, d
Felloes and Spokes, wooden—		a, b
„ „ „ Dressed	*C	
„ „ „ „ (see (ai))	*B	
„ „ „ Rough	*M	
Felspar	M	a
Felt for upholstering	*2	b, c, d
Felt, Insulating	1	b, c, d
„ Roofing	C	b, c, d
„ Paper for packing furniture or laying under linoleum	C	b
Fenders and Fireirons (same as furniture)		
Fibre—		
„ Basaltic	C	a, b
„ Coir	*A	a, b
„ Cocoanut, n.o.s.	*1	a, b
„ „ *when declared on consignment note to be for use in the* manufacture of Fibrous Plaster	*A	a, b
„ Flax	*A	a, b
„ Millet	A	a, b
„ Silk, Subject to General Condition No. 2	*2	d
„ Vegetable	1	b
„ Woollen, teased	C	b
Fibreboard	1	d, g
„ (see (af))	C	d, g
Fibro-cement or Plaster Sheets (see Plaster)		
Field Bins on iron skids	2	g
Films, Cinematograph, non-dangerous	2	c, d
Finings, beer	*1	c, d, e, g, i
„ „ minimum 5 tons per 4-wheeled truck	*C	c, d, e, g, i
Fire Brigade equipment (see page 155)		
Firearms, n.o.s.	2	d
Fire Beaters	1	b, d
„ Blocks, bars, and bearers (see Castings)		
„ Bricks, Fireclay and Fire Lumps (see Bricks)		
Firewood (see page 117)	Fire-wood	a
Fish—		
„ Dried, salted and in brine n.o.s.	2	d, e
„ Fresh, smoked, salted or dried, Produce of Commonwealth, also oysters and other shell fish (see page 128)		d

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Fish—*continued.*		
„ Preserved n.o.s. ...	2	d
„ „ „ (see (af)) ...	C	d
„ „ Barracouta, Produce of Commonwealth ...	B	d
„ „ „ „ „ (see (af)) ...	A	d
Fittings—		
„ Door ...	2	c, d
„ Electric Light or Gas ...	2	c, d
„ Shop and Counter ...	2	d, g
Flagging (see Stone)		
Flannels (see Blankets)		
Flavour for Cordials—		
„ „ in Casks ...	1	e
„ „ in Jars ...	2	d, g, k
Flax Refuse (see (ab)) ...	Grain	a, b
Flock ...	*2	a, b
„ dust (see (ab)) ...	M	a, b
Flour—		
„ White and Brown (see pages 115, 150 and 151) ...	Flour, Bran and Pollard	a
„ Self-raising ...	A	a, c, d
„ „ in jute or calico bags (see pages 115 and 151) ...	Flour, Bran and Pollard	a
„ „ „ „ „ in less than truck loads... ...	A	a, c, d
Flowers, cut ...	1	c, d, g
Fluid, washing, including solvent T, for woollen mills ...	*2	e, i
Fluorescent Tubes ...	2	c, d
Fluorspar ...	M	a
Fodder, compressed (see (aj) (see page 153) ...	Grain	
Food—		
„ Breakfast (see page 37).		
„ Pets, for Cats and Dogs in tins or cartons ...	2	c, d
„ Stock, Poultry, and Animal ...	AP	a, c, d
„ Infants, n.o.s. ...	2	c, d
Foot Rot Specifics, n.o.s. ...	1	c, d
Forges, Portable ...	2	
Forgings (see Castings)		
Forks, Hay and Garden ...	2	b, d, g
Formaldehyde } (same as Disinfectant Fluids) (see page 151)		
Formalin }		
Frames—		
„ Picture (Packed) ...	2	d, g
„ Loose at **O.R.** only ...	Double 2	
„ Window, glazed (protected by timber, **C.R.**) ...	2	
„ Unglazed, (protected by timber, **C.R.**)	2	
„ Chair, Couch, and Settee, set up and partly polished ...	*2	h
Fruit Bottling Outfits ...	1	d, g
Fruit—		
„ Canned, bottled, or preserved, in tins, or in bottles or jars ...		c, d, g
„ n.o.s. ...	2	
„ Produce of Commonwealth on "Up" journey ...	*A	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Fruit, Canned—*continued.*		
„ Produce of Commonwealth, consigned direct to the seaboard for immediate export beyond the State (see page 148)	*AP	
„ Product of Victorian Industry exported by rail beyond the State (see page 152)		
„ Dried—		d
„ „ n.o.s.	*2	
„ „ Produce of Commonwealth on "Up" journey ...	*A, less 10 p.c.	
„ „ Produce of Commonwealth, forwarded to distilleries for distillation (see page 152)		
„ „ For export (see page 151)		
„ „ Produce of Commonwealth, returned to the original forwarding station for distillation purposes on production of declaration	*Half A, less 10 p.c.	
„ „ Refuse (see (ae))	A, less 25 p.c.	
„ „ Mixture, on "Up" journey	*A	
„ „ Single Package Rate (see page 133)		
„ §Fresh—		
„ „ In lidded or unlidded cases or in bags, "Smalls" minimum	AA	d
„ „ In trays or punnets "Smalls" minimum	AA	d, g
„ „ In buckets, casks, tubs, or tins, "Smalls" minimum at **O.R.** only	B, less 10 p.c.	
„ „ Consigned direct to the seaboard for immediate export beyond the Commonwealth (see pages 148, 152, 188 and 189)	** Grain	d
Single Package Rate (See page 133)		
Fruit Juice or Fruit Juice Cordial manufactured from fruit grown in the Commonwealth		d, e, g, i, k
„ n.o.s., "Smalls" minimum	C	
„ On "Up" journey (see page 152)	*A	
„ Consigned direct to the seaboard for export beyond the State (see page 148)	*AP	
„ Product of Victorian Industry exported by rail beyond the State (see page 152)		
Fruit and Tomato Pulp—		d, i
„ „ „ "Smalls" minimum	A	
„ „ „ In tins in cases (see (ai))	AA	
„ „ „ In tins uncased, minimum 8½ tons per 11 ton truck ; 11 tons per 16, 22 or 27 ton truck ...	AA	
„ „ „ Consigned direct to the seaboard for immediate export beyond the State (see pages 148 and 152)	*AP	
Fruit stones "Small" minimum	A	a,
„ „ see (af)	M	a,
Fuller's Earth—		c, d, f, j
„ „ n.o.s.	2	
„ „ For scouring purposes	C	
„ „ Produce of Commonwealth on "Up" journey ...	*A	
Furnaces, Tobacco—		
„ „ n.o.s.	2	
„ „ Produce of Commonwealth	1	

§ Fruit, fresh, in Refrigerator trucks, shall be charged at Tariff rates plus 25 per cent., subject to a minimum of the full weight carrying capacity of the truck used.

** May be accepted in cases at Commissioners' Risk on payment of 5 per cent. additional.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Furniture, New or Secondhand—	*2	h
„ A packing charge of £1/10/10 per ton (minimum 3/2 per consignment) is to be charged in addition to the Tariff rate (District rates shown on page 163 excepted) on any furniture not packed in accordance with the Commissioners' specifications (see page 28)		
(a) On furniture loaded by the Department		
(b) On furniture loaded by carriers or others the packing charge calculated on the weight of the furniture shall be imposed for the use of Departmental packing materials ...		
„ In van bodies or sling vans, minimum 30 cwt	1	
„ In van bodies on wheels minimum 2 tons	2	
„ Wickerwork, seagrass or cane, (plus the packing charge shown above for furniture when not protected by packing) ...	Double 2	
„ Furniture Van and Sling Vans without wheels, returned empty or otherwise	1	
Furs, subject to General Condition No. 2	2	d
G.		
(For Explosives and Goods of a Dangerous Nature see Vol. II.)		
Gambier	A	a
Game, Dead		d
„ (C.R.)	1, plus 10 p.c.	
„ (O.R.)	1	
Gangboards, Wooden	B	
Garages—See Houses		
Garlic	A	a, d
Gas Manufacturing Plant	2	d, g
Gas Liquor, in steel drums	M	i
Gates, Iron* or Wood	2	
Gelatine	*2	c, d, e, i
Ginger—		
„ Dry or Preserved	2	a, c, d, e, f
„ Green, "Smalls" minimum	A, less 25 p.c.	c, d, f
Girders, Iron—(see Iron and Steel)		
Glass—		
„ Broken (see (af))	M	d
„ Pavement and Floor lights	1	d, g
„ Plate or Window, and Glassware, subject to General Condition No. 2	*2	d, g
„ Window in cases to be used in the erection of hot-houses and covering tomato plants (see af))	C	
Glaxo—		
„ n.o.s.	2	c, d
„ Produce of Commonwealth on "Up" journey	BB	c, d
„ Product of Victorian country industry consigned direct to the seaboard for immediate export beyond the Commonwealth (see (ad) and page 148)	BB, less 25 p.c.	c, d
Gliders (see Aeroplanes)		
Glucose or Maize Syrup	*C	c, d, e, i
„ „ „ in 3-ton lots	*B, plus 25 p.c.	c, d, e, i
Glue—		
„ n.o.s.	1	a, d, e, i
„ Produce of Commonwealth, on "Up" journey	C	a, d, e, i

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Glue—*continued.*		
„ When sent with 1-ton or more of other goods from one consignor at one station to the same consignee at destination station, minimum 10 cwt.	C	a, d, e, i
„ When loaded in same truck with not less than— 2 tons of leather / 10 cwt. of leather { same as leather P. of C.		
„ Pieces	1	a, b, c, d
Glycerine—Crude	1	c, d, e, i
„ Pure, in bottles or jars	2	c, d
Gold, manufactured or unmanufactured (see General Condition No. 20)		
Golden Syrup	1	c, d, e,
Go-carts—		
„ Packed	2	d, g
„ Loose, at **O.R.** only	Double 2	
Goloshes	2	c, d
Goods, n.o.s.	2	
Grain, n.o.s. (see pages 152 and 153)	Grain	a
Grains, Brewers' (see (af) and page 156)	Grain	a
„ „ If loose to be charged truck load minimum		
Grain Bin Trailers on wheels	Vehicle rate	
Granite (see Stone)		
Grape Skins, crushed (see (af))	M	e
Grape Spirit, Produce of Commonwealth (certificate from Customs Department to accompany consignment note)	*B	e, i
Graphite, n.o.s.	1	c, d, f, j
„ crude	A	c, d, f, j
„ „ (see (ai))	A, less 25 p.c.	c, d, f, j
Grass and Rushes, for rope and broom making (see (ab))	A.P.	a, b
Grass Rope, twisted (see (aa))	A.P.	a, b
Grates	2	d, g
Gratings—		
„ Wooden	A	d, g
„ Iron	2	d, g
Gravel (see page 154)	M	a
Grave Railings, iron, in bundles, **C.R.**	2	
Grease—		
„ Axle and Anti-friction	C	c, d, i
„ Curriers', for tanning	A	c, d, i
Grindstones	1	d, g
Grit, Steel, for dressing stone	M	c, d, f, j
Groceries, n.o.s.	2	c, d
Gum—		
„ n.o.s.	2	c, d, e, i
„ Dry, wattle	1	a, d
„ Yacca	M	
Gun trailers	1	
Gunmetal—		
„ n.o.s.	2	d
„ Scrap	*A	d
Guns—		
„ Sporting	2	d
„ Small arms	2	d
„ Heavy and Field, including Limbers	1	
Guttering, galvanized iron, or Asbestos Cement	2	g
Gypsum	A.P.	a
„ For manure, and so declared on consignment note (see page 111)	Manure	a
„ Building Blocks (see (ad))	M	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
H.		
(For Explosives and Goods of a Dangerous Nature see Vol. II.)		
Hair—		
„ Builders', and n.o.s.	2	a, b
„ Pigs'	1	a, b
„ Upholsterers'	2	a, b
„ Tanners', waste (see af))	M	a
§Hams	*B less 10 p.c.	a
„ For export (see page 155)		
„ Single package rate (see page 133) ...		
Hames and Chains	2	b, d, g
„ „ (see (ai))	1	b, d, g
Hand-carts	2	
Handles—		
„ Wooden, n.o.s.	1	b. d
„ Hardwood, Produce of Commonwealth, on "Up" journey ...	*A	b, d
„ Broom, hardwood, product of Victorian Country Industry, on "Up" journey—minimum 2 tons	A, less 33⅓ p.c.	b, d
„ „ Smaller quantities, "Smalls" minimum ...	B, less 17 p.c.	b, d
Hardware, n.o.s.	2	d, g
Hares—(same as Rabbits)		
Harness, n.o.s.	2	d
Hats and Millinery		
„ Felt, untrimmed, including berets, men's felt hats and caps, and straw (pandan)	2	c, d
„ n.o.s.	2, plus 50 p.c.	c, d
Hay, in sheaves or bales (see (ag))	Grain	
Hemp	*A	b
Herbs	2	a, c, d
Hessian—		
„ n.o.s.	*2	b
„ declared on consignment note to be for use in packing blankets and woollens to be forwarded by rail	B	b
„ for covering wheat or salt stacks or fruit-drying racks or for wrapping around tobacco forwarded by rail	B	b
Hides—		
„ n.o.s.	B	
„ for country tanneries in Victoria—		
minimum 2 tons	*A.P.	
minimum 10 cwt.	*A, less 25 p.c.	
smaller lots, "Smalls" minimum	*B	
NOTE.—When loose hides are required to be tallied, an extra charge of 4s. 0d. per ton shall be made in addition to Commissioners' Risk rate.		
„ waste product of, for glue making, when forwarded with leather or basils, Produce of the Commonwealth, on the "Up" journey to be charged same rates and conditions.		
„ pieces, waste product, consigned for making glue (see (af)) ...	M	
Hinges	2	c, d
Hoes, Garden	2	b, d, g
Hollowware, n.o.s.	2	c, d, g
Honey—		
n.o.s., minimum 10 cwt.	B. less 10 p.c.	d, g, i
„ smaller quantities, "Smalls" minimum	C, less 10 p.c.	

§ Hams in Refrigerator trucks shall be charged Tariff rates plus 25 per cent., subject to a minimum of the full weight carrying capacity of the truck used.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Honey—*continued.*		
„ Produce of Commonwealth on "Up" journey— minimum 10 cwt.	*A, less 10 p.c.	
„ „ smaller quantities, "Smalls" minimum	*B, less 10 p.c.	
„ Single Package Rate, (see page 133)		
„ For export (see pages 188 and 189)		
Hooks, Reaping	2	b, d, g
Hoops, Cheese	1	d, g
Hops (see page 155)		a, b
„ loaded to full cubical capacity of trucks utilized	C, less 25 p.c.	
„ minimum 10 cwt.	C	
„ smaller quantities	1	
Horns and Hoofs	B	a
„ „ (see (ab))	A	a
Horseshoes	1	a, d
„ see (ai))	C	a, d
Hose—		
„ Canvas	2	b
„ Reels (see page 155)		
Hosiery, same as Clothing		
Houses or Offices—		
„ „ Wooden, whole, or in packages	B	
Houses, Sheds, Sleepouts, Huts or Garages, Prefabricated in sections or of galvanised iron, fibro cement, fibrolite or similar material and including doors, windows and iron angle frames	C	
Hurdles, wooden or iron	1	
Huts (same as Houses)		
I.		
(For Explosives and Goods of a Dangerous Nature see Volumn II.)		
Ice, in waterproof cases or sacks, "Smalls" minimum	B	
„ (see (aa))	A	
„ dry (carba), in cartons in insulated containers	B	
„ (see page 155)		
„ Chests, (same as furniture)		d, g
Ice Cream—		
„ n.o.s., "Smalls" minimum	B	
„ in Refrigerator trucks—		
„ Minimum 7 tons per 12-ton and 10-tons per 15-ton truck ...	B	
Ink	2	c, d, e, i
Insecticide, non-dangerous	2	c, d e, f, i, j
Instruments, Musical, Optical, Scientific, Surgical, and n.o.s.	2	d
Insulators—		
n.o.s.	2	d, g
for telegraph or electric light poles	C	d, g
„ „ „ „ (see (ac))	A	d, g
Insulwool	C	c, d
Iron and Steel—		
„ „ Angle or Plate, drilled, ready for bolting together ...	1	
„ „ „ „ „ (see (ai)) ...	C	
„ „ Bar, Rod, Plate and Angle, not prepared in any way ...	*C	
„ „ „ „ „ (see (ai)) ...	*B	
„ „ Billets and Blooms (same as Bar, Rod, &c.)		
„ „ Channels, n.o.s.	1	
„ „ „ „ (see (ai))	C	
„ „ „ to Victorian Country Foundries	*C	
„ „ „ „ „ (see (ai))	*B	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Iron and Steel— *continued.*		
,, ,, Columns		
Cast or wrought	2	
Rolled "H" steel with riveted attachments	1	
,, ,, Corrugated galvanised or black—		
,, ,, ,, ,, Packed	*C	d, g
,, ,, ,, ,, ,, 20-ton lots	*C, less 10 p.c.	d, g
,, ,, ,, ,, Loose **O.R.** only	*1	
,, ,, ,, ,, Secondhand, loose—		
minimum 3 tons per 4-wheeled truck	C	
smaller lots	1	
,, ,, Girders	2	
,, (see (ai))	1	
,, ,, Hoop	*C	
,, ,, ,, (see (ai))	*B	
,, ,, ,, used in the manufacture of hogsheads, cream and milk cans, on declaration	A	
,, ,, Joists, n.o.s.	1	
,, ,, ,, ,, (see (ai))	C	
,, ,, ,, To Victorian Country Foundries	*C	
,, ,, ,, ,, ,, ,, ,, (see (ai)) ...	*B	
,, ,, Magnet	M	
,, ,, Pig	*M	
,, ,, Reinforcements, fabricated	*C	
,, ,, ,, in rolls minimum 6½ tons per 16, 22 or 27 ton truck	*C less 20 p.c.	
,, ,, ,, n.o.s. (see (ab))	*C less 20 p.c.	
,, ,, Scrap	*M	
,, ,, Sheet, black, tinned or galvanized, packed	*C	d, g
,, ,, Shafting	*2	d, g
,, ,, Trusses, roofing	2	
Ironmongery, n.o.s.	2	d, g
Isinglass	2	c, d, g
J.		
(**For Explosives and Goods of a Dangerous Nature see Vol. II.**)		
Jams and Jellies—		c, d, g
,, ,, n.o.s.	2	
,, ,, Produce of Commonwealth on "Up" journey ...	*A	
,, ,, Produce of Commonwealth consigned direct to the seaboard for immediate export beyond the State (see page 148)	*A.P.	
,, ,, Product of Victorian Industries exported by rail beyond the State (see page 152)		
Jars, glass or earthenware—		d, g
,, ,, ,, n.o.s.	B	
,, ,, ,, Confectionery, "Smalls" minimum	C	
,, ,, ,, Druggists	2	
,, ,, ,, Acid and Handled Jars or Bottles for Demijohns, Products of Victorian Country Industry on "Up" journey (see (ac) and page 158) ...	M, less 21 p.c.	
,, ,, ,, ,, n.o.s.	2	
,, ,, ,, returned Empties (see page 125)		
Jelly Crystals—		
,, ,, n.o.s.	2	c, d
,, ,, on "Up" journey	*1	c, d
Jewellery (see General Condition No. 20)		
Joinery	*2	d, g
Joists (see Iron and Steel)		
Jutex (see Wallboards)		

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
K.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Kaolin	M	a
,, pulverized and cleaned	M	a
Kapok	*2	b, d
Kerbing (see Stone)		
Kindlers, fire	1	c, d
Knife Grinders	1	d
L.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Lace, subject to General Condition No. 2, ...	*2	d
Lactofarina	A	
Lactogen—		c, d, g, f, j
,, n.o.s.	2	
,, *Produce of Commonwealth*		
On "Up" journey	BB	
On Down journey consigned direct to the seaboard for immediate export beyond the State (see page 155) ...	BB	
,, Product of Victorian country industry consigned direct to the seaboard for immediate export beyond the Commonwealth (see (ad)) (see page 148)	BB, less 25 p.c.	
Ladders, including step ladders, up to 18 feet in length, rate and half ... ... } **C.R.**	2, plus 10 p.c.	
Over 18 feet in length, double rate ... } O.R.	2	
Lamp Black	2	c, d,j
Lamps—		
,, packed	2	c, d
,, loose, at **O.R.** only	2 plus 50 p.c.	
Lampware	*2	c, d
Lanoline (crude)	B	d, e, j
Lard (see page 155)	*B less 10 p.c.	d, f
Lasts	2	d, g
Lathes, (see machinery)		
Lathing Steel, interlocking	1	
Lattice Bars, Struts and Ties, iron or steel	2	
Laundry, in hampers	2	
,, ,, when returned to the original despatching station after having been laundered, and so declared on consignment note	2 less 50 p.c.	
Lead—		
,, Ingot to Victorian Country Foundries	*C	
,, ,, ,, ,, minimum 5 tons per 4-wheeled truck	*B	
,, Pig and Ingot, n.o.s.	C	
,, Red* or White	2	d, i
,, Scrap	*A	a
,, Sheet	*1	g
,, Slag	A	a
Leather—		b, d, g
,, Fancy and Bookbinders	2	
,, Patent	*1	
,, Plain and Basils, n.o.s.	*1	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Leather—*continued.*		
„ Produce of Commonwealth on "Up" journey—		
minimum 2 tons	*A.P.	
minimum 10 cwt.	*A, less 25 pc.	
smaller lots	*B	
„ Cloth	*2	
„ Leatherboard	1	
„ „ (see (af))	C	
Leatherware	2	c, d
Legs, Chair and Table, sawn or turned in the rough, product of Victorian Country Industry, on "Up" journey	*A	b
Lemon Peel Candied —		
„ „ „ n.o.s.	2	d, f
„ „ „ Produce of Commonwealth, on "Up" journey	*A	d, f
Lemon Skins—		
„ „ in brine or brine cured, "Smalls" minimum	AA	e
„ „ in brine, consigned direct to the seaboard for immediate export beyond the State—(see page 148) minimum 7 tons per 11-ton truck and 9½ tons per 16, 22 or 27 per ton truck	Grain	e
„ „ fresh, "Smalls' minimum	AA	e
Lentils	1	a, c, d
Lime, Quick or Unslaked (see Volume II)		
Lime and Lime, Hydrate of		a
„ „ n.o.s.	M	
„ „ minimum 3 tons	A	
„ „ Smaller quantities, "Smalls" minimum (see pages 155 and 159)	B	
„ Bisulphite	1	d, g, f
„ Carbonate of (see Manures)		
„ Chloride of	1	
„ „ (see (af))	C	
„ Water	1	c, d, e, i
Limestone (see page 155)	M	a
Linerite	*C	g
Linoleum (when rolled tightly on wooden cores)	2	b
Links, Bucket, iron, for dredging plant	1	
Linotype Metal	2	a, d
„ „ Waste	A	a, d
Linseed	Grain	a
Liqueurs, n.o.s.	2	c, d
Liquor, black, by-product of wood pulping	*C	e, i
Liquorice	2	c, d
Litharge, used in assaying of gold	1	i
Live Stock (see page 204)		
Loam	M	a
Locks	2	c, d
Logwood	1	a, f
Lucerne, leafmeal, (see (af))	A.P.	a
Luggage (when in suitable receptacles **C.R.**)	2	
Lupol (see (ak))	A.P.	a
M.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Machinery and Machines as under may be accepted when new at Commissioners' Risk under the following conditions :–		
„ „ ‡In cases or crates		
„ „ §All projecting parts and light castings to be crated or removed and cased		

APLHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Machinery &c.—*continued.*		
Machinery, product of Victorian Country Industry and new, manufactured in the Commonwealth, (see page 148)		
„ shown below, classified under Classes 1 and 2 shall be charged one rate lower than the rate specified hereunder, when in truck loads, subject to minimums of 8 tons per 11-ton truck, 12 tons per 16, 22, or 27-ton truck, 16 tons per QR truck and 24 tons per Q or QB truck		
„ ‡Bone Cutters and Crushers	2	
„ §Brickmaking and Scooping	1	
„ ‡Buttermaking and Dairy-cooling plant (including Cheese Hoops and Milking Machinery, but not including Vats, Pasteurisers or Tanks)	1	
„ ‡Butterworkers and Printers	1	
„ ‡Concrete Mixers	2	
„ ‡Cream Separators	1	
„ ‡De-hydrators for Fruit Drying	2	
„ ‡Distillery Apparatus	2	
„ ‡Drilling	2	
„ §Drills, Diamond and Rock	1	
„ ‡Electrical	2	
„ §Excavators	2	
„ ‡Flour Dressing	1	
„ ‡Front End Loader	2	
„ ‡Fruit Graders	1	
„ ‡Grain Grinding (see note (a) below)	1	
„ ‡Grain Pickling	1	
„ ‡Grape Crushers	2	
„ §Grubbing	1	
„ ‡Hop Sulphurating	2	
„ ‡Irrigation and Water Supply	1	
„ §Jacks, Stump Grubbing or Lifting	1	
„ ‡Lathes	2	
„ ‡Mangles	2	
„ §Mining	1	
„ ‡n.o.s.	2	
„ §Pile Driving	C	
„ ‡Post Hole Diggers	1	
„ §Presses, Cheese	1	
„ ‡ „ Copying	2	
„ § „ Wine and Wool	2	
„ ‡Printing	2	
„ ‡Pulpers	2	
„ §Rabbit Exterminating	1	
„ Refrigerating—		
„ „ ‡Light	1	
„ „ §Heavy	1	
„ §Road Graders	1	
„ §Road Scarifiers	C	
„ ‡Sausage	2	
„ ‡Sawmilling	1	
„ Sewing, Treadle and Electric (same as Furniture)		
„ ‡Sheep Shearing	1	
„ ‡Sole Plates	*2	
„ ‡Spraying (see note (a) below)	1	
„ §Stokers, Mechanical	1	
„ §Stone Breaking	2	
„ ‡Textile, consigned to Victorian country woollen mills ...	1	
„ ‡Water Boring	1	
„ ‡Weighbridges	1	
„ ‡Wheels, Fly	*2	
„ §Winches, Crab	2	

(a) Grain Grinders and Spraying Machines and Spraying Outfits when uncrated may be accepted for carriage at Commissioners' Risk on payment of 10 per cent. additional.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Machinery, &c.—*continued.*		
„ Secondhand may be accepted at Commissioners' Risk when packed or protected as described on page 54 and subject to inspection by Departmental Engineer at cost of sender		
„ Secondhand on Declaration Form G.F. 104 that machinery has been in use for three years being lodged with consignment note	C	
„ Mining, including boilers, secondhand, subject to minimums of 8 tons per 11-ton truck, 12 tons per 16, 22 or 27-ton truck, 16 tons per QR truck, and 24 tons per Q or QB truck ...	B	
Macaroni	*2	c, d
Magnesia, Fluid	2	c, d
Magnesite	M	
Magnesium Ingot	C	
Maize	Grain	a
Maizena	2	c, d
Mallee Roots (see pages 117 and 118)	Fire-wood	
Malt and crushed Malt Grains	Grain	a
„ Combings, minimum 6 tons per 11 ton truck, 9½ tons per 16, 22 or 27-ton truck (see page 156)	Grain	a
„ Extract	2	c, d
„ (see page 151)		
Manganese	C	a
Mangel Wurzel	Grain	a
Manures (see page 111)		a
„ Ammonia, Sulphate of		
„ Bonedust		
„ Bone and Blood mixture ...		
„ Calcium Silicate Slag ...		
„ Carbonate of Lime ...		
„ Castor Meal		
„ Diammonphos		
„ Dolomite		
„ Floranid		
„ Floraphos		
„ Lawnex		
„ Lime (‡ see page 57 and 155)		
„ Lime and Hair (see (af)) ...		
„ Marl		
„ Monaphos		
„ Muriate of Potash ...		
„ Nitrate of Potash (‡see page 57)		
„ Nitrate of Lime	†Manure***	
„ Nitrate of Soda (‡see page 57)		
„ Nitro-chalk		
„ Organite		
„ Peatmoss		
„ Peat Mulch		
„ Phosphatic Rock		
„ Poultry Humoss		
„ Salt (see page 159)... ...		
„ Seaweed, Wet		
„ Stable Manure minimum 6 tons per 11-ton truck and 10 tons per 16, 22 or 27-ton truck ...		
„ Street Sweepings (see (af))		

† and *** See footnote page 57.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Manures, Artificial—*continued.*		
,, Sulphate of Potash ...	†Manure***	
,, Superphosphate		
,, Wool dust and waste from wool carbonizing (see (af))		
,, Wood Ashes (see (af)) ...		
,, Zinc Sulphate		
Mantle-pieces	2	d, g
Maps—Subject to General Condition No. 2	2	d
Marble (see Stone)		
Margarine	C	d,
Marl (see Manures)	M	a
Masonite, (see Wallboards)		
Matting and Mats	2	b, d
Mattresses*, (same as Furniture)		
Mauls, wooden	C	
Meal—		
,, in bags	A	a
,, in packets	1	c, d
,, n.o.s.	2	c, d
,, Castor (see Manures)	A.P.	a
,, for stock food, n.o.s.	A.P.	a
,, Meat	A.P.	a
,, Rice, in bags	A.P.	a
,, Velvet Oil, in bags	A.P.	a
,, Wheat, in bags (see pages 114 and 151)...	Wheat	a
Meat—		
,, Fresh, or Salted, including veal and pork, "Smalls" minimum ...	B, plus 25 p.c.	
,, Fresh in louvre trucks, minimum 5 tons	A	
,, Fresh (see page 156)		
,, Frozen or chilled also Poultry or rabbits frozen or chilled in "T" trucks minimum 8 tons per 12-ton truck and 12 tons per 15-ton truck except that the minimum for frozen meat loaded in "T" trucks numbered 343 to 392 inclusive shall be 10 tons, 5d. per ton per mile, minimum charge 74s. 0d. per 12-ton "T" truck, and 112s. 4d. per 15-ton "T" truck (see pages 156 and 188)		
,, Extract of and preserved, in bottles or jars, n.o.s.	2	c, d
,, ,, ,, Produce of Commonwealth on "Up" journey ...	*B	c, d
Meat Wrappers (see Empties, page 126)		
Medicines, n.o.s.	2	c, d
Medicinal Preparations, Non-poisonous	2	c, d
Meloine—		
,, n.o.s.	C	d, e, j
,, Minimum, 3 tons	B plus 25 p.c.	d, e, j
Melons—		
,, (see (af))	Grain	a, d, g
,, Small lots (must be in bags, crates, or cases), same as fresh fruit ...		a, d, g
Merry-go-rounds (see Circuses)		

‡ See Volume II for Conditions of Carriage.

*** The Manure rate only applies when the commodity specified is to be used solely for manuring land or in the manufacture of artificial manures and is so declared on the consignment note.

† When forwarded to the Seaboard for export, Class M rate and conditions shall be applicable.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Metalcrete for Cement floors	1	a
„ „ „ „ (see (ai))	C	a
Metal—		
„ Expanded, packed	1	b, d, g
„ „ Loose at **O.R.** only	2	
„ Scrap, n.o.s.	*A	
„ Ingot (Brass, Copper, Lead, Zinc) to Victorian Country Foundries	*C	
„ „ „ Minimum 5 tons per 4-wheeled truck ...	*B	
„ Road (see Stone)		
Meters, water and gas	2	d, g
Mexphalt (see Bitumen Emulsions)		
Mica	2	d
„ Crude, not processed in any way	B	d
„ „ Minimum 15 tons per 4-wheeled truck	A	d
Milk—		
„ Condensed and concentrated, n.o.s.	1	c, d, e, i
„ „ Produce of the Commonwealth ...	BB	c, d, e, i
„ „ „ Product of Victorian Country Industries consigned direct to the seaboard for immediate export beyond the Commonwealth (see (ad) and page 148)	BB less 25 p.c.	c. d, e, i
„ Dried (same as Milk, condensed)		
„ Fresh (see page 130)		
„ Malted, n.o.s.	2	c, d
Produce of Commonwealth on "Up" journey	BB	c, d
„ Powder, Skim (same as milk, condensed)		
„ Powder (Skim) for poultry and animal food (see page 157)		
„ Sugar of, n.o.s.	1	c, d
Produce of Commonwealth.	BB	c, d
Millet Seed (see page 160)	Grain	a
Millet Straw, for broom-making—		
„ „ in bundles, minimum 3 tons	S.A.P., less 10 p.c.	
„ „ „ „ Smaller quantities, "Smalls" minimum ...	B, less 10 p.c.	
„ „ Loose—		
Minimum 3 tons for any quantity loaded in 4-wheeled truck	S.A.P., less 10 p.c.	
Millinery (see Hats)		
Millstones—		
„ **C.R.**	2, plus 10p.c.	
„ O.R.	2	
Mineral Waters—		
„ „ n.o.s.	C	d, e, g, i
„ „ Produce of Commonwealth	*B	d, e, g, i
„ „ „ (see (aa))	*A	d, e, g, i
Mineral Wool for covering steam pipes	C	a, b
Mirrors, Packed, same as Furniture and subject to General Condition No. 2		
„ Unpacked, at O.R. only, plus the packing charge shown on page 48 for furniture not protected by packing	Double 2	
Molasses—		
„ n.o.s.	1	c, d, i
„ For cattle food on declaration (see (ae))	A.P.	c, d, i
„ For distillation purposes (see (ae))	A.P.	i
Moss, dry	1	a, b
„ green	A	a, d

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Motor Cars, Motor Chassis on wheels ... }	Vehicle Rate	
„ Bodies, and Cabs for motor trucks ... }		
„ Chassis in cases, or loose (without wheels or fittings)	*2	
„ Body parts	*2	b, d g
„ Cycles, Motor Cycle Side Cars,* or Motor Cycles with Side Car attached, Packed	2, plus 25 p.c.	d, g
„ „ Loose at O.R. only	Double 2	
„ Cycle Side Car Chassis, packed in pairs	2, plus 25 p.c.	d, g
„ Engine or Cylinder Blocks (see page 158)	2	d, g
Motor Tractors—		
„ „ n.o.s.	*B	
„ „ Attached to agricultural implements or machines ...	1	
„ „ Attached to other descriptions of Machines	2	
„ „ Parts (excluding tyres and tubes)	*1	d, g
Motor wheels without tyres	*2	d, g
Mould Boards	*1	b
Mouldings—		
„ Gilt or n.o.s.	2	d, g
„ Plaster (see Plaster)		
„ Timber (see Timber)		
Moulds, Steel, secondhand, for manufacture of concrete pipes	C	
Mowers, garden	2	d, g
‡Mushrooms, "Smalls" minimum	A	d
Mustard	2	c, d, g
Myrabolams—		a
„ consigned to country tanneries in Victoria—		
(see (aa))	*M	
minimum 10 cwt.	*A, less 25 p.c.	
„ n.o.s.	A	a
„ Extract of	1	d, e, i
N.		
(For Explosives and Goods of a Dangerous Nature see Vol. II.)		
Nails—		d, , j
„ Aluminium	1	
„ Boot, Iron or Brass	2	
„ Iron or Wire	*1	
In 20-ton lots	C, less 10 p.c.	
„ n.o.s.	2	
Naphthaline	1	d
Naves—		g
„ Turned and Mortised	*C	
„ „ „ (see (ai))	*A	
„ Turned only	*M	
„ Rough	*M	
„ Blocks	M	
Neon Tubes	2	c, d
Nescafe, } n.o.s.	2	c, d
Nescory, } Produce of Commonwealth on "Up" journey	*1	c, d
Nets, Fishing	2	b, d
New Char (Brewers Compound)	B	a

‡ The freight charges on Mushrooms must be *prepaid* and consignments fully addressed to facilitate delivery.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Nitrate of Lime (see Manures)		
Noils	*C	a, b
Nulomoline	*C	a
„ minimum 3 tons	*B, plus 25 p.c.	a
Nutmegs	2	a, c, d
Nuts, edible		
„ n.o.s.	2	a, c, d
„ Produce of Commonwealth, on "Up" journey, "Smalls" minimum	A, less 25 p.c.	a, c, d
„ Iron	*1	a, d
O.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Oars, boat	2	g
Oats	Grain	a
„ for seed (see page 160)		
„ Crushed or ground "without addition of other ingredients for stock food" and so consigned	Grain	a
„ Rolled	A	a, c, d
„ Hulled	Grain	a
„ Husks (see (ab))	Grain	a
Oatmeal	A	a, c, d
Ochre	*A	c, d
Oils— Non-dangerous as under		d, e, g, i
„ n.o.s.	2	
„ Asphaltic	C	
„ „ consigned solely for the purpose of fluxing or thinning bitumen, and so declared on the consignment note (see (af))	M	
„ Batching	*1	
„ Castor	2	
„ Chinese	2	
„ Cocoanut	*1	
„ Fish, including Cod (unrefined), Seal, and Whale	A	
„ Lubricating or Transformer	1	
„ „ „ (see (ae))	C	
„ Lubricating, spent forwarded for reconditioning	A	
„ Neatsfoot	*1	
„ Oleine	1	
„ „ for Soap making	B	
„ Olive, n.o.s.	2	
„ „ Produce of the Commonwealth on "Up" journey	*B	
„ Paraffin (see page 158)	2	
„ Palm	1	
„ Peanut, ground	1	
„ Pine	1	
„ Red or White, for spraying purposes	A	
„ Salad	2	
„ Sheep, Blowfly	1	
„ Sunflower Seed	C	
„ Tar, n.o.s.	*C	
„ „ „ used directly for fuel in open furnaces, and so declared on consignment note (see (af))	M	
„ Textile	*1	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Oil Cake for cattle food (see (af))	A.P.	a
Oil Cloth (when rolled tightly on wooden cores)	2	b
Olives (same as fresh fruit)		
Onions, minimum 10 tons per 11-ton truck, 13½ tons per 16, 22 or 27-ton truck	Grain	a
„ small quantities, "Smalls" minimum	A, less 10 p.c.	a
Opium, subject to General Condition No. 2	Double 2	d
Orchard Pests Specifics—including Slug and Snail exterminators		
„ „ „ Non-poisonous	A	c, d, i, j
„ „ „ Poisonous, the name of the commodity to be shown on the Consignment note. (See Volume II for conditions of carriage)	A	
Ores—Crude—		a
„ „ Consigned to a Victorian Country Metallurgical Industry ...	A.P.	
„ „ Barytes, iron, lead, manganese	M	
„ „ n.o.s.	A	
„ „ see (ai)	A less 25 p.c.	
Ornaments—		
„ Plaster, cement, and n.o.s., Packed	2	d, g
„ Loose, at **O.R.** only	Double 2	
Ovens, Camp and Colonial	2	d, g
Ovolos (see Plaster)		
Oxide of Iron, crude, for gas purification	M	a
Oxide, Red	*A	c , d, f, j
Oysters and other Shell Fish in Bottles	2	c, d
„ „ „ Fresh, in bags (see page 128)		
P.		
(For Explosives and Goods of a Dangerous Nature see Volumn II)		
Paintings (see Engravings)		
Paints and colours, non-inflammable	*2	c, d, g, i
Paints containing Inflammable Liquids, (see Volume II for conditions of carriage)		
Palings (see Timber)		
Pallets (Timber)	B	
Pans, Sanitary	2	d, g
Paper—		
„ Paper, Paper Bags or Wrapping, n.o.s. (see page 158)	1	b
„ „ „ „ „ (see (af))	C	b
„ Bags, Produce of the Commonwealth on "Up" journey ...	B	b
„ Bags for containing Cement, Manure, Bottled Beer, Lime and Chemicals	B	b
„ „ „ minimum 6 tons per 11-ton truck and 10 tons per 16, 22 or 27-ton truck...	A	b
„ Emery, Fly or Sand	2	c, d
„ Felt (see Felt)		
„ Glass	*1	c, d
„ „ (see (ab))	*C	c, d
„ For Paper Bag manufacture, "Smalls" minimum	C	b, d
„ „ „ „ (see (af))	B	b, d
„ For printing newspapers and so declared on consignment note, "Smalls" minimum	C	b, d
„ For wrapping fruit or lining fruit cases and so declared on consignment note	B	b, d
„ For use in fruit drying	C	b, d
„ Hangings	2	b, d
„ Patterns	2	c, d

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Paper—*continued.*		
„ Roofing and Insulating, in rolls, cored	1	b
„ Strawpaper	*A	b, d
„ Toilet—		
„ „ n.o.s.	2	b, c, d
„ „ Unfinished, in original rolls from paper mills	1	b, d
„ „ „ „ „ „ „ (see (af)) ...	C	
„ Waste and Old, for paper making or packing	*A	a, b
„ Writing	2	c, d
Paperboard	*C	b
„ (see (af))	*A	b
Parsnips	Grain	a
„ small quantities, "Smalls" minimum	A, less 10 p.c.	a
Pastes, Meat or Fish	2	c, d
Pasteurizers (see Vats)		
Patterns—		
„ Wooden, n.o.s.	2	d, g
„ „ When sent to or from a Victorian country industry ...	C	d, g
Pearl Ash	A	a
Peas—		
„ Blue, in packets	1	c, d
„ „ in bulk	Grain	a
„ Field	Grain	a
„ Green, fresh (same as vegetables)		
„ Split, n.o.s.	1	a, c, d
Produce of Commonwealth on "Up" journey	B	a, c, d
Peat	M	a
„ Dust	C	
Pelts, pickled	*B	e
Pepper, ground or whole, also cayenne	*2	c, d
Perambulators—		
„ Packed	2	d, g
„ In nests, O.R. only	2	
„ Loose, at O.R. only	Double 2	
Perfumery, in bottles	2	c, d
Petroleum Jelly	1	c, d
Phosphatic Rock (see Manures)		
Pianos and Organs— Same as Furniture		
Pickeyes, Packed	*1	d, g
„ Loose, at O.R. only	*2	
Pickles—		c, d
„ In bottles or jars n.o.s.	2	
„ „ „ Produce of Commonwealth on "Up" journey	*A	
„ „ „ Produce of Commonwealth, consigned direct to the seaboard for immediate export beyond the State (see page 148)	*A.P.	
„ „ „ Product of Victorian Industry exported by rail beyond the State (see page 152)		
Pictures (see Engravings)		
Pigs' Feet (see page 155)	B, less 10 p.c.	a
Piles (see Timber)		
Pins, Wooden, for telegraph poles	1	a, d
Pipes—		
„ Brass or Copper	2	d, g
„ Concrete and Collars‡		g
„ „ „ High pressure (exceeding 13lb. per sq. inch) ...	*B	
„ „ „ „ „ (see (ah)) ...	*A	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Pipes, Concrete and Collars‡—*continued.*		
" " " Low pressure (not exceeding 13lb. per sq.inch, and so declared on consignment note) ...	A	
" " " " " (see (ah)) ...	A, less 25 p.c.	
" " " Product of Victorian Country Industry ...	*A	
" " " " " (see (ah)) ...	M, less 21 p.c.	
Pipes, Earthenware—		g
" " Glazed—		
" " " High pressure (exceeding 13 lb. per sq. inch)	B	
" " " Low pressure (not exceeding 13 lb. per sq. inch and so declared on consignment note) ...	A	
" " " " " (see (ac)) ...	A, less 25 p.c.	
" " " Product of Victorian Country Industry ...	*A	
" " " " " (see (ac)) ...	M, less 21 p.c.	
" " Unglazed without sockets—for Agricultural purposes	A	
" " " Minimum 7 tons per 11-ton truck, and 12 tons per 16, 22 or 27-ton truck ...	M	
" " " Product of Victorian Country Industry ...	*A	
" " " " " Minimum 7 tons per 11-ton truck, and 12 tons per 16, 22 or 27-ton truck ...	M, less 21 p.c.	
" Asbestos-Cement—		
" " High pressure, including cast-iron fittings ...	B	g
" " " " (see (ah))	A	g
" " Down	2	g
" Iron or Steel—		
" " " Black or Galvanized, in bundles, **C.R.**		
" " " n.o.s.	*1	
" " " " (see (ai))	*C	
" " " " Consigned to Victorian Country Water Trusts and Municipalities	C	
" " " " Down	2	g
" " " Cast Iron—		g
" " " For city or town reticulation by Victorian public bodies	B	
" " " Gas and n.o.s.	C	
" " " Vent	*2	g
" " " Wrought Iron or Steel	*B	
" " " " " " (see (ah)) ...	*A	
" Plastic	2	g
" Smoking	2	c, d
" Wine and Tallow (see Empties, page 127)		
" Wooden—		
" " Consigned to the country for use by Victorian Government Bodies, Municipalities, or Water and Sewerage Trusts	B	g
" " For irrigation	A	g
Piping, Lead and Compo	2	d, g
Pitch	C	f, j
Pitchers (see Stone)		
Plants and Shrubs—		
" " In cases or bundles	C	d, g
" " Loose, at O.R. only	2	

‡ Concrete Pipes in crates and declared on consignment note to have been manufactured at least five weeks may be accepted at Commissioners' Risk.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Plaster Articles—		
„ „ Plaster, *Fibro Plaster and Asbestos Cement sheets and strips such as Durabestos, Fibro Cement, Fibrolite		d, g
„ „ Plasterboard	B	d, g
„ „ Packed, minimum 8½ tons per 11-ton truck, and 14½ tons per 16, 22 or 27-ton truck	A	d, g
„ „ Loose (see (ai)) at O.R. only	A	
„ „ Cornices, Mouldings, Ovolos and Scotia—		
„ „ Packed	C	d, g
„ „ Loose, at O.R. only	2, plus 25 p.c.	
„ „ Ceilings, Centre Pieces, and Centre Panels—		
„ „ Packed	2	d, g
„ „ Loose at O.R. only	2, plus 25 p.c.	
Plaster of Paris—		a, d, f
„ „ n.o.s.	*B	
„ „ Produce of Commonwealth (see (ai))	*A	
„ „ „ „ On "Up" journey	*M	
Plastic Ware	2	d, g
Plate or Plated Ware—		
„ „ „ Gold or silver (see General Condition No. 20)		
„ „ „ n.o.s., subject to General Condition No. 2 ...	2	d
Plates—		g
„ Earth, lead	1	
„ Fish	C	
„ „ (see (ai))	B	
„ Floor, iron	2	
„ Perforated steel, for fruit buckets, consigned to Victorian country factories	A	
„ Sole (see Machinery)		
Ploughshares	*1	d, g
„ Minimum 7½ tons per 11 ton truck and 10½ tons per 16, 22 or 27 ton truck	*C	d, g
Plumbago, n.o.s.	2	d, f
„ In sacks at O.R. only	1	
Poles—		
„ Curtain, wooden	2	b, d, g
„ Shafts and bars, for vehicles, n.o.s.	*2	g
„ „ „ „ „ „ Unfinished and in the rough ...	*1	g
„ Telegraph and scaffolding, wooden (see Timber)		
„ „ Concrete (see Cement and Concrete Articles)		
„ „ Iron	C	
„ „ (see (ai))	A	
Polishes—		
„ Metal, wood, furniture, with solvent base (see Volume II)		
„ Knife, metal, and n.o.s.	2	c, d, g
„ Wax, for boots, furniture, or linoleums, Produce of Commonwealth, on "Up" journey	*A	c, d, g
„ Marble, Produce of Commonwealth, on "Up" journey, "Smalls" minimum	*C	c, d, g
„ Soap	C	e, i
„ Stove, if with solvent base see Volume II for conditions of carriage		
„ n.o.s.	2	c, d, g
„ Produce of Commonwealth, on "Up" journey	*A	c, d, g

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ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Pollard (see pages 115, 150 and 153)	Flour, Bran and Pollard	a
Postal Line Material—		
" " " Bands, transposition, galvanized iron	1	g
" " " Bars, check, iron		
" " " Braces, arm, galvanized iron		
" " " Combiners, arm, iron		
" " " Rings, bridle, galvanized iron		
" " " Stakes, earth, galvanized iron		
" " " Stay rods, galvanized iron		
" " " Steps, pole, galvanized iron		
" " " Steps, manhole, iron		
" " " Asbestos Cement Pillar bases and hoods	A	g
" " " Cement jointing pits	A	g
Posts—		
" Concrete (see Cement and Concrete Articles)		
" Fencing, Iron (see Wire Fencing)		
" Lamp and verandah, Iron	2	g
" Verandah—		
" " Hardwood, dressed	A	
" " Softwood	C	
" and Rails, hardwood (see Timber)		
Potash	1	d, f, j
" (see (af))	C	d, f, j
" Bisulphite of	2	d, f, j
" Carbonate of	A	d, f, j
" Muriate of (see Manures)		
Potash Salts—		a
" " n.o.s.	M	
" " (a) If consigned to a Victorian country station and declared on consignment note to be used for manurial purposes only	Manure	
" " (b) If consigned to a Manure Manufacturer in Victoria and declared on consignment note to be used in the manufacture of Artificial Manure	Manure	
Potato Crisps	1	c, d
Potato flour, product of Victorian country industry	*C	a
Potatoes—	Grain	a
" Small quantities, "Smalls" minimum	A, less 10 p.c.	a
Pots, Flower, Earthenware	C	d, g
Potteryware—		d, g
" Including locally manufactured sanitary pedestal pans		
" Packed (see (ac))	*A	
" " Smaller lots	*C	
" Loose at O.R. only	*2	
" For export (see pages 148 and 158)		

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Poultry, dead—		d
„ „ C.R.	1 plus 10 p.c.	
„ „ O.R.	1	
„ Chilled or Frozen in Refrigerator trucks (see Frozen Meat)		
„ For export (see page 188)		
„ Living, in crates, coops, and cases, "Smalls" minimum ...	B, plus ½ p.c.	
Poultry Humoss (see Manures) ...		
Preservatives and Compounds used in manufacture of Cheese, Butter, Meat, Preserves, &c.	1	c, d, e, i
Preserves, n.o.s.	2	c, d
Presses (see Machinery)		
Pulleys, Blocks and Pulley Wheels	1	g
Pulpboard	1	d, g
„ (see (af))	C	d, g
Pumicestone	C	a
Pumpkins (including Turks Caps and Triambles)		
„ (see (af))	Grain	a, d, g
„ Small lots (must be in bags, crates, or cases) "Smalls" minimum	A, less 10 p.c.	a, d, g
Pumps—		
„ n.o.s.	2	d, g
„ Spraying	1	d, g
Puncheons (see Empties page 127)		
Putty	2	c, d, f, j
Pyrites (same as Ores, crude)		
Q.		
(For Explosives and Goods of a Dangerous Nature see Volnme II.)		
Quartz and Quartz Tailings	M	a
R.		
(For Explosives and Goods of a Dangerous Nature see Volumn II.)		
Rabbits—		
„ Canned	*B	c, d
„ „ (see (ai))	*A	c, d
„ Fresh or chilled—		
„ In bags or crates, minimum 5 cwt.	A	g
„ „ „ „ smaller lots, "Smalls" minimum	B	g
„ Chilled in truck loads, minimum 6⅓ tons per 11-ton louvre or 12 ton "T" truck ; 9⅓ tons per 15 ton louvre or "T" truck, maximum rate 82s. 2d. per ton	A	g
„ Frozen or chilled, loaded in refrigerator "T" trucks (see frozen meat, page 57), (see pages 188 and 189)		
Rabbit Traps	2	d
Radiators (same as Furniture)		c, d
Radio Sets (same as Furniture)		c, d
Raffia	C	a, b
Rags, n.o.s.	C	a, b
„ old, for paper or flock making, on declaration	A	a, b
Rails, railway and tramway, iron.	C	
„ „ „ „ (see (ai))	B	
„ „ „ „ (see page 159)		
Rails, wooden (see Timber)		
Railings, iron (in bundles C.R.)	2	
Railways, switchback (see Circuses)		
Rakes—		
„ Hand and Garden	2	g
„ Fire (in bundles, C.R.)	1	

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Rattans (in bundles, **C.R.**)	2	
Rayon Goods		b, c, d
„ „ n.o.s.	2	
„ „ Produce of Commonwealth	1	
„ „ „ „ on "Up" journey, "Smalls" minimum	*C	
Reaping Hooks	2	b, d, g
Reels—		
„ Hose (minimum 5 cwt. each, including equipment), O.R. (see page 155)	2	
„ Hose, for fixing inside buildings	2	g
Refrigerators, household or shop (same as Furniture)		d, g
Resin	*A	c, d, f, j
Retorts and Crucibles, clay, n.o.s.	C	g
„ „ „ Produce of Commonwealth, on "Up" journey (see (aa))	*M	
„ „ iron	2	g
Rice and Rice Flour	C	a
„ Bran and Pollard	A.P.	a
„ Palm or Flaked	1	c, d
„ Paddy	Grain	a
„ Hulls (see (ab))	Grain	a
Ridging, Galvanized Iron	2	d, g
„ Fibro Cement	2	d, g
Rifles	2	d
Rims—		
„ Buggy, unfinished and in the rough	*1	b
„ „ finished	*2	b
„ Wooden, for harvester wheels (same as Agricultural Machines, parts of)		
Rivets—		a, d, j
„ n.o.s.	2	
„ Iron	*1	
„ „ for Boilers	*1	
„ „ „ (see (ai))	*B	
„ when forming part of a truck load with Bar, Rod, Plate, Hoop, and Angle Iron and Steel (see (ai))	*B	
Rollers—		
„ Garden	2	d, g
„ Mill	1	d, g
„ „ for repairs, half rate on return journey on declaration		
„ Road	C	
„ Wooden, for windlasses	A	
Roofing Materials (such as Malthoid, Ormonoid and Rubberoid, tightly rolled on wooden cores	C	b
Roots—		
„ Medicinal	A	a
„ Agricultural and Garden (see Seeds)		
„ Orris, powdered	*2	a
Rope, old, for paper making on declaration	A	a, b
„ Hempen	*1	a, b
„ Wire and Steel, in coils	1	
„ for mining, on declaration (see (af))	C	
„ Scrap for reclamation purposes—"Smalls" minimum	A	a, b
Rubber Goods	*2	c, d
Rubble (see Stone)		
Rugs—		
„ n.o.s.	2	b, c, d
„ Woollen (see Blankets)		
Runners, Undressed, Produce of Commonwealth	B	d, e
Rushes, Green or Dried, n.o.s. (also see Grass)	A	a, b
Rye (see page 151)	Grain	a

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
S.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Saddlery	2	b, d, g
Safes, Iron or n.o.s.	2	g
Sago	2	a, c, d
Salt—		
„ n.o.s.	1	a, c, d
„ Cake	M	a, c, d
„ Refined	*A	a, c, d
„ „ Product of Victorian Country Industry on "Up" journey (see (ai))	*A, less 25 p.c.	a, c, d
„ Lick for Stock	A.P.	c, d
„ Rock	A.P.	a
„ Unrefined, in bags or in bulk—		a
„ For destroying noxious weeds (see page 159)		
„ n.o.s.	A.P.	a
„ See Manures, and page 159		
„ ex Cheetham Salt Co's. Siding (see page 159)		
Salts—		
„ Epsom		
„ „ Crude	1	a, c, d
„ „ In packets (Druggists)	2	c, d
„ Glauber, refined	*2	c, d
„ „ crude	*B	a, c, d
Sand—		
„ n.o.s. (see pages 113 and 159)	Sand	
„ Antimony	A	
„ „ (see (ai))	A, less 25 p.c.	
„ Black (Ilmenite)	A	
„ „ (see (ai))	A, less 25 p.c.	
Sanitaryware	*2	d, g
„ (see page 158)		
Sarsaparilla	2	c, d, g, k
Sashes, window, set up—		
„ Glazed (when protected by timber, **C.R.**)	2	
„ Unglazed, in secure bundles, **C.R.**	2	
Sauces—		c, d, e, g, i, k
„ n.o.s.	2	
„ Produce of Commonwealth, on "Up" journey	*A	
„ Produce of Commonwealth consigned direct to the seaboard for immediate export beyond the State (see page 148)	*A.P.	
„ Product of Victorian Industry exported by rail beyond the State (see page 152)		
Sausage Skins and Hog Casings	1	d, e
Sausage Skins, Undressed, Produce of Commonwealth	B	d, e
Sausage Meal	1	a, d, f
Sausages, "Smalls" minimum	C	d
Saws	2	d, g
Sawdust (see (af)	M	a
Scaffolding Clips (Steel)	*2	a
Scales and Beams	2	d, g
Scheelite	2	a
„ (see (af))	1	
Screenings—		
„ Coke (see (ae))	M	a
„ Stone (see page 154)	M	a

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Screens, iron, mining (when in bundles, **C.R.**)	1	
Screws and Washers—		
„ „ n.o.s.	*2	a, d
„ „ Iron	*1	a, d
Scythe Blades	2	d, g
„ Stones	2	d
Seats—		
„ Church, (when in pieces)	1	d, g
„ „ or Hall, complete	2	h
„ Garden	2	
„ Wooden, for chairs	1	d, g
Seaweed, Dried	B	a
„ Wet (see Manure)		
Securities for the payment of money (see General Condition No. 20)		
Seeds and Roots—		
„ „ Agricultural, Lupin, Grass, Sorghum, Canary, Panni-cum, Soudan, and Vegetable, n.o.s. (see (aa)) ...	Grain	a
Seeds—		
„ Roots and bulb, garden and flower	2	a, c, d
„ Sunflower (see (aa))	A.P.	a, c, d
„ Carraway	2	a, c, d
„ Ambercane	A.P.	a, c, d
„ Mustard, "Smalls" minimum	C	a, c, d
„ Prepared for birds	C	a, c, d
Semolina—		
„ In bulk in bags	Grain	a
„ In packets	1	c, d
Shafting, steel	*2	
Shafts (see Poles)		
Shale—		
„ Oil	B	i
„ Other than oil	M	a
Sharps (see (aj) and page 150)	Grain	a
Shavings, Wood (see (ab))	M	a
Sheep Branding commodities	C	c, d, e, g, i
„ Drench	B	c, d, i
„ Trotters for glue making	A	a
Sheet Piling—		
„ „ New	2	
„ „ „ (see (ai))	1	
„ „ Secondhand—	1	
„ „ „ (see (ai))	C	
Shells, gravel	M	a
Shell Grit (see page 160)	M	a
Shingles (see Timber)		
Shop Fronts—		
„ „ Not set up	2	d, g
„ „ Set up	Double 2	d, g
Shot	2	a, d
Shovels and Spades—		
„ „ (in cases, **C.R.**)	1	d
„ „ (in bales, **C.R.**)	2	b
Shrubs (see Plants)		
Sieves	2	d, g
Signboards, wooden, signs to be protected—		
„ „ „ „ Up to 18 feet in length ...	2	
„ „ „ „ Over 18 feet in length ...	2 plus 50 p.c.	

APLHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Signs—		
„ Electric	2	c, d, g
„ Metal, including advertising plates	2	d, g
Silica, crude	M	a
Silicate of Cotton, for covering steam pipes	C	a, b
Silk Waste	A	a,
Silks, subject to General Condition No. 2	2	d
Sillimanite	M	a
Silos, galvanised iron or steel frame, in sections	C	g
Silver, manufactured, or unmanufactured (see General Condition No. 20)		
Sisal	*A	a, b
Skewers, wooden	C	c, d
Skins—		
„ Kangaroo, Wallaby, Opossum and Goat	1	a, b, d
„ Fox—		
„ „ O.R.	2	
„ „ **C.R.**, subject to General Condition No. 2	2	a, b, d
„ Sheep—		
„ „ In bundles	*B	
„ „ Loose—		
„ „ „ minimum 3 tons (which may be comprised of sheep skins, hides and tallow)	*B	
„ „ „ Smaller lots—		
„ „ „ „ „ **C.R.** subject to Note (a) ...	*1, plus 10 p.c.	
„ „ „ „ „ O.R.	*1	
NOTE (a)—When loose sheep skins are required to be tallied, or are consigned at Commissioners' Risk, an extra charge of 4s. 0d. per ton for tallying shall be made in addition to Commissioners' Risk rate.		
„ Rabbit and Hare—Minimum 1 ton	B	a, b, d
„ „ „ Smaller lots	1	a, b, d
Skirtings, Hardwood (see Timber)		
„ Softwood	C	
Slabs—Ironite	A	g
„ „ (see (ai))	A, less 25 p.c.	g
Slates—		d, g
„ Fibro cement	C	
„ Asbestos, roofing	C	
„ Roofing, n.o.s.	B	
„ „ Produce of Commonwealth, on "Up" journey	*M	
Slate Slabs (see Stone)		
Sleepers, Hardwood (see Timber)		
Sleep Outs (see Houses)		
Slippers	*2	c, d
Soap—		
„ Fancy		
„ „ n.o.s.	2	c, d
„ „ Produce of Commonwealth	*1	c, d
„ Foam, Extract of	2	c, d
„ Household and Extract of	*C	c, d
„ „ (see (ai))	*B	c, d
„ Lye Refuse (see (ae))	M	e, i
„ Polish	C	c, d, e, i
„ Soft	*C	c, d, e, i
„ „ accompanying red oil as spraying material	A	i
Soda Ash	*A	a

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Soda—		
„ Bicarbonate of	B	a, c, d
„ Carbonate of, for fruit dipping and so declared on consignment note	A	a, c, d
„ Crystals, washing	B	a, c, d
„ Hypo-phosphate of	B	c, d, e, f
„ Hypo-sulphate of	A	c, d, e, f
„ Silicate of	*A	c, d, ,e, i
Sodium Sulphite—		
„ „ Crude, consigned to Victorian paper mills for manufacture of paper	M	a
„ „ n.o.s.	B	a
Soft drinks (Aerated and Temperance)	*B	d, e, g, k
„ „ (see (aa))	*A	d, e, g, k
Soil, n.o.s.	M	a
„ for top dressing Bowling Greens, Cricket Pitches or Golf Greens	Fire-wood plus 20 p.c.	
Solder	1	d
Solvol	1	c, d
Soup, Tomato and Vegetable (same as Sauces)		
„ Tablets and Powders		
„ n.o.s.	2	c, d
„ Produce of Commonwealth, on "Up" journey ...	*A	c, d
Spalls (see Stone)		
Spanish Clay, for wine fining	1	a, d
Spice, Condition, for stock	A	a, d
Spices, Culinary	2	c, d
Spikes, Iron	1	a, d
Spindles, Wood or Iron	1	b, d
Spirits—		c, d, e, g, i
„ Denatured	1	
„ n.o.s.	2	
„ Produce of Commonwealth "Smalls" minimum	*C	
„ For fortifying wine (Certificate from Customs Department to accompany consignment note)	*B	
Spokes—		
„ Wooden (see Felloes)		
„ Iron, for agricultural machines	1	b, d
Spongemix	1	c, d
Spouting, galvanized iron	2	g
Spraying materials (see Orchard Pests Specifics)		
Springs—		
„ Vehicle	*2	b, d
„ Furniture	*2	b, d
Stamps (see General Condition No. 20)		
Standards, Fencing (see Wire)		
Starch—		
„ n.o.s.	*2	a, c, d
„ Confectionery	*A	a, c, d
„ Potato	*A	a, c, d
Stationery	2	c, d
Stearax	A	a
Stearine	A	f, j
Steatite, crude or manufactured	M	a
Steel—		
„ Expanded, Packed	1	d, g
„ „ Loose at O.R. only	2	
„ Polished	*2	d, g

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Steel—*continued.*		
„ Shafting	*2	
„ Sheet (see Iron)		
„ „ Stainless	2	g
„ Strapping Bands for securing timber	C	
„ „ „ „ (see (ai))	B	
Steelite or Steelcrete—		
„ „ For cement floors	1	
„ „ „ (see (ai))	C	
Stock Feeders, automatic—		
„ „ „ Dismantled	B	g
„ „ „ Set up, manufactured of		
„ „ „ Galvanized and angle iron	2, plus 50 p.c.	
„ „ „ Concrete and timber	2	g
Stone, as under—		
„ Ballast		
„ Coring		
„ Dust (see page 154)		
„ Flagging		
„ Granite, rough blocks (see page 160)		
„ Kerbing or street paving		
„ Marble, rough blocks (see page 160) ...	M	
„ Metal, road (see page 154)		
„ Pitchers		
„ Rubble (see page 154)		
„ Spalls (see page 154)		
„ Toppings (see page 154)		
„ Undressed (including slate), n.o.s. ...		
„ Granite, sawn, but not otherwise dressed (see page 160) ...	M	d, g
„ Granite, marble, and other stone—		
„ Dressed for buildings and foundations for monuments, grave railings or headstones, but not polished or carved either wholly or partially	A	d, g
„ Dressed, and only partially polished or carved, or wholly polished on only one side	C	d, g
„ Wholly polished or carved, Packed	*1	d, g
„ Loose, at O.R. only	*2	
„ Gravestones—		
„ „ Dressed, polished or carved, Packed	1	d, g
„ „ Loose, O.R. only ...	2	
„ Marble (see page 160)		
„ Marble Chips (see page 160)	M	a
„ Slate slabs, for paving, flooring, building, and graves	A	g
„ Slate slabs, for billiard tables	1	d, g
Stones—		
„ Fruit, "Smalls" minimum	A	a
„ „ (see (af))	M	a
„ Precious (see General Condition No. 20)		
Stoneware (see Earthenware)		
Stout (see Ale)		
Stoves and Stove Pipes—		
„ „ „ n.o.s.	2	d, g
„ „ „ Produce of Commonwealth	*1	d, g
Straw—		
„ n.o.s., in bundles or bales (see (ag))	Grain	
„ Flax (see (ab))	Grain	
„ „ Minimum 3 tons for any quantity loaded in a 4-wheeled truck ...	S.A.P. less 10 p.c.	
Strawboard	*C	d, g
„ (see (af))	*A	d, g
„ (see page 160) ...		

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Sugar—		
„ n.o.s. minimum 1 ton	*B, plus 25 p.c.	a
„ „ "Smalls" minimum	*C	a
„ Icing, castor, and loaf	2	a, c, d
„ Cane, "Smalls" minimum	A	a
Sulphate of—		
„ Ammonia (see Manures)	A	a
„ Iron	A	a
„ „ consigned to Artificial Manure Manufacturers (see page 111)	Manure	a
„ Potash (see Manures)		
„ Sodium (see Salts Glauber)		
Sulphur	*B	a
„ (see (af))	A, less 20 p.c.	a
Sulphur Lick	B	a
„ Rock, for manufacture of sheep dip, and orchard pests specifics, and so declared on the consignment note	M	a
Swingle Bars (when in bundles, **C.R.**)—		
„ „ For agricultural machines	1	
„ „ n.o.s.	2	
Syphons, soda-water, in packing in cases or crates (same as Bottles)		
T.		
(For Explosives and Goods of a Dangerous Nature see Volumn II.)		
Tacks	*2	d, f, j
Tags, flour	1	c, d
Tallow	*B	f, j
Tan, Spent (see (ab))	M	a, b
Tanning Compounds—		
„ „ If dangerous, the name of the commodity is to be shown on the Consignment Note. (See Volume II for Conditions of Carriage.)		
„ „ n.o.s.	A	c, d, e, f, g, i
„ „ Consigned to a country tannery in Victoria— ...		
„ „ (see (aa))	*M	c, d, e, f, g, i
„ „ Minimum 10 cwt.	*A, less 25 p.c.	c, d, e, f, g, i
Tanks, petrol or oil, plate iron—		
„ „ „ „ (Crated, **C.R.**)	*2	g
„ „ „ „ (Not crated, **C.R.**)	*2, plus 10 p.c.	
„ „ „ „ „ O.R.	*2	
Tanks, plate iron, empty, n.o.s.—		
„ „ „ „ Up to 400 gallons	**C.R.**	
„ „ „ „ Over 400 gallons, and not exceeding 1,000 gallons, *rate and a half* ...	2, plus 10 p.c.	
„ „ „ „ Over 1,000 gallons, *double rate*	O.R. 2	
Tanks or Vats, galvanized or corrugated iron—		
„ „ „ „ „ Up to 1,000 gallons, *rate and a half* ...	**C.R.** 2 plus 10 p.c.	
„ „ „ „ „ Over 1,000 gallons *double rate*	O.R. 2	
Tanks, Filled. Tanks as above; contents as per Classification		
„ Concrete (see Cement and Concrete Articles)		
Tapioca	2	a, c, d

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Taps, Metal or Wooden	2	d
Tapes (Clothing)	*2	c, d
Tar, n.o.s.	B	i
„ Minimum 6 tons per 11-ton truck, and 9 tons per 16, 22 27-ton truck	A	i
„ Consigned to the country to be used in combating frosts in orchards or vineyards, and so declared on the consignment note	A.P.	i
„ Crude, used directly for fuel in place of wood, coal, or coke, and so declared on the consignment note (see (ae))	M	i
„ Dehydrated, consigned solely for the purpose of fluxing or thinning bitumen and so declared on the consignment note (see (af)) ...	M	i
„ Compo (for tarring telegraph poles)	A	i
„ Stock	*C	i
Tarpaulins	2	b
Tea	2	c, d, g
Tents	2	b
Tent floors, wooden	B	
Terne plate (same as tinplate)		
Terra-cotta, architectural.	2	d, g
„ „ (see (ae))	C	d, g
„ Lumber, Kerbing, and Channelling, minimum 8½ tons per 11-ton truck, and 14½ tons per 16, 22 or 27-tons truck ...	M	d, g
Tetracarnit, to woollen and knitting mills	*2	i
Texaco Cold Patch (cold mix asphalt) (see Bitumen Emulsion)		
Theatrical Scenery—		d, g
Per Truck — Per Mile 4-wheeled ... 3 4 Q.R. ... 6 8 Q., E. or S. ... 10 0 } Minimum charge £5 7s. 6d.		
„ „ Small lots in cases or crates	2	d, g
Three-ply Timber, secured by wire in bundles **C.R.**	C	
Ticking	*2	b, d
Tiles—		d, g
„ Common Paving, n.o.s.	A	
„ Produce of Commonwealth on "Up" journey ...	*M	
„ Roofing, glazed or unglazed, n.o.s.	A	
„ „ Produce of Commonwealth, on "Up" journey, minimum 7½ tons per 11-ton truck, and 11 tons per 16, 22 or 27-ton truck	*M	
„ Drain and Edging, n.o.s.	A	
„ „ Produce of Commonwealth, on "Up" journey (see (af))	*M	
„ Ornamental and Tesselated	*2	
„ Glazed, n.o.s.	1	
„ „ Produce of Commonwealth, on "Up" journey	*C	
Timber—		
„ **Hardwood, Unseasoned (See page 112)**		
„ Sawn, dressed or undressed n.o.s.	Unseasoned Hardwood Timber	
„ Cut to sizes for case making		
„ Sleepers, sawn or hewn		

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Timber—*continued.*		
„ **Hardwood, Unseasoned**—*continued.*		
„ Palings, Pickets, Battens, Droppers, sawn or split	Unseason-Hard-wood timber	
„ Posts and Rails, sawn or split		
„ Rough, n.o.s. } See loading scale, page 119 re tonnage to be charged	Special M	
„ Stakes Vine		
„ Timber (unseasoned) of dimensions requiring the services, of bogie trucks, 5 4/5 d. per ton per mile, minimum rate 13s. 7d. per ton. Minimum 17 tons per Q.R. truck, 24 tons per "E", "S," or "Q" truck. When truckloads are forwarded a distance of over 167 miles, 5 4/5 d. per ton per mile, less 20 per cent. shall be charged subject to a minimum rate of £4.0.8 per ton. Class A rate and conditions shall apply if cheaper.		
„ Props and Slabs, undressed, for mining (see Loading Scale page 119 re tonnage to be charged)	Fire-wood plus 20 p.c.	
„ **Hardwood, Seasoned,** sawn, dressed or undressed, n.o.s.	A	
„ (see (ad))	A, less 25 p.c.	
„ Seasoned at Victorian country seasoning works when railed therefrom on "Up" journey (see (ad))	Special M	
„ Cut to sizes for case making		
„ "Smalls" minimum	A	
„ (see (ad))	Special M	
„ Architraves, Moulding and Skirtings	A	
„ (see (ad))	A, less 25 p.c.	
„ Oak, imported	*C	
„ (see (ac))	*B	
„ **Logs, Piles, Poles and Sawn Hardwood),** in long lengths, n.o.s.; minimum 17 tons per "Q.R." truck, 24 tons per "E," "S" or "Q." See note (a)	Log rate see§	
„ Poles, scaffolding, (see (af) and Note (a)		
„ „ for case making (see (af) and Note (a)		
§ Log rate 7d. per ton per mile; minimum rate, 16s. 4d. per ton. When truckloads are forwarded a distance of over 167 miles, 7d. per ton per mile, less 20 per cent., shall be charged subject to a minimum rate of £4.16.10 per ton. Class A rate and conditions shall apply if cheaper.		

Note (a). The tonnage minimum for piles and poles in 30 and 35 feet lengths, loaded in "Q" trucks is20 tons.

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Timber—*continued.*		
„ **Logs, Piles and Poles (Hardwood)**—*continued.*		
„ Poles and Logs, in short lengths, loaded in a 4-wheeled truck without overhanging	Unseasoned Hardwood, Timber,	
„ **Softwood**—		
„ Sawn, n.o.s.	C	
„ „ (see (ac))	B	
„ Architraves, Mouldings and Skirtings	C	
„ Cut to sizes, for case making, n.o.s. "Smalls" minimum ...	A	
„ „ „ „ „ (see (ab)	† A, less 25 p.c.	
„ Staves	C	
„ **Pinus Radiata, Murray Pine and Cyprus Pine,** Australian grown, as under :—		
„ Unseasoned, sawn—		
„ Pinus Radiata, minimum 8½ tons per 11-ton truck, 14½ tons per 16, 22 or 27-ton truck	Special M	
„ Murray Pine (see(af))		
„ Cypress Pine (see (af))		
„ Seasoned, sawn—		
„ Pinus Radiata (see (ad))	A, less 25 p.s.	
„ Murray Pine (see (af))		
„ Cypress Pine (see (af))		
„ Smaller lots	A	
„ Pinus Radiata, seasoned at Victorian country seasoning works and railed on the "Up" journey therefrom (see (ad))	Special M	
„ Unseasoned or seasoned, cut to sizes, for case making, "Smalls" minimum	A	
„ „ (see (ad))	† Special M	
„ **Logs**		
„ Pinus Radiata and Murray Pine, Australian grown, in long lengths (see (af))	Log rate § page 75	
„ „ In short lengths, loaded in a 4-wheeled truck without overhanging (see (ad))	Special M	
„ Three-ply, secured by wire in bundles, **C.R.** ...	C	
„ Veneer	C	d, g
Tinfoil	2	d
Tin—		
„ Concentrates	A	a
„ Ingot	2	
„ Plate, n.o.s.	C	
„ „ (see (ai))	B	
„ „ to Victorian country factories	*A	
„ „ Clippings (see (aa))	M	
„ Scrap (see (aa))	M	a
Tins—		
„ Empty (see Empties page 125)		
„ Kerosene, flattened, plain, or corrugated, in bundles "Smalls" minimum	B	

† maximum rate 98s. 7d. per ton

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Tinware, n.o.s.	2	d, g
Title Deeds (see General Condition No. 20.)		
Tobacco—		b
„ Leaf and Stalks (in bundles, **C.R.**)		
„ „ „ Minimum 10 cwt.	B	
„ „ „ "Smalls" minimum	C	
„ Smoking, n.o.s.	2	c, d
„ Plant Refuse	A	a, b
Tomato—		
„ Paste, Pulp, Puree (same as Fruit Pulp)		
„ Soup (same as Sauce)		
„ Juice (same as Fruit Juice)		
Tomatoes (same as Fruit)		
Tools	2	d
Toppings, Stone (see Stone)		
Tourist Bureaux, exhibits for (see page 149)		
Tow	*A	a, b
Toys—		
„ (in cases or crates, **C.R.**)	2	d, g
„ (in packages at O.R. only)	2	
„ (loose, at O.R. only)	2 plus 50 p.c.	
„ Wheeled, unassembled in packages, at O.R. only	2	
Tram Cars (same as Vehicles, page 132)		
Transport Boxes	1	
Transformers	2	d, g
„ Secondhand (see Machinery)	C	d, g
Treacle, n.o.s.	1	c, d, g, i
„ For cattle Food, on declaration (see (ae))	A.P.	c, d, g, i
Trees—		
„ Fruit or Shelter and Fruit Tree Cuttings, in cases, tins or bundles	C	d, g
„ Citrus or Palm, with roots balled in earth, Product of Victorian Country Industry, on "Up" journey—		
„ Minimum 1 ton	C, less 20 p.c.	d, g
„ Smaller quantities	1, less 20 p.c.	d, g
Trellis, Wooden, collapsible (in bundles, **C.R.**)	1	g
Tricycles, childrens (see Toys)		
Troughing, Iron	2	g
Troughs—		
„ Concrete (see Cement and Concrete Articles)		
„ Iron	2	
Trucks—		
„ Cable	2	
„ Hand	2	
„ Trucks and Skips, Iron, mining, and timber	1	
Trufood (Dried Milk) (same as Milk, condensed)		
Trunks, Steel	2	d, g
Tubes—		
„ Boiler, Iron, n.o.s.	1	g
„ „ „ On "Down" journey	*1	g
„ „ „ „ „ (see (ai))	*B	
Tubing—		
„ Brass	2	g
„ For Diamond Drill and Water Boring	1	g
„ Polished	2	g
Tubs—		
„ Tin or other metal	2	d, g
„ Wooden	2	
Turnips	Grain	a
„ In small quantities, "Smalls" minimum	A, less 10p.c.	a
Twine and Lines, n.o.s.	*2	b d,

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Twine, Reaper and Binder and Seaming	*1	b, d
„ „ „ Minimum, 8 tons per 4-wheeled truck ...	*C	b, d
Twine, scrap, including Reaper and Binder Twine, for reclamation purposes —"Smalls" minimum	A	a
Type, Printing	2	d
Tyres—		
„ Iron, n.o.s.	*1	
„ „ (see (ai))	*C	
„ Motor and Cycle		
„ „ „ n.o.s.	*2	
„ „ „ Old for reconditioning	C	
„ „ „ Old for scrap when so declared on consignment note, "Smalls" minimum	A	
„ Setters, Iron	C	g
„ Shrinkers	C	g
U.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Urinals, Earthenware	2	d, g
V.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Valves, Metal	2	d, g
Vans, Furniture, and sling when without wheels, returned empty or otherwise	1	
Vats or Pasteurisers—		
„ „ Milk, tinned, steel or plain iron, not exceeding 100 gallons capacity. **C.R.**	1., plus 10 p.c.	d, g
„ „ O.R.	1	
„ „ over 100 gallons capacity ...	2	d, g
„ „ Wooden (see Empties, page 127)		
„ „ Galvanised Iron (see Tanks)		
Vegetables—		
„ Compressed	1	c, d
„ Fresh, must be in bags, crates or cases, "Smalls" minimum ...	A, less 10 p.c.	a, d, g
„ Loose, minimum 5½ tons per louvre truck	A, less 10 p.c.	
„ Single Package Rate (See page 133) ...		
„ Preserved, n.o.s.	2	c, d, g
„ „ Produce of Commonwealth on "Up" journey ...	*A	c, d, g
„ „ Produce of Commonwealth consigned direct to the seaboard for immediate export beyond the State. (See page 148)	*A.P.	c, d, g
„ „ Product of Victorian Industry exported by rail beyond the State. (See page 152)		c, d, g
„ Dehydrated or dried, n.o.s.	B	c, d, g
„ „ „ „ Product of Victorian country industry on "Up" journey	*A	c, d, g
Vegetol	C	c, d, f, g, i
Vehicles (see page 132)		
Vehicle Parts, wooden, such as hood bows, dash bends, and half circles, unfinished and in the rough	1	g
Vells	1	d, e
Veneer	C	d, g

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Ventilators—		
„ Terra Cotta Air Vents	C	d, g
„ Iron	2	d, g
Vent Pipes	*2	d, g
Verandah Posts (see Posts) ...		
Vermicelli	2	c, d
Vialit Emulsion (see Bitumen Emulsion)		
Vine Cuttings	C	b
Vinegar (in jars or bottles) n.o.s.	2	c, d, g, k
„ In bulk, n.o.s.	1	e
„ Produce of Commonwealth, "Smalls" minimum	*A, less 10 p.c.	c, d, e, g
Vineyard—Waste Products (see (aa))	A	
Vyces, Iron	2	d, g
W.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Wadding	*2	b, d
Walls, Metal (see Ceilings)		
Wallboards (pulped wood paper or cane, including Caneite, Celotex, Masonite and Jutex	C	g
„ (see also Plaster Articles)		
Washers—		
„ n.o.s.	*2	c, d
„ Iron	*1	c, d
Washing Boards, Glass and n.o.s.	1	d, g
Washing Machines	2	d, g
Waste Products consigned to Australian Red Cross Society, Melbourne ...	A	
Waste Wool, Silk and Cotton, including Card Flyings, Card Fetlings, Cotton Clippings, Cotton Waste, Wool Clippings, Wool Fluff, Wool Waste, n.o.s.	A	a, b
Watches (see General Condition No. 20)		
Water—		
„ Minimum 9 tons per 2,000 gallon tank truck plus cost of water ...	M	
„ (see page 160)		
„ Clarifier, n.o.s.	A	c, d, g, i
„ Distilled (see (af))	M	c, d, g, k, l
„ Heaters	*2	d, g
Wattle Tops and Leaves	B	a
Wax—		
„ n.o.s.	2	c, d
„ Bees*, Paraffin, and Ceresine	1	a, c, d
„ For sealing bottles, and so described on consignment note ...	1	c, d
„ Mineral	B	c, d
Weed Killer—		
„ „ Non-poisonous	A	c, d, g, i
„ „ Poisonous, the name of the commodity to be shown on the consignment note (see Volume II for Conditions of Carriage)	A	
Weights, Iron—		
„ „ Sash	1	a
„ „ (see Castings, rough)		
„ „ used by Municipal Councils for testing Weighbridges ...	C	
„ „ n.o.s. (56 lb. and under to be cased)	1	
Weighbridges (see Machinery)		
Wheat—		
„ in bags or in bulk (see page 114)	Wheat	a

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Wheat—*continued.*		
„ (see pages 150, 152 and 153)		
„ crushed or ground, "without addition of other ingredients for stock food," and so consigned (see pages 114 and 151)	Wheat	a
„ germ	1	a, c, d
„ offal and combings in bags (see page 114)	Wheat	a
„ for seed (see page 160)		
„ salts	1	a, d
Wheel-barrows	2	
Wheels—		
„ n.o.s.	*2	g
„ Barrow	2	
„ Fly (see Machinery)		
„ Iron, for mining and timber trucks and traction engines ...	1	
„ Iron, attached to axles	2	
„ Motor, without tyres	*2	
„ Plow	1	
„ Rough and unfinished	*1	
Whipple-trees (in bundles, **C.R.**)		
„ for Agricultural Machines	1	
„ n.o.s.	2	
Whiting, n.o.s.	*B	a, d, j
„ Produce of Commonwealth on "Up" journey	*A	a, d, j
Willows, for basket making (in bundles, **C.R.**)	C	
Windmills—		
„ Dismantled	*1	d, g
„ Set up, at O.R. only	*2	
Windows, wire, protected by timber, **C.R.**	2	
Wine—		c, d, e, g, i, k
„ When accepted at **C.R.**, Fibreboard containers shall be subject to a maximum of twelve 26-oz. bottles to each container		
„ Single Package Rate, (see page 133)		
„ n.o.s.	2	
„ Produce of Commonwealth "Smalls" minimum	*A, less 10 p.c.	
„ consigned direct to the seaboard for immediate export beyond the State—in hogsheads, minimum 7 tons per 11-ton truck, and 9½ tons per 16, 22 or 27-ton truck ; in quarter casks, minimum 5 tons per 11-ton truck, and 8 tons per 16, 22 or 27-ton truck (see page 148)	‡Grain	
„ Off, previously carried by rail returned to vignerons at the original forwarding station, shall be charged at half of Class A, less 10 per cent. rate, subject to Class A conditions. Declaration to be furnished with consignment note.		
„ Off, forwarded to distilleries for distillation, and so declared on the consignment note, shall be charged under Class A, less 10 per cent. rate and conditions in the first instance, and on production of a certificate from the Customs Officer that the identical wine has been distilled, the charges may be reduced to Class M, less 10 per cent. (see (ae)*)		

‡ May be accepted at **C.R.** on payment of 5 per cent. additional

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Wine—*continued.*		
„ Off, forwarded to vinegar manufacturers for vinegar making and so declared on the consignment note, shall be charged under Class A, less 10 per cent. rate and conditions in the first instance, and on production of a certificate from the Customs Officer that the identical wine has been used for vinegar making, the charges may be reduced to Class M, less 10 per cent. (see (ae)*)		
„ Finings	2	a, d
„ Quinine—		
„ „ n.o.s.	2	c, d, g
„ „ Produce of Commonwealth	B	c, d, g
„ Lees, n.o.s.	B	e, i
„ „ Produce of Commonwealth "Smalls" minimum ...	*A, less 10 p.c.	e, i
„ „ (when forwarded to distilleries for distillation and so declared on consignment note shall be charged under Class A, less 10 per cent. rate and conditions in the first instance and on production of a certificate from the Customs Officer that the identical Wine Lees has been distilled, the charges may be reduced to Class M less 10 per cent. (see (ae)*)		
„ Spirit, Produce of Commonwealth (Certificate from Customs Department to accompany consignment note)	*B	e, i
„ Stone	*C	a, d
Wire—		
„ Baling or Tying or for binding cases	A	b, d
„ Brass	2	b, d
„ Copper	2	b, d
„ Insulated	1	b, d
Iron*—		
„ Fencing, Galvanized, Barbed and Cyclone Fencing in bundles also Droppers, Staples, Clamps Standards, Strainers, Wedges, and Steel or Iron Fencing Posts—		
„ n.o.s.	C	
„ Minimum 3 tons	A	
(Wire Netting may be amalgamated with any of the above-mentioned commodities to make up the minimum of 3 tons, and also charged at Class A)		
„ Netting	A	
„ „ Minimum 7½ tons per 11-ton truck, and 10 tons per 16, 22 or 27-ton truck	Special M	
„ For Nail Making (same as Fencing Wire)		
„ For Wire Mattresses, woven	*1	b, d
„ For Mattress making, on declaration	*C	b, d
„ Cloth	2	b, d
„ Stands and Frames	2, plus 50 p.c.	d, g
Wire-cutters	1	c, d
Wireless Sets (same as Furniture)		c, d
Wolfram	2	a
„ (see (af))	1	a
Wood Pulp	*B	
„ „ (see (aj))	*A	
Woodmeal (see (af))	M	a
Woodwool, n.o.s.	C	a, b
„ For packing fruit or eggs and so declared on consignment note ...	B	a, b

ALPHABETICAL CLASSIFICATION OF GOODS (Continued).

Article	Class	Packing Symbol
Wool—		
„ In bales, bags or fadges (see pages 190)	Wool	a, b
„ Slag	C	a
„ Fluff	A	a
„ Clippings or Waste, n.o.s.	A	a
„ Dust and Waste (residue from wool carbonizing) (see Manures) ...		
„ Tops	*1	a, b
Woollen Goods (see Blankets)		
Woolpacks—		
„ "Smalls" minimum	A	
„ Secondhand, minimum 2s. 0d.	A	
Wool Bale Clips (iron)	1	d, g
Wringers	2	d, g
Y.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Yarn—		
„ Coir	*1	a, b
„ Cotton, n.o.s.	*C	b, d.
„ „ Consigned to Victorian country Knitting or Woollen Mills, Cotton Factories or Rope and Twine Works ...	*B	b, d.
„ Worsted or Woollen		
„ n.o.s.	2	d
„ Consigned to Victorian Country Knitting Mills, Woollen Mills or Cotton Factories	*B	d
„ On "Up" journey, "Smalls" minimum	*C	d
„ Fibro (same as artificial silk yarn)		
„ Flax	*B	a, b
„ Hemp	*C	a, b
„ Jute	*B	a, b
„ Silk, artificial—		
„ „ „ n.o.s.	C	b, c, d
„ „ „ Consigned to Victorian Country Knitting Mills ...	*B	b, c, d
Yeast—		
„ Liquid	2	c, d, e, k
„ Powder	2	a, c, d, f
Z.		
(For Explosives and Goods of a Dangerous Nature see Volume II.)		
Zinc—		
„ n.o.s.	2	d
„ Ingots, n.o.s.	C	
„ „ to Victorian Country Foundries	*C	
„ „ minimum 5 tons per 4-wheeled truck ...	*B	
„ perforated	2	d, g
„ Scrap, in bags	*A	a
„ Shavings, n.o.s.	2	a, d
„ „ used in assaying of gold	1	a, d
„ Sheet	2	d, g
„ White, Oxide of, ...	2	a, c, d, f, g, i
„ „ „ (see (ai))	C	a, c, d, f, g, i
Zincanneal	C	d, g
„ 20 ton lots	C less 10 p.c.	d, g

Rates for the Carriage of Goods

including Special Schedules of Rates for—

MILEAGE RATES PER TON FOR GOODS.

The following rates are for carriage at Owner's Risk. For carriage at Commissioners' Risk, see General Condition No. 4, page 10.

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
1–10	0 8 8	0 11 1	0 11 1	0 11 1	0 10 6	0 11 1
11	0 11 2	0 12 1	0 12 1	0 12 1	0 11 2	0 12 1
12	0 11 2	0 12 1	0 12 1	0 12 1	0 11 2	0 12 1
13	0 11 2	0 12 1	0 12 1	0 12 1	0 11 2	0 12 1
14	0 11 2	0 12 9	0 12 9	0 12 4	0 11 2	0 12 9
15	0 11 2	0 12 9	0 12 9	0 12 4	0 11 2	0 12 9
16	0 11 2	0 12 9	0 12 9	0 12 4	0 11 2	0 12 9
17	0 11 2	0 12 9	0 12 9	0 12 9	0 12 4	0 12 9
18	0 11 2	0 14 8	0 14 11	0 13 10	0 12 4	0 14 11
19	0 11 2	0 14 8	0 14 11	0 13 10	0 12 4	0 14 11
20	0 11 2	0 14 8	0 14 11	0 13 10	0 12 4	0 14 11
21	0 11 2	0 14 8	0 14 11	0 14 8	0 13 2	0 14 11
22	0 11 6	0 14 8	0 15 8	0 14 8	0 13 2	0 16 7
23	0 11 6	0 14 8	0 15 8	0 14 8	0 13 2	0 16 7
24	0 12 4	0 14 8	0 15 8	0 16 4	0 14 6	0 16 7
25	0 12 4	0 14 8	0 15 8	0 16 4	0 14 6	0 16 7
26	0 12 10	0 14 8	0 15 8	0 16 4	0 14 6	0 19 3
27	0 13 6	0 15 8	0 16 4	0 16 10	0 15 2	0 19 3
28	0 13 6	0 15 8	0 16 4	0 16 10	0 15 2	0 19 3
29	0 14 4	0 16 4	0 17 2	0 16 10	0 15 2	0 19 3
30	0 14 4	0 16 4	0 17 2	0 16 10	0 15 2	1 0 8
31	0 14 4	0 16 4	0 17 2	0 18 8	0 16 10	1 1 6
32	0 15 8	0 16 10	0 18 0	0 18 8	0 16 10	1 1 6
33	0 15 8	0 16 10	0 18 0	0 18 8	0 16 10	1 1 6
34	0 16 2	0 17 10	0 18 6	0 18 8	0 16 10	1 2 0
35	0 16 2	0 17 10	0 18 6	1 0 8	0 18 8	1 3 1
36	0 17 2	0 18 8	1 0 0	1 0 8	0 18 8	1 3 1
37	0 17 2	0 18 8	1 0 0	1 2 0	1 0 0	1 3 1
38	0 17 10	1 0 2	1 0 10	1 2 0	1 0 0	1 5 4
39	0 17 10	1 0 2	1 0 10	1 2 0	1 0 0	1 5 4
40	0 17 10	1 0 2	1 0 10	1 3 6	1 0 10	1 5 4
41	0 18 4	1 0 8	1 1 10	1 3 6	1 0 10	1 5 4
42	0 18 4	1 0 8	1 1 10	1 3 6	1 0 10	1 7 2
43	0 19 8	1 1 4	1 2 4	1 4 10	1 2 0	1 7 2
44	0 19 8	1 1 4	1 2 4	1 4 10	1 2 0	1 7 2
45	1 0 4	1 2 0	1 3 4	1 4 10	1 2 0	1 7 2
46	1 0 4	1 2 0	1 3 4	1 5 4	1 2 8	1 9 8
47	1 0 10	1 2 8	1 4 2	1 5 4	1 2 8	1 9 8
48	1 1 8	1 4 2	1 5 4	1 5 4	1 2 8	1 9 8
49	1 1 8	1 4 2	1 5 4	1 7 4	1 4 10	1 9 8
50	1 1 8	1 4 2	1 5 4	1 7 4	1 4 10	1 10 11
51	1 2 4	1 4 10	1 6 0	1 8 0	1 5 0	1 10 11
52	1 2 4	1 4 10	1 6 0	1 8 0	1 5 0	1 10 11
53	1 2 8	1 6 0	1 7 4	1 8 0	1 5 0	1 10 11
54	1 2 8	1 6 0	1 7 4	1 9 4	1 6 4	1 13 7
55	1 4 4	1 7 4	1 8 6	1 9 4	1 6 4	1 13 7
56	1 4 4	1 7 4	1 8 6	1 10 8	1 7 6	1 13 7
57	1 5 4	1 8 0	1 9 4	1 10 8	1 7 6	1 13 7
58	1 5 4	1 8 0	1 9 4	1 10 8	1 7 6	1 15 10
59	1 5 10	1 8 6	1 10 0	1 10 8	1 7 6	1 15 10
60	1 5 10	1 8 6	1 10 0	1 10 8	1 7 6	1 15 10

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES						
	A ‡ Minimum 2 tons	AA Smalls Minimum	B ‡ Minimum 1 ton	BB ‡ Minimum 1 ton	C ‡ Minimum 1 ton	1 Smalls Minimum	2 Smalls Minimum
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
1–10	0 11 1	0 8 5	0 12 1	0 9 8	0 12 7	0 12 7	0 13 7
11	0 12 1	0 8 10	0 12 1	0 10 11	0 13 0	0 13 0	0 15 6
12	0 12 1	0 8 10	0 12 1	0 10 11	0 13 7	0 13 7	0 16 6
13	0 12 1	0 8 10	0 12 1	0 10 11	0 15 6	0 15 6	0 18 5
14	0 12 9	0 8 10	0 14 3	0 11 9	0 16 6	0 16 6	0 19 10
15	0 12 9	0 8 10	0 14 3	0 11 9	0 17 11	0 17 11	1 0 9
16	0 12 9	0 8 10	0 14 3	0 11 9	0 19 0	0 19 5	1 1 9
17	0 12 9	0 8 10	0 15 9	0 12 11	0 19 10	0 19 10	1 2 9
18	0 14 11	0 10 2	0 15 9	0 12 11	1 0 9	1 0 9	1 4 8
19	0 14 11	0 10 2	0 15 9	0 12 11	1 1 9	1 1 9	1 5 8
20	0 14 11	0 10 2	0 18 4	0 15 0	1 2 5	1 2 5	1 6 7
21	0 14 11	0 10 2	0 18 4	0 15 0	1 2 9	1 2 9	1 7 7
22	0 16 7	0 11 5	0 18 4	0 15 0	1 4 3	1 5 1	1 9 0
23	0 16 7	0 11 5	1 0 5	0 16 4	1 5 8	1 5 8	1 10 6
24	0 16 7	0 11 5	1 0 5	0 16 4	1 6 0	1 6 2	1 12 0
25	0 16 7	0 11 5	1 0 5	0 16 4	1 6 10	1 7 7	1 14 0
26	0 19 3	0 12 10	1 2 0	0 17 11	1 8 2	1 8 2	1 14 10
27	0 19 3	0 12 10	1 2 0	0 17 11	1 8 10	1 9 0	1 15 4
28	0 19 3	0 12 10	1 2 0	0 17 11	1 9 3	1 10 6	1 16 3
29	0 19 3	0 12 10	1 4 1	0 19 8	1 10 0	1 12 0	1 18 2
30	1 1 6	0 14 7	1 4 1	0 19 8	1 10 3	1 12 11	1 18 9
31	1 1 6	0 14 7	1 4 1	0 19 8	1 11 4	1 14 3	2 0 1
32	1 1 6	0 14 7	1 5 11	1 1 0	1 12 1	1 14 10	2 1 2
33	1 1 6	0 14 7	1 5 11	1 1 0	1 13 1	1 15 4	2 2 1
34	1 3 1	0 15 9	1 5 11	1 1 0	1 14 0	1 16 3	2 4 0
35	1 3 1	0 15 9	1 8 3	1 2 11	1 14 11	1 17 10	2 5 0
36	1 3 1	0 15 9	1 8 3	1 2 11	1 17 1	1 18 2	2 7 5
37	1 3 1	0 15 9	1 8 3	1 2 11	1 18 2	1 19 2	2 8 4
38	1 5 4	0 17 2	1 9 5	1 4 0	1 19 0	2 0 1	2 9 5
39	1 5 4	0 17 2	1 9 5	1 4 0	1 19 5	2 0 6	2 10 4
40	1 5 4	0 17 2	1 9 5	1 4 0	2 0 1	2 2 1	2 11 8
41	1 5 4	0 17 2	1 12 0	1 6 1	2 0 10	2 2 6	2 13 4
42	1 7 2	0 18 5	1 12 0	1 6 1	2 1 5	2 4 0	2 13 8
43	1 7 2	0 18 5	1 12 0	1 6 1	2 1 11	2 5 0	2 14 8
44	1 7 2	0 18 5	1 14 1	1 7 8	2 3 3	2 7 5	2 16 7
45	1 7 2	0 18 5	1 14 1	1 7 8	2 4 1	2 7 10	2 17 1
46	1 9 8	1 0 4	1 14 1	1 7 8	2 4 4	2 8 10	2 18 0
47	1 9 8	1 0 4	1 15 9	1 9 0	2 5 1	2 9 5	3 0 6
48	1 9 8	1 0 4	1 15 9	1 9 0	2 6 2	2 10 4	3 1 6
49	1 9 8	1 0 4	1 15 9	1 9 0	2 6 4	2 11 8	3 3 3
50	1 10 11	1 1 0	1 17 4	1 10 4	2 7 2	2 12 4	3 4 11
51	1 10 11	1 1 0	1 17 4	1 10 4	2 9 0	2 13 4	3 5 3
52	1 10 11	1 1 0	1 17 4	1 10 4	2 9 4	2 14 2	3 6 9
53	1 10 11	1 1 0	1 19 11	1 12 9	2 10 3	2 14 8	3 7 9
54	1 13 7	1 2 8	1 19 11	1 12 9	2 11 2	2 15 8	3 8 3
55	1 13 7	1 2 8	1 19 11	1 12 9	2 11 6	2 17 1	3 10 1
56	1 13 7	1 2 8	2 1 1	1 13 1	2 14 3	2 17 6	3 11 1
57	1 13 7	1 2 8	2 1 1	1 13 1	2 14 11	2 18 0	3 11 6
58	1 15 10	1 4 6	2 1 1	1 13 1	2 15 6	3 0 6	3 13 5
59	1 15 10	1 4 6	2 3 9	1 15 7	2 16 0	3 1 6	3 14 11
60	1 15 10	1 4 6	2 3 9	1 15 7	2 16 4	3 1 11	3 16 6

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No. 15,*

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
61	1 5 10	1 8 6	1 10 0	1 10 8	1 7 6	1 15 10
62	1 6 4	1 9 4	1 10 10	1 11 8	1 8 6	1 17 7
63	1 6 4	1 9 4	1 10 10	1 11 8	1 8 6	1 17 7
64	1 7 4	1 10 0	1 11 6	1 11 8	1 8 6	1 17 7
65	1 7 4	1 10 0	1 11 6	1 11 8	1 8 6	1 17 7
66	1 8 6	1 11 8	1 13 4	1 11 8	1 8 6	1 18 6
67	1 8 6	1 11 8	1 13 4	1 13 0	1 10 0	1 18 10
68	1 9 0	1 12 6	1 14 2	1 13 0	1 10 0	1 18 10
69	1 9 0	1 12 6	1 14 2	1 13 0	1 10 0	1 18 10
70	1 9 8	1 13 0	1 14 8	1 13 8	1 10 8	2 0 10
71	1 10 8	1 13 8	1 15 4	1 13 8	1 10 8	2 0 10
72	1 10 8	1 13 8	1 15 4	1 15 10	1 12 4	2 1 11
73	1 11 4	1 14 6	1 16 4	1 15 10	1 12 4	2 1 11
74	1 11 4	1 14 6	1 16 4	1 15 10	1 12 4	2 2 6
75	1 12 2	1 15 10	1 17 10	1 17 0	1 13 4	2 3 2
76	1 12 2	1 15 10	1 17 10	1 17 0	1 13 4	2 3 2
77	1 12 2	1 16 6	1 18 6	1 17 0	1 13 4	2 3 2
78	1 12 2	1 16 6	1 18 6	1 17 10	1 13 8	2 5 6
79	1 12 2	1 17 10	1 19 8	1 17 10	1 13 8	2 5 6
80	1 12 2	1 17 10	1 19 8	1 17 10	1 13 8	2 5 6
81	1 12 2	1 18 6	2 0 4	1 17 10	1 13 8	2 5 6
82	1 12 2	1 18 6	2 0 4	1 17 10	1 13 8	2 5 6
83	1 12 2	1 18 6	2 0 4	1 18 10	1 14 10	2 6 10
84	1 12 2	1 18 6	2 0 4	1 18 10	1 14 10	2 6 10
85	1 13 4	1 18 10	2 0 10	1 18 10	1 14 10	2 6 10
86	1 13 4	1 18 10	2 0 10	1 18 10	1 14 10	2 6 10
87	1 13 4	1 18 10	2 0 10	2 0 4	1 16 6	2 9 0
88	1 13 4	1 18 10	2 0 10	2 0 4	1 16 6	2 9 0
89	1 13 4	1 18 10	2 0 10	2 0 4	1 16 6	2 9 0
90	1 13 4	2 0 4	2 2 6	2 0 4	1 16 6	2 9 0
91	1 13 4	2 0 4	2 2 6	2 1 6	1 17 4	2 9 6
92	1 13 4	2 0 4	2 2 6	2 1 6	1 17 4	2 9 6
93	1 13 4	2 0 4	2 2 6	2 1 6	1 17 4	2 9 6
94	1 13 4	2 0 10	2 2 10	2 1 6	1 17 4	2 9 6
95	1 14 0	2 0 10	2 2 10	2 2 6	1 18 0	2 10 10
96	1 14 0	2 0 10	2 2 10	2 2 6	1 18 0	2 10 10
97	1 14 0	2 0 10	2 2 10	2 2 6	1 18 0	2 10 10
98	1 14 0	2 1 6	2 3 8	2 2 6	1 18 0	2 10 10
99	1 14 0	2 1 6	2 3 8	2 2 6	1 18 0	2 10 10
100	1 14 0	2 2 6	2 4 6	2 2 6	1 18 0	2 13 2
101	1 14 0	2 2 6	2 4 6	2 2 6	1 18 0	2 13 2
102	1 14 0	2 4 2	2 6 6	2 2 6	1 18 0	2 13 2
103	1 14 0	2 4 2	2 6 6	2 2 6	1 18 0	2 13 2
104	1 14 0	2 5 0	2 7 0	2 2 6	1 18 0	2 13 2
105	1 14 6	2 5 0	2 7 0	2 4 2	1 19 8	2 13 10
106	1 14 6	2 5 0	2 7 0	2 4 2	1 19 8	2 13 10
107	1 14 6	2 5 0	2 7 0	2 4 2	1 19 8	2 13 10
108	1 14 6	2 5 6	2 7 8	2 4 2	1 19 8	2 13 10
109	1 14 6	2 5 6	2 7 8	2 4 2	1 19 8	2 13 10
110	1 14 6	2 5 6	2 7 8	2 5 6	2 0 10	2 15 6

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES						
	A ‡ Minimum 2 tons	AA Smalls Minimum	B ‡ Minimum 1 ton	BB ‡ Minimum 1 ton	C ‡ Mini mum 1 ton	1 Smalls Minimum	2 Smalls Minimum
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
61	1 15 10	1 4 6	2 3 9	1 15 7	2 17 5	3 3 10	3 17 11
62	1 17 7	1 5 5	2 5 3	1 16 7	2 18 6	3 4 11	3 19 3
63	1 17 7	1 5 5	2 5 3	1 16 7	2 19 7	3 5 3	3 19 10
64	1 17 7	1 5 5	2 5 3	1 16 7	3 0 3	3 6 9	4 0 8
65	1 17 7	1 5 5	2 7 11	1 18 8	3 1 1	3 7 2	4 3 3
66	1 19 2	1 6 7	2 7 11	1 18 8	3 2 1	3 7 9	4 3 8
67	1 19 2	1 6 7	2 7 11	1 18 8	3 2 4	3 9 8	4 4 7
68	1 19 2	1 6 7	2 9 0	1 19 9	3 3 1	3 10 1	4 6 2
69	1 19 2	1 6 7	2 9 0	1 19 9	3 4 2	3 10 9	4 7 0
70	2 1 11	1 8 7	2 9 0	1 19 9	3 5 3	3 11 6	4 8 5
71	2 1 11	1 8 7	2 11 5	2 1 11	3 5 7	3 12 1	4 10 0
72	2 1 11	1 8 7	2 11 5	2 1 11	3 6 4	3 13 5	4 11 6
73	2 1 11	1 8 7	2 11 5	2 1 11	3 6 6	3 14 11	4 12 10
74	2 3 2	1 9 6	2 13 0	2 3 0	3 7 2	3 16 6	4 13 10
75	2 3 2	1 9 6	2 13 0	2 3 0	3 9 5	3 16 11	4 14 2
76	2 3 2	1 9 6	2 13 0	2 3 0	3 11 1	3 18 4	4 16 2
77	2 3 2	1 9 6	2 14 11	2 4 8	3 11 6	3 19 3	4 17 2
78	2 6 0	1 10 11	2 14 11	2 4 8	3 12 7	3 19 10	4 17 9
79	2 6 0	1 10 11	2 14 11	2 4 8	3 13 2	4 0 8	4 19 2
80	2 6 0	1 10 11	2 17 0	2 6 3	3 14 3	4 1 3	5 0 8
81	2 6 0	1 10 11	2 17 0	2 6 3	3 14 8	4 3 3	5 1 1
82	2 7 6	1 12 2	2 17 0	2 6 3	3 15 2	4 4 3	5 3 5
83	2 7 6	1 12 2	2 19 0	2 7 11	3 16 5	4 4 7	5 5 0
84	2 7 6	1 12 2	2 19 0	2 7 11	3 17 2	4 5 1	5 6 4
85	2 7 6	1 12 2	2 19 0	2 7 11	3 17 6	4 7 0	5 7 5
86	2 10 3	1 14 5	3 1 1	2 9 8	3 18 3	4 7 6	5 8 10
87	2 10 3	1 14 5	3 1 1	2 9 8	3 19 8	4 8 5	5 9 8
88	2 10 3	1 14 5	3 1 1	2 9 8	4 0 3	4 10 0	5 10 9
89	2 10 3	1 14 5	3 3 1	2 11 4	4 0 7	4 11 6	5 12 2
90	2 11 5	1 14 9	3 3 1	2 11 4	4 1 3	4 12 5	5 12 7
91	2 11 5	1 14 9	3 3 1	2 11 4	4 2 4	4 13 5	5 14 2
92	2 11 5	1 14 9	3 5 3	2 13 0	4 2 11	4 13 10	5 15 6
93	2 11 5	1 14 9	3 5 3	2 13 0	4 4 6	4 14 2	5 16 7
94	2 14 0	1 16 2	3 5 3	2 13 0	4 5 7	4 16 2	5 18 5
95	2 14 0	1 16 2	3 6 11	2 14 1	4 6 5	4 16 8	5 19 5
96	2 14 0	1 16 2	3 6 11	2 14 1	4 7 9	4 17 2	6 0 5
97	2 14 0	1 16 2	3 6 11	2 14 1	4 8 4	4 18 2	6 1 9
98	2 15 8	1 17 8	3 9 5	2 16 5	4 9 3	4 19 2	6 3 9
99	2 15 8	1 17 8	3 9 5	2 16 5	4 9 10	5 0 0	6 4 5
100	2 15 8	1 17 8	3 9 5	2 16 5	4 11 3	5 1 1	6 5 9
101	2 16 8	1 18 5	3 9 11	2 16 5	4 11 7	5 3 0	6 7 1
102	2 16 8	1 18 5	3 10 1	2 17 0	4 12 2	5 3 5	6 7 7
103	2 16 11	1 18 10	3 10 8	2 17 1	4 12 6	5 5 0	6 9 1
104	2 16 11	1 18 10	3 10 8	2 17 1	4 13 9	5 6 4	6 10 8
105	2 17 8	1 18 10	3 11 0	2 17 7	4 15 4	5 6 11	6 12 0
106	2 17 8	1 18 10	3 11 8	2 18 4	4 15 8	5 7 10	6 12 11
107	2 18 5	1 19 8	3 12 3	2 18 11	4 16 5	5 8 10	6 14 0
108	2 18 5	1 19 8	3 13 4	2 19 4	4 16 10	5 9 8	6 15 4
109	2 19 0	2 0 2	3 13 7	2 19 8	4 17 7	5 10 9	6 16 9
110	2 19 3	2 0 2	3 13 7	2 19 8	4 18 7	5 11 3	6 18 4

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No.* 15.

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
111	1 14 6	2 6 0	2 8 2	2 5 6	2 0 10	2 15 6
112	1 14 6	2 6 0	2 8 2	2 5 6	2 0 10	2 15 6
113	1 14 6	2 6 10	2 9 2	2 5 6	2 0 10	2 15 6
114	1 14 6	2 6 10	2 9 2	2 5 6	2 0 10	2 15 6
115	1 15 4	2 6 10	2 9 2	2 6 0	2 1 6	2 17 2
116	1 15 4	2 7 6	2 9 10	2 6 0	2 1 6	2 17 2
117	1 15 4	2 7 6	2 9 10	2 6 0	2 1 6	2 17 2
118	1 15 10	2 9 6	2 11 10	2 6 0	2 1 6	2 17 2
119	1 15 10	2 9 6	2 11 10	2 6 0	2 1 6	2 17 2
120	1 15 10	2 9 6	2 11 10	2 6 0	2 1 6	2 17 2
121	1 17 4	2 9 6	2 11 10	2 6 0	2 1 6	2 17 2
122	1 17 4	2 10 0	2 12 8	2 6 0	2 1 6	2 17 2
123	1 17 4	2 10 0	2 12 8	2 6 0	2 1 6	2 17 2
124	1 17 4	2 10 10	2 13 2	2 6 0	2 1 6	2 17 2
125	1 18 2	2 10 10	2 13 2	2 7 6	2 2 8	2 18 4
126	1 18 2	2 10 10	2 13 2	2 7 6	2 2 8	2 18 4
127	1 18 2	2 11 6	2 13 10	2 7 6	2 2 8	2 18 4
128	1 18 8	2 11 10	2 14 4	2 7 6	2 2 8	2 18 4
129	1 18 8	2 11 10	2 14 4	2 7 6	2 2 8	2 18 4
130	1 18 8	2 11 10	2 14 4	2 7 6	2 2 8	2 18 4
131	1 19 6	2 13 2	2 16 0	2 7 6	2 2 8	2 18 4
132	1 19 6	2 13 2	2 16 0	2 7 6	2 2 8	2 18 4
133	1 19 6	2 13 2	2 16 0	2 7 6	2 2 8	2 18 4
134	1 19 6	2 13 10	2 16 6	2 7 6	2 2 8	2 18 4
135	2 0 2	2 13 10	2 16 6	2 9 0	2 3 10	2 19 10
136	2 0 2	2 13 10	2 16 6	2 9 0	2 3 10	2 19 10
137	2 0 2	2 15 6	2 18 2	2 9 0	2 3 10	2 19 10
138	2 0 6	2 15 6	2 18 2	2 9 0	2 3 10	2 19 10
139	2 0 6	2 16 0	2 18 10	2 9 0	2 3 10	2 19 10
140	2 0 6	2 16 0	2 18 10	2 9 0	2 3 10	2 19 10
141	2 2 0	2 16 0	2 18 10	2 9 0	2 3 10	2 19 10
142	2 2 0	2 17 2	3 0 0	2 9 0	2 3 10	2 19 10
143	2 2 0	2 17 2	3 0 0	2 9 0	2 3 10	2 19 10
144	2 2 0	2 17 2	3 0 0	2 9 0	2 3 10	2 19 10
145	2 2 0	2 17 2	3 0 0	2 10 0	2 5 2	3 2 0
146	2 2 6	2 18 0	3 0 6	2 10 0	2 5 2	3 2 0
147	2 2 6	2 18 0	3 0 6	2 10 0	2 5 2	3 2 0
148	2 2 6	2 18 4	3 1 2	2 10 0	2 5 2	3 2 0
149	2 2 6	2 18 4	3 1 2	2 10 0	2 5 2	3 2 0
150	2 3 6	2 18 4	3 1 2	2 10 0	2 5 2	3 2 0
151	2 3 6	2 19 0	3 2 2	2 10 0	2 5 2	3 2 0
152	2 3 6	2 19 0	3 2 2	2 10 0	2 5 2	3 2 0
153	2 3 6	2 19 0	3 2 2	2 10 0	2 5 2	3 2 0
154	2 3 6	2 19 0	3 2 2	2 10 0	2 5 2	3 2 0
155	2 4 2	2 19 0	3 2 2	2 10 10	2 5 6	3 3 0
156	2 4 2	2 19 10	3 2 8	2 10 10	2 5 6	3 3 0
157	2 4 2	2 19 10	3 2 8	2 10 10	2 5 6	3 3 0
158	2 4 2	3 2 0	3 5 2	2 10 10	2 5 6	3 3 0
159	2 4 2	3 2 0	3 5 2	2 10 10	2 5 6	3 3 0
160	2 5 0	3 2 0	3 5 2	2 10 10	2 5 6	3 3 0

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																	
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Minimum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
111	2	19	10	2	0	3	3	14	9	3	0	8	4	19	8	5	12	2	6	18	9
112	2	19	10	2	0	3	3	14	9	3	0	8	5	0	5	5	13	7	7	0	8
113	3	0	8	2	1	2	3	15	0	3	0	11	5	1	5	5	14	2	7	1	8
114	3	0	8	2	1	2	3	15	7	3	1	4	5	2	6	5	14	7	7	2	6
115	3	1	4	2	1	9	3	16	3	3	2	0	5	2	8	5	16	7	7	4	2
116	3	1	4	2	1	9	3	16	11	3	2	5	5	5	2	5	17	1	7	6	2
117	3	1	11	2	2	0	3	17	9	3	3	1	5	6	3	5	18	5	7	6	6
118	3	1	11	2	2	0	3	17	9	3	3	1	5	6	6	5	19	5	7	8	0
119	3	2	4	2	2	6	3	18	1	3	3	1	5	7	4	6	0	5	7	8	10
120	3	2	8	2	2	6	3	18	5	3	3	5	5	7	11	6	0	11	7	10	10
121	3	2	8	2	2	6	3	19	0	3	3	9	5	8	5	6	3	4	7	11	11
122	3	3	4	2	2	8	3	19	3	3	4	7	5	8	8	6	3	9	7	12	9
123	3	3	4	2	2	8	4	0	5	3	5	0	5	10	6	6	4	5	7	14	3
124	3	3	8	2	3	2	4	1	1	3	5	7	5	11	1	6	5	9	7	15	4
125	3	3	8	2	3	2	4	1	7	3	6	1	5	11	8	6	6	2	7	16	8
126	3	5	0	2	4	0	4	1	7	3	6	1	5	12	4	6	7	1	7	17	1
127	3	5	0	2	4	0	4	2	3	3	6	4	5	12	8	6	8	8	7	19	1
128	3	5	5	2	4	1	4	2	3	3	6	4	5	13	8	6	9	1	8	0	6
129	3	5	10	2	4	8	4	3	1	3	7	3	5	14	9	6	9	7	8	1	1
130	3	6	3	2	4	8	4	3	4	3	7	5	5	15	6	6	12	0	8	2	6
131	3	6	3	2	4	8	4	4	4	3	8	3	5	16	7	6	12	7	8	4	3
132	3	7	0	2	5	5	4	4	4	3	8	3	5	18	0	6	12	11	8	5	4
133	3	7	0	2	5	5	4	4	7	3	8	9	5	18	9	6	14	0	8	6	11
134	3	7	9	2	6	1	4	5	4	3	9	4	5	19	3	6	15	4	8	8	3
135	3	7	9	2	6	1	4	6	7	3	10	0	6	0	4	6	16	4	8	8	9
136	3	8	6	2	6	2	4	6	7	3	10	0	6	1	11	6	17	10	8	10	3
137	3	8	6	2	6	2	4	6	9	3	10	1	6	2	6	6	18	4	8	11	2
138	3	9	1	2	6	11	4	7	7	3	11	0	6	2	10	6	18	9	8	12	1
139	3	9	4	2	7	2	4	8	4	3	11	8	6	3	6	7	0	8	8	14	1
140	3	9	4	2	7	2	4	8	4	3	11	8	6	4	7	7	1	2	8	15	1
141	3	10	3	2	7	6	4	9	4	3	12	4	6	5	9	7	1	8	8	16	6
142	3	10	3	2	7	6	4	9	4	3	12	4	6	6	1	7	3	2	8	17	5
143	3	10	7	2	8	2	4	9	11	3	12	11	6	6	8	7	4	2	8	18	5
144	3	10	7	2	8	2	4	10	1	3	13	5	6	7	9	7	5	6	8	19	10
145	3	10	10	2	8	4	4	10	7	3	13	5	6	8	10	7	6	6	9	1	3
146	3	10	10	2	8	4	4	10	7	3	13	5	6	9	7	7	7	6	9	3	3
147	3	11	6	2	8	4	4	12	1	3	14	9	6	10	8	7	8	0	9	3	8
148	3	12	11	2	9	7	4	12	7	3	15	0	6	10	10	7	8	10	9	4	8
149	3	13	5	2	9	7	4	13	0	3	15	7	6	11	6	7	10	10	9	6	8
150	3	13	5	2	9	7	4	13	0	3	15	7	6	12	9	7	11	4	9	7	7
151	3	13	7	2	9	10	4	13	11	3	16	1	6	13	10	7	11	11	9	8	8
152	3	13	7	2	9	10	4	14	4	3	16	7	6	14	5	7	12	4	9	9	6
153	3	13	7	2	9	10	4	14	9	3	16	7	6	16	0	7	12	9	9	10	6
154	3	14	2	2	10	3	4	15	5	3	17	8	6	16	0	7	14	3	9	11	6
155	3	14	7	2	10	9	4	16	0	3	17	9	6	16	7	7	14	8	9	12	0
156	3	14	7	2	10	9	4	16	0	3	17	9	6	16	11	7	15	4	9	13	5
157	3	14	11	2	10	9	4	16	8	3	18	5	6	18	0	7	15	9	9	14	10
158	3	14	11	2	10	9	4	17	0	3	19	0	6	18	0	7	16	8	9	15	10
159	3	15	10	2	11	5	4	17	8	3	19	3	6	19	3	7	17	1	9	16	3
160	3	17	0	2	12	0	4	18	0	3	19	5	7	0	2	7	17	9	9	17	2

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No.* 15.

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
161	2 5 0	3 2 0	3 5 2	2 10 10	2 5 6	3 3 0
162	2 5 0	3 2 4	3 5 8	2 10 10	2 5 6	3 3 0
163	2 5 0	3 2 4	3 5 8	2 10 10	2 5 6	3 3 0
164	2 5 0	3 2 4	3 5 8	2 10 10	2 5 6	3 3 0
165	2 5 0	3 2 4	3 5 8	2 11 10	2 6 10	3 3 10
166	2 5 0	3 2 4	3 5 8	2 11 10	2 6 10	3 3 10
167	2 6 2	3 3 0	3 6 2	2 11 10	2 6 10	3 3 10
168	2 6 2	3 3 0	3 6 2	2 11 10	2 6 10	3 3 10
169	2 6 2	3 3 0	3 6 2	2 11 10	2 6 10	3 3 10
170	2 6 2	3 3 0	3 6 2	2 11 10	2 6 10	3 3 10
171	2 6 2	3 3 0	3 6 2	2 11 10	2 6 10	3 3 10
172	2 6 10	3 3 10	3 6 8	2 11 10	2 6 10	3 3 10
173	2 6 10	3 3 10	3 6 8	2 11 10	2 6 10	3 3 10
174	2 6 10	3 3 10	3 6 8	2 11 10	2 6 10	3 3 10
175	2 7 6	3 3 10	3 6 8	2 13 10	2 8 2	3 5 8
176	2 7 6	3 3 10	3 6 8	2 13 10	2 8 2	3 5 8
177	2 7 6	3 4 4	3 7 8	2 13 10	2 8 2	3 5 8
178	2 7 6	3 4 4	3 7 8	2 13 10	2 8 2	3 5 8
179	2 7 6	3 4 4	3 7 8	2 13 10	2 8 2	3 5 8
180	2 8 2	3 4 4	3 7 8	2 13 10	2 8 2	3 5 8
181	2 8 2	3 4 4	3 7 8	2 13 10	2 8 2	3 5 8
182	2 8 2	3 5 8	3 8 10	2 13 10	2 8 2	3 5 8
183	2 8 2	3 5 8	3 8 10	2 13 10	2 8 2	3 5 8
184	2 8 2	3 5 8	3 8 10	2 13 10	2 8 2	3 5 8
185	2 8 8	3 5 8	3 8 10	2 14 4	2 9 2	3 6 8
186	2 8 8	3 5 8	3 8 10	2 14 4	2 9 2	3 6 8
187	2 8 8	3 6 8	3 10 2	2 14 4	2 9 2	3 6 8
188	2 8 8	3 6 8	3 10 2	2 14 4	2 9 2	3 6 8
189	2 8 8	3 6 8	3 10 2	2 14 4	2 9 2	3 6 8
190	2 8 8	3 6 8	3 10 2	2 14 4	2 9 2	3 6 8
191	2 8 8	3 6 8	3 10 2	2 16 0	2 10 2	3 8 4
192	2 9 6	3 7 8	3 10 10	2 16 0	2 10 2	3 8 4
193	2 9 6	3 7 8	3 10 10	2 16 0	2 10 2	3 8 4
194	2 9 6	3 7 8	3 10 10	2 16 0	2 10 2	3 8 4
195	2 9 6	3 7 8	3 10 10	2 16 0	2 10 2	3 8 4
196	2 9 6	3 7 8	3 10 10	2 16 0	2 10 2	3 8 4
197	2 11 0	3 8 4	3 11 10	2 16 0	2 10 2	3 8 4
198	2 11 0	3 8 4	3 11 10	2 16 0	2 10 2	3 8 4
199	2 11 0	3 8 4	3 11 10	2 16 0	2 10 2	3 8 4
200	2 11 0	3 8 4	3 11 10	2 16 0	2 10 2	3 8 4
201	2 11 8	3 8 10	3 12 6	2 17 2	2 11 6	3 10 2
202	2 11 8	3 8 10	3 12 6	2 17 2	2 11 6	3 10 2
203	2 11 8	3 8 10	3 12 6	2 17 2	2 11 6	3 10 2
204	2 11 8	3 8 10	3 12 6	2 17 2	2 11 6	3 10 2
205	2 11 8	3 10 2	3 13 8	2 17 2	2 11 6	3 10 2
206	2 11 8	3 10 2	3 13 8	2 17 2	2 11 6	3 10 2
207	2 11 8	3 10 10	3 14 4	2 17 2	2 11 6	3 10 2
208	2 11 8	3 10 10	3 14 4	2 17 2	2 11 6	3 10 2
209	2 11 8	3 10 10	3 14 4	2 17 2	2 11 6	3 10 2
210	2 11 8	3 10 10	3 14 4	2 17 2	2 11 6	3 10 2

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																	
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Minimum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
161	3	17	0	2	12	0	4	19	1	4	0	4	7	1	6	7	19	1	9	17	10
162	3	17	3	2	12	2	4	19	9	4	0	7	7	1	6	8	0	0	9	19	2
163	3	17	3	2	12	2	5	0	1	4	1	4	7	1	10	8	0	6	10	0	3
164	3	17	8	2	12	7	5	0	1	4	1	4	7	2	8	8	1	1	10	1	2
165	3	18	6	2	12	10	5	0	4	4	1	7	7	2	8	8	1	7	10	2	6
166	3	18	6	2	12	10	5	1	0	4	1	9	7	3	8	8	2	6	10	4	1
167	3	18	9	2	13	5	5	1	3	4	2	3	7	4	0	8	3	10	10	4	5
168	3	18	9	2	13	5	5	2	4	4	3	1	7	4	8	8	4	3	10	5	0
169	3	19	4	2	13	11	5	3	0	4	3	3	7	4	8	8	5	4	10	6	4
170	4	0	0	2	14	0	5	3	0	4	3	3	7	5	7	8	5	10	10	7	0
171	4	0	0	2	14	0	5	3	4	4	3	7	7	6	2	8	6	11	10	8	4
172	4	0	9	2	14	7	5	3	9	4	4	0	7	7	3	8	7	9	10	9	11
173	4	0	9	2	14	7	5	4	3	4	4	5	7	7	3	8	8	3	10	10	4
174	4	1	8	2	15	5	5	5	1	4	5	4	7	7	11	8	8	9	10	10	9
175	4	1	8	2	15	5	5	5	4	4	5	8	7	8	8	8	9	3	10	12	9
176	4	2	0	2	15	5	5	5	11	4	5	11	7	9	9	8	10	3	10	13	3
177	4	2	0	2	15	5	5	6	8	4	6	9	7	9	9	8	10	9	10	13	9
178	4	2	4	2	15	8	5	6	8	4	6	9	7	11	7	8	11	2	10	15	8
179	4	2	11	2	16	3	5	7	8	4	7	0	7	12	1	8	12	1	10	16	2
180	4	2	11	2	16	3	5	7	11	4	7	3	7	12	1	8	13	5	10	17	2
181	4	3	2	2	16	6	5	8	5	4	7	11	7	12	8	8	14	1	10	18	7
182	4	3	2	2	16	6	5	8	9	4	8	1	7	13	4	8	14	6	10	19	0
183	4	4	6	2	17	3	5	9	1	4	8	8	7	13	11	8	15	1	10	19	7
184	4	5	2	2	17	9	5	10	8	4	9	11	7	14	10	8	15	5	11	0	11
185	4	5	2	2	17	9	5	10	11	4	10	1	7	15	7	8	17	1	11	2	5
186	4	5	8	2	17	9	5	10	11	4	10	1	7	16	9	8	17	5	11	3	5
187	4	5	8	2	17	9	5	11	11	4	10	7	7	17	0	8	17	11	11	4	5
188	4	5	10	2	17	10	5	11	11	4	10	7	7	17	0	8	18	5	11	5	5
189	4	6	4	2	18	5	5	12	1	4	10	9	7	18	1	8	19	10	11	6	2
190	4	6	4	2	18	5	5	12	11	4	11	5	7	18	9	9	0	10	11	7	2
191	4	7	3	2	19	0	5	13	4	4	12	0	7	18	11	9	1	3	11	7	9
192	4	7	3	2	19	0	5	13	4	4	12	0	7	18	11	9	2	4	11	8	9
193	4	7	6	2	19	3	5	14	4	4	12	7	7	19	8	9	3	3	11	10	8
194	4	7	6	2	19	3	5	14	8	4	13	0	8	2	0	9	3	8	11	11	7
195	4	8	7	3	0	0	5	15	0	4	13	7	8	2	0	9	4	3	11	12	1
196	4	8	7	3	0	0	5	15	11	4	13	11	8	2	9	9	4	8	11	13	0
197	4	8	9	3	0	0	5	16	3	4	14	4	8	2	11	9	6	3	11	13	5
198	4	8	9	3	0	0	5	16	9	4	14	7	8	3	8	9	6	8	11	15	0
199	4	9	7	3	0	5	5	17	0	4	14	11	8	3	8	9	7	7	11	15	11
200	4	10	2	3	1	3	5	17	0	4	14	11	8	4	2	9	8	0	11	16	4
201	4	10	11	3	1	3	5	17	8	4	15	8	8	5	1	9	8	8	11	17	4
202	4	10	11	3	1	3	5	18	9	4	16	4	8	6	10	9	9	6	11	19	3
203	4	11	1	3	1	10	5	18	9	4	16	4	8	6	10	9	10	6	11	19	10
204	4	11	1	3	1	10	5	19	0	4	16	8	8	7	3	9	10	11	12	0	4
205	4	11	1	3	1	10	5	19	0	4	16	8	8	8	0	9	11	6	12	2	2
206	4	11	11	3	2	4	5	19	8	4	16	8	8	9	0	9	12	0	12	2	9
207	4	11	11	3	2	4	5	19	8	4	16	8	8	9	0	9	12	10	12	4	2
208	4	11	11	3	2	4	6	0	0	4	17	0	8	9	4	9	13	5	12	5	6
209	4	12	9	3	2	8	6	0	0	4	17	0	8	10	1	9	14	10	12	6	2
210	4	12	9	3	2	8	6	0	3	4	17	8	8	10	1	9	15	10	12	6	7

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No.* 15.

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
211	2 13 0	3 11 8	3 15 0	2 18 0	2 11 10	3 11 8
212	2 13 0	3 11 8	3 15 0	2 18 0	2 11 10	3 11 8
213	2 13 0	3 12 8	3 16 6	2 18 0	2 11 10	3 11 8
214	2 13 0	3 12 8	3 16 6	2 18 0	2 11 10	3 11 8
215	2 13 0	3 12 8	3 16 6	2 18 0	2 11 10	3 11 8
216	2 13 0	3 12 8	3 16 6	2 18 0	2 11 10	3 11 8
217	2 13 0	3 14 0	3 17 8	2 18 0	2 11 10	3 11 8
218	2 13 0	3 14 0	3 17 8	2 18 0	2 11 10	3 11 8
219	2 13 0	3 14 0	3 17 8	2 18 0	2 11 10	3 11 8
220	2 13 0	3 14 0	3 17 8	2 18 0	2 11 10	3 11 8
221	2 13 6	3 14 0	3 17 8	2 19 0	2 13 2	3 12 8
222	2 13 6	3 14 10	3 18 6	2 19 0	2 13 2	3 12 8
223	2 13 6	3 14 10	3 18 6	2 19 0	2 13 2	3 12 8
224	2 13 6	3 14 10	3 18 6	2 19 0	2 13 2	3 12 8
225	2 13 6	3 14 10	3 18 6	2 19 0	2 13 2	3 12 8
226	2 13 6	3 14 10	3 18 6	2 19 0	2 13 2	3 12 8
227	2 13 6	3 15 8	3 19 0	2 19 0	2 13 2	3 12 8
228	2 13 6	3 15 8	3 19 0	2 19 0	2 13 2	3 12 8
229	2 13 6	3 15 8	3 19 0	2 19 0	2 13 2	3 12 8
230	2 13 6	3 15 8	3 19 0	2 19 0	2 13 2	3 12 8
231	2 15 6	3 15 8	3 19 0	2 19 10	2 13 10	3 14 0
232	2 15 6	3 16 2	4 0 0	2 19 10	2 13 10	3 14 0
233	2 15 6	3 16 2	4 0 0	2 19 10	2 13 10	3 14 0
234	2 15 6	3 16 2	4 0 0	2 19 10	2 13 10	3 14 0
235	2 15 6	3 16 2	4 0 0	2 19 10	2 13 10	3 14 0
236	2 15 6	3 16 2	4 0 0	2 19 10	2 13 10	3 14 0
237	2 15 6	3 16 8	4 0 6	2 19 10	2 13 10	3 14 0
238	2 15 6	3 16 8	4 0 6	2 19 10	2 13 10	3 14 0
239	2 15 6	3 16 8	4 0 6	2 19 10	2 13 10	3 14 0
240	2 15 6	3 16 8	4 0 6	2 19 10	2 13 10	3 14 0
241	2 16 4	3 16 8	4 0 6	3 0 4	2 14 2	3 14 10
242	2 16 4	3 17 4	4 1 2	3 0 4	2 14 2	3 14 10
243	2 16 4	3 17 4	4 1 2	3 0 4	2 14 2	3 14 10
244	2 16 4	3 17 4	4 1 2	3 0 4	2 14 2	3 14 10
245	2 16 4	3 17 4	4 1 2	3 0 4	2 14 2	3 14 10
246	2 16 4	3 17 4	4 1 2	3 0 4	2 14 2	3 14 10
247	2 16 4	3 19 0	4 3 0	3 0 4	2 14 2	3 14 10
248	2 16 4	3 19 0	4 3 0	3 0 4	2 14 2	3 14 10
249	2 16 4	3 19 0	4 3 0	3 0 4	2 14 2	3 14 10
250	2 16 4	4 0 0	4 4 0	3 0 4	2 14 2	3 14 10
251	2 16 8	4 0 0	4 4 0	3 2 0	2 15 8	3 15 8
252	2 16 8	4 0 0	4 4 0	3 2 0	2 15 8	3 15 8
253	2 16 8	4 0 0	4 4 0	3 2 0	2 15 8	3 15 8
254	2 16 8	4 0 0	4 4 0	3 2 0	2 15 8	3 15 8
255	2 16 8	4 0 6	4 4 6	3 2 0	2 15 8	3 15 8
256	2 16 8	4 0 6	4 4 6	3 2 0	2 15 8	3 15 8
257	2 16 8	4 0 6	4 4 6	3 2 0	2 15 8	3 15 8
258	2 16 8	4 0 6	4 4 6	3 2 0	2 15 8	3 15 8
259	2 16 8	4 0 6	4 4 6	3 2 0	2 15 8	3 15 8
260	2 16 8	4 1 2	4 5 4	3 2 0	2 15 8	3 15 8

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																							
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Minimum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
211	4	13	3	3	3	4	6	1	8	4	18	9	8	10	9	9	16	3	12	8	7
212	4	13	3	3	3	4	6	2	1	4	19	0	8	13	0	9	16	9	12	8	10
213	4	13	10	3	3	8	6	2	1	4	19	0	8	13	9	9	17	2	12	9	6
214	4	13	10	3	3	8	6	2	5	4	19	4	8	13	9	9	17	10	12	10	6
215	4	14	5	3	3	8	6	2	5	4	19	4	8	14	4	9	19	2	12	11	4
216	4	14	5	3	3	8	6	3	0	4	19	11	8	15	0	9	19	8	12	12	10
217	4	14	5	3	3	8	6	3	0	4	19	11	8	15	5	10	0	3	12	13	10
218	4	14	7	3	3	11	6	3	5	5	0	1	8	15	5	10	1	2	12	14	8
219	4	14	7	3	3	11	6	3	5	5	0	1	8	17	0	10	2	6	12	15	4
220	4	14	7	3	3	11	6	4	3	5	0	4	8	17	9	10	3	0	12	16	4
221	4	15	1	3	4	5	6	4	7	5	1	0	8	18	3	10	4	1	12	18	2
222	4	15	1	3	4	5	6	4	7	5	1	0	8	18	3	10	4	5	12	18	8
223	4	16	6	3	5	1	6	5	4	5	1	9	8	18	10	10	5	0	12	19	8
224	4	16	6	3	5	1	6	5	4	5	1	9	8	19	10	10	6	1	13	0	1
225	4	16	9	3	5	7	6	5	8	5	1	11	8	19	10	10	6	4	13	2	1
226	4	17	2	3	5	10	6	5	8	5	1	11	9	0	2	10	7	0	13	3	0
227	4	17	2	3	5	10	6	6	3	5	2	5	9	0	11	10	8	0	13	3	7
228	4	17	2	3	5	10	6	6	3	5	2	5	9	2	4	10	8	4	13	4	5
229	4	17	8	3	5	10	6	7	1	5	3	1	9	2	4	10	9	11	13	5	5
230	4	17	8	3	5	10	6	7	1	5	3	1	9	3	5	10	10	4	13	6	3
231	4	18	7	3	6	7	6	7	11	5	3	8	9	4	10	10	10	9	13	7	11
232	4	18	7	3	6	7	6	7	11	5	3	8	9	5	2	10	11	3	13	8	10
233	4	18	7	3	6	7	6	8	4	5	3	9	9	5	2	10	12	9	13	9	3
234	4	19	1	3	7	0	6	8	4	5	3	9	9	5	10	10	13	3	13	9	11
235	4	19	1	3	7	0	6	9	5	5	5	1	9	6	3	10	13	9	13	12	2
236	4	19	1	3	7	0	6	9	5	5	5	1	9	7	10	10	14	2	13	12	7
237	4	19	6	3	7	0	6	9	11	5	5	4	9	7	10	10	15	8	13	13	2
238	5	0	3	3	7	11	6	9	11	5	5	4	9	8	5	10	16	2	13	15	1
239	5	0	3	3	7	11	6	10	5	5	5	7	9	9	6	10	17	2	13	15	8
240	5	0	3	3	7	11	6	10	5	5	5	7	9	9	6	10	17	7	13	16	1
241	5	0	10	3	8	3	6	10	9	5	6	4	9	10	2	10	18	7	13	17	6
242	5	0	10	3	8	3	6	11	3	5	6	7	9	11	0	10	19	0	13	18	0
243	5	1	6	3	8	10	6	11	3	5	6	7	9	12	2	10	19	7	13	18	5
244	5	1	6	3	8	10	6	11	9	5	7	0	9	12	2	10	19	11	14	0	5
245	5	1	6	3	8	10	6	11	9	5	7	0	9	12	11	11	0	11	14	1	6
246	5	1	10	3	9	2	6	12	11	5	7	8	9	13	2	11	2	5	14	2	4
247	5	1	10	3	9	2	6	12	11	5	7	8	9	13	11	11	2	11	14	3	4
248	5	1	10	3	9	2	6	13	3	5	7	11	9	13	11	11	3	5	14	4	10
249	5	2	7	3	9	5	6	13	5	5	8	5	9	15	0	11	4	5	14	5	4
250	5	2	11	3	9	5	6	13	5	5	8	5	9	16	2	11	5	5	14	6	8
251	5	4	1	3	10	7	6	14	1	5	8	9	9	16	2	11	6	2	14	7	7
252	5	4	1	3	10	7	6	14	11	5	9	3	9	16	5	11	6	9	14	8	8
253	5	4	1	3	10	7	6	14	11	5	9	3	9	16	5	11	6	9	14	9	1
254	5	4	9	3	10	7	6	15	5	5	9	9	9	16	5	11	7	2	14	9	7
255	5	4	9	3	10	7	6	15	5	5	9	9	9	17	6	11	7	9	14	10	8
256	5	4	9	3	10	7	6	15	11	5	10	4	9	17	6	11	7	9	14	11	1
257	5	5	1	3	10	10	6	15	11	5	10	4	9	17	6	11	8	9	14	11	6
258	5	5	1	3	10	10	6	16	8	5	10	9	9	19	3	11	9	7	14	12	0
259	5	5	4	3	11	6	6	16	8	5	10	9	9	19	3	11	9	7	14	13	0
260	5	5	4	3	11	6	6	17	0	5	11	1	9	19	3	11	10	8	14	14	0

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No.* 15.

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
261	2 18 0	4 1 2	4 5 4	3 2 4	2 16 4	3 16 2
262	2 18 0	4 1 2	4 5 4	3 2 4	2 16 4	3 16 2
263	2 18 0	4 1 2	4 5 4	3 2 4	2 16 4	3 16 2
264	2 18 0	4 1 2	4 5 4	3 2 4	2 16 4	3 16 2
265	2 18 0	4 2 6	4 6 6	3 2 4	2 16 4	3 16 2
266	2 18 0	4 2 6	4 6 6	3 2 4	2 16 4	3 16 2
267	2 18 0	4 2 6	4 6 6	3 2 4	2 16 4	3 16 2
268	2 18 0	4 2 6	4 6 6	3 2 4	2 16 4	3 16 2
269	2 18 0	4 2 6	4 6 6	3 2 4	2 16 4	3 16 2
270	2 18 0	4 3 0	4 7 2	3 2 4	2 16 4	3 16 2
271	2 18 4	4 3 0	4 7 2	3 3 0	2 17 0	3 17 4
272	2 18 4	4 3 0	4 7 2	3 3 0	2 17 0	3 17 4
273	2 18 4	4 3 0	4 7 2	3 3 0	2 17 0	3 17 4
274	2 18 4	4 3 0	4 7 2	3 3 0	2 17 0	3 17 4
275	2 18 4	4 4 6	4 8 8	3 3 0	2 17 0	3 17 4
276	2 18 4	4 4 6	4 8 8	3 3 0	2 17 0	3 17 4
277	2 18 4	4 4 6	4 8 8	3 3 0	2 17 0	3 17 4
278	2 18 4	4 4 6	4 8 8	3 3 0	2 17 0	3 17 4
279	2 18 4	4 4 6	4 8 8	3 3 0	2 17 0	3 17 4
280	2 18 4	4 4 10	4 9 4	3 3 0	2 17 0	3 17 4
281	2 19 10	4 4 10	4 9 4	3 3 10	2 17 2	3 18 6
282	2 19 10	4 4 10	4 9 4	3 3 10	2 17 2	3 18 6
283	2 19 10	4 4 10	4 9 4	3 3 10	2 17 2	3 18 6
284	2 19 10	4 4 10	4 9 4	3 3 10	2 17 2	3 18 6
285	2 19 10	4 5 10	4 10 2	3 3 10	2 17 2	3 18 6
286	2 19 10	4 5 10	4 10 2	3 3 10	2 17 2	3 18 6
287	2 19 10	4 5 10	4 10 2	3 3 10	2 17 2	3 18 6
288	2 19 10	4 5 10	4 10 2	3 3 10	2 17 2	3 18 6
289	2 19 10	4 5 10	4 10 2	3 3 10	2 17 2	3 18 6
290	2 19 10	4 6 10	4 11 2	3 3 10	2 17 2	3 18 6
291	3 0 4	4 6 10	4 11 2	3 4 4	2 18 0	3 19 0
292	3 0 4	4 6 10	4 11 2	3 4 4	2 18 0	3 19 0
293	3 0 4	4 6 10	4 11 2	3 4 4	2 18 0	3 19 0
294	3 0 4	4 6 10	4 11 2	3 4 4	2 18 0	3 19 0
295	3 0 4	4 8 0	4 12 2	3 4 4	2 18 0	3 19 0
296	3 0 4	4 8 0	4 12 2	3 4 4	2 18 0	3 19 0
297	3 0 4	4 8 0	4 12 2	3 4 4	2 18 0	3 19 0
298	3 0 4	4 8 0	4 12 2	3 4 4	2 18 0	3 19 0
299	3 0 4	4 8 0	4 12 2	3 4 4	2 18 0	3 19 0
300	3 0 4	4 8 6	4 12 10	3 4 4	2 18 0	3 19 0
301	3 1 0	4 8 6	4 12 10	3 5 8	2 19 0	4 0 6
302	3 1 0	4 9 0	4 13 4	3 5 8	2 19 0	4 0 6
303	3 1 0	4 9 0	4 13 4	3 5 8	2 19 0	4 0 6
304	3 1 0	4 9 0	4 13 4	3 5 8	2 19 0	4 0 6
305	3 1 0	4 9 0	4 13 4	3 5 8	2 19 0	4 0 6
306	3 1 0	4 9 0	4 13 4	3 5 8	2 19 0	4 0 6
307	3 1 0	4 10 4	4 15 0	3 5 8	2 19 0	4 0 6
308	3 1 0	4 10 4	4 15 0	3 5 8	2 19 0	4 0 6
309	3 1 0	4 10 4	4 15 0	3 5 8	2 19 0	4 0 6
310	3 1 0	4 10 4	4 15 0	3 5 8	2 19 0	4 0 6

* *For minimum weights and alternative rates for smaller quantities, see General Condition No.* 15.

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES						
	A ‡ Minimum 2 tons	AA Smalls Minimum	B ‡ Minimum 1 ton	BB ‡ Minimum 1 ton	C ‡ Minimum 1 ton	1 Smalls Minimum	2 Smalls Minimum
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
261	5 5 4	3 11 6	6 17 11	5 11 11	9 19 11	11 11 7	14 14 6
262	5 6 1	3 11 8	6 17 11	5 11 11	9 19 11	11 11 7	14 14 11
263	5 6 1	3 11 8	6 18 3	5 12 1	10 0 4	11 12 1	14 15 4
264	5 6 1	3 11 8	6 18 3	5 12 1	10 0 4	11 12 7	14 15 10
265	5 6 3	3 11 8	6 18 7	5 12 5	10 0 4	11 12 7	14 16 11
266	5 7 5	3 12 10	6 18 7	5 12 5	10 1 0	11 13 0	14 18 3
267	5 7 5	3 12 10	6 19 4	5 12 11	10 1 0	11 13 5	14 18 10
268	5 7 5	3 12 10	6 19 7	5 13 3	10 1 0	11 13 5	15 0 3
269	5 8 2	3 13 0	6 19 7	5 13 3	10 1 3	11 15 0	15 1 2
270	5 8 2	3 13 0	7 0 9	5 13 11	10 1 3	11 15 0	15 2 2
271	5 8 6	3 13 6	7 1 0	5 14 7	10 3 0	11 15 0	15 2 9
272	5 8 6	3 13 6	7 1 0	5 14 7	10 3 0	11 15 11	15 3 7
273	5 8 6	3 13 6	7 1 7	5 14 9	10 3 0	11 16 4	15 4 1
274	5 9 0	3 13 7	7 1 7	5 14 9	10 3 7	11 16 4	15 4 7
275	5 9 5	3 14 2	7 2 0	5 15 0	10 3 7	11 17 0	15 5 0
276	5 9 5	3 14 2	7 2 0	5 15 0	10 3 7	11 17 4	15 5 6
277	5 9 5	3 14 2	7 2 3	5 15 7	10 4 1	11 18 4	15 7 0
278	5 10 4	3 14 7	7 2 3	5 15 7	10 4 1	11 19 3	15 7 5
279	5 10 7	3 14 11	7 3 4	5 16 3	10 4 6	11 19 10	15 8 0
280	5 10 7	3 14 11	7 3 4	5 16 3	10 4 6	11 19 10	15 8 5
281	5 10 7	3 14 11	7 4 1	5 16 11	10 5 11	12 0 4	15 9 0
282	5 11 2	3 15 3	7 4 5	5 16 11	10 6 3	12 0 10	15 9 5
283	5 11 2	3 15 3	7 4 5	5 16 11	10 6 3	12 0 10	15 10 5
284	5 11 2	3 15 3	7 5 0	5 17 4	10 6 3	12 2 2	15 11 4
285	5 12 0	3 15 9	7 5 9	5 18 1	10 8 0	12 2 9	15 11 9
286	5 12 0	3 15 9	7 5 9	5 18 1	10 8 0	12 2 9	15 13 3
287	5 13 1	3 16 6	7 6 3	5 18 8	10 8 0	12 4 2	15 14 3
288	5 13 1	3 16 6	7 6 3	5 18 8	10 8 4	12 5 1	15 15 3
289	5 13 1	3 16 6	7 6 5	5 18 9	10 8 4	12 5 1	15 15 9
290	5 13 3	3 16 6	7 6 5	5 18 9	10 8 4	12 5 6	15 16 7
291	5 13 10	3 17 1	7 6 11	5 19 0	10 9 0	12 6 2	15 17 2
292	5 13 10	3 17 1	7 7 9	6 0 0	10 9 0	12 6 2	15 17 7
293	5 13 10	3 17 1	7 7 9	6 0 0	10 10 2	12 6 7	15 18 3
294	5 14 3	3 17 4	7 8 8	6 0 3	10 10 2	12 7 1	15 18 7
295	5 14 3	3 17 4	7 8 8	6 0 3	10 10 2	12 7 1	16 0 0
296	5 14 3	3 17 4	7 9 1	6 0 9	10 10 6	12 8 7	16 0 11
297	5 14 7	3 17 8	7 9 1	6 0 9	10 10 6	12 8 10	16 1 7
298	5 16 2	3 18 6	7 9 4	6 1 0	10 10 6	12 8 10	16 2 0
299	5 16 2	3 18 6	7 9 4	6 1 0	10 11 2	12 9 6	16 2 5
300	5 16 2	3 18 6	7 9 11	6 1 4	10 11 2	12 9 6	16 3 5
301	5 16 5	3 18 9	7 10 4	6 1 9	10 11 7	12 9 11	16 4 0
302	5 16 5	3 18 9	7 10 4	6 1 9	10 11 7	12 10 6	16 4 11
303	5 16 9	3 18 9	7 10 7	6 2 3	10 11 7	12 11 4	16 5 5
304	5 16 9	3 18 9	7 11 11	6 3 5	10 13 4	12 11 4	16 6 3
305	5 17 6	3 19 4	7 11 11	6 3 5	10 13 4	12 11 11	16 6 9
306	5 17 6	3 19 4	7 12 7	6 3 9	10 13 4	12 12 10	16 8 9
307	5 17 9	3 19 5	7 12 7	6 3 9	10 14 1	12 13 4	16 9 10
308	5 17 9	3 19 5	7 12 9	6 3 9	10 14 1	12 13 10	16 10 1
309	5 17 9	3 19 5	7 12 9	6 3 9	10 14 1	12 14 8	16 10 9
310	5 18 6	3 19 11	7 13 1	6 4 3	10 15 2	12 14 8	16 11 2

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
311	3 1 8	4 10 4	4 15 0	3 6 2	2 19 10	4 1 2
312	3 1 8	4 11 4	4 16 4	3 6 2	2 19 10	4 1 2
313	3 1 8	4 11 4	4 16 4	3 6 2	2 19 10	4 1 2
314	3 1 8	4 11 4	4 16 4	3 6 2	2 19 10	4 1 2
315	3 1 8	4 11 4	4 16 4	3 6 2	2 19 10	4 1 2
316	3 1 8	4 11 4	4 16 4	3 6 2	2 19 10	4 1 2
317	3 1 8	4 12 2	4 16 10	3 6 2	2 19 10	4 1 2
318	3 1 8	4 12 2	4 16 10	3 6 2	2 19 10	4 1 2
319	3 1 8	4 12 2	4 16 10	3 6 2	2 19 10	4 1 2
320	3 1 8	4 12 2	4 16 10	3 6 2	2 19 10	4 1 2
321	3 2 4	4 12 2	4 16 10	3 6 8	3 0 0	4 2 6
322	3 2 4	4 12 10	4 17 4	3 6 8	3 0 0	4 2 6
323	3 2 4	4 12 10	4 17 4	3 6 8	3 0 0	4 2 6
324	3 2 4	4 12 10	4 17 4	3 6 8	3 0 0	4 2 6
325	3 2 4	4 12 10	4 17 4	3 6 8	3 0 0	4 2 6
326	3 2 4	4 12 10	4 17 4	3 6 8	3 0 0	4 2 6
327	3 2 4	4 13 4	4 18 4	3 6 8	3 0 0	4 2 6
328	3 2 4	4 13 4	4 18 4	3 6 8	3 0 0	4 2 6
329	3 2 4	4 13 4	4 18 4	3 6 8	3 0 0	4 2 6
330	3 2 4	4 13 4	4 18 4	3 6 8	3 0 0	4 2 6
331	3 2 10	4 13 4	4 18 4	3 7 8	3 0 6	4 3 0
332	3 2 10	4 14 6	4 19 0	3 7 8	3 0 6	4 3 0
333	3 2 10	4 14 6	4 19 0	3 7 8	3 0 6	4 3 0
334	3 2 10	4 14 6	4 19 0	3 7 8	3 0 6	4 3 0
335	3 2 10	4 14 6	4 19 0	3 7 8	3 0 6	4 3 0
336	3 2 10	4 14 6	4 19 0	3 7 8	3 0 6	4 3 0
337	3 2 10	4 15 0	4 19 4	3 7 8	3 0 6	4 3 0
338	3 2 10	4 15 0	4 19 4	3 7 8	3 0 6	4 3 0
339	3 2 10	4 15 0	4 19 4	3 7 8	3 0 6	4 3 0
340	3 2 10	4 15 0	4 19 4	3 7 8	3 0 6	4 3 0
341	3 4 6	4 15 0	4 19 4	3 8 10	3 2 2	4 4 0
342	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
343	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
344	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
345	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
346	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
347	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
348	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
349	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
350	3 4 6	4 16 10	5 1 6	3 8 10	3 2 2	4 4 0
351	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
352	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
353	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
354	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
355	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
356	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
357	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
358	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
359	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10
360	3 5 2	4 18 4	5 3 0	3 10 2	3 3 0	4 4 10

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																							
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Minimum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
311	5	18	6	3	19	11	7	14	0	6	4	11	10	15	2	12	15	4	16	11	9
312	5	18	6	3	19	11	7	14	0	6	4	11	10	15	4	12	15	9	16	12	1
313	5	19	0	4	0	5	7	14	4	6	5	1	10	15	4	12	15	9	16	13	8
314	5	19	0	4	0	5	7	14	4	6	5	1	10	15	4	12	16	4	16	14	1
315	6	0	1	4	1	7	7	15	3	6	6	0	10	15	11	12	18	2	16	14	6
316	6	0	1	4	1	7	7	15	3	6	6	0	10	15	11	12	18	2	16	15	0
317	6	0	1	4	1	7	7	15	11	6	6	3	10	15	11	12	18	8	16	15	11
318	6	0	5	4	1	7	7	15	11	6	6	3	10	17	3	12	19	1	16	16	6
319	6	0	9	4	1	8	7	16	3	6	6	8	10	17	3	12	19	1	16	17	0
320	6	0	9	4	1	8	7	16	3	6	6	8	10	17	3	12	19	8	16	17	11
321	6	0	9	4	1	8	7	16	11	6	7	1	10	18	6	13	0	1	16	18	4
322	6	1	8	4	2	4	7	17	4	6	7	7	10	18	6	13	0	1	16	19	5
323	6	2	0	4	2	4	7	17	11	6	7	11	10	19	2	13	2	1	16	19	11
324	6	2	0	4	2	4	7	17	11	6	7	11	10	19	2	13	2	6	17	0	11
325	6	2	0	4	2	4	7	18	1	6	8	0	10	19	2	13	2	6	17	1	4
326	6	2	7	4	2	11	7	18	1	6	8	0	10	19	7	13	3	0	17	3	4
327	6	2	7	4	2	11	7	19	4	6	9	4	10	19	7	13	3	7	17	4	2
328	6	2	7	4	2	11	7	19	4	6	9	4	10	19	7	13	3	7	17	4	8
329	6	2	10	4	3	2	7	19	11	6	9	8	11	0	6	13	4	5	17	5	3
330	6	2	10	4	3	2	7	19	11	6	9	8	11	0	6	13	4	5	17	6	2
331	6	3	7	4	3	5	8	0	4	6	9	11	11	1	4	13	4	11	17	7	2
332	6	3	7	4	3	5	8	0	9	6	10	5	11	1	4	13	5	5	17	7	6
333	6	4	9	4	4	4	8	0	9	6	10	5	11	1	4	13	6	3	17	8	2
334	6	4	9	4	4	4	8	1	0	6	10	5	11	2	0	13	6	3	17	8	7
335	6	4	11	4	4	7	8	1	0	6	10	5	11	2	0	13	6	11	17	9	6
336	6	4	11	4	4	7	8	1	7	6	10	9	11	3	1	13	7	11	17	9	11
337	6	4	11	4	4	7	8	1	7	6	10	9	11	3	9	13	7	11	17	10	6
338	6	5	8	4	4	7	8	2	4	6	11	9	11	3	9	13	8	10	17	11	6
339	6	5	8	4	4	7	8	2	4	6	11	9	11	3	9	13	9	3	17	12	6
340	6	5	8	4	4	7	8	3	0	6	12	1	11	4	6	13	9	3	17	12	11
341	6	6	0	4	5	5	8	3	7	6	12	9	11	4	6	13	9	11	17	13	4
342	6	6	0	4	5	5	8	4	1	6	13	1	11	5	7	13	11	3	17	13	10
343	6	6	3	4	5	8	8	4	1	6	13	1	11	5	7	13	11	3	17	14	5
344	6	6	3	4	5	8	8	4	11	6	13	5	11	5	7	13	12	2	17	15	10
345	6	7	9	4	6	4	8	4	11	6	13	5	11	5	7	13	12	7	17	16	9
346	6	8	3	4	6	10	8	5	1	6	13	5	11	5	7	13	12	7	17	17	9
347	6	8	3	4	6	10	8	5	1	6	13	5	11	5	7	13	13	2	17	18	9
348	6	8	3	4	6	10	8	5	9	6	14	1	11	6	4	13	14	1	17	19	1
349	6	8	7	4	7	3	8	5	9	6	14	1	11	6	4	13	14	1	18	0	9
350	6	8	7	4	7	3	8	6	4	6	14	11	11	6	4	13	15	1	18	1	1
351	6	9	2	4	7	3	8	7	0	6	15	5	11	6	6	13	15	8	18	1	8
352	6	9	2	4	7	3	8	7	0	6	15	5	11	6	6	13	15	8	18	2	9
353	6	9	2	4	7	3	8	7	7	6	16	0	11	8	6	13	16	1	18	3	0
354	6	9	7	4	7	5	8	7	7	6	16	0	11	8	6	13	16	6	18	3	0
355	6	9	7	4	7	5	8	8	4	6	16	7	11	8	6	13	16	6	18	4	1
356	6	9	7	4	7	5	8	8	4	6	16	7	11	9	2	13	17	6	18	5	5
357	6	10	5	4	8	2	8	8	7	6	16	9	11	9	2	13	18	0	18	5	11
358	6	10	9	4	8	2	8	9	0	6	17	3	11	9	2	13	18	0	18	6	6
359	6	10	9	4	8	2	8	9	0	6	17	3	11	10	1	13	18	5	18	6	11
360	6	10	9	4	8	2	8	9	7	6	17	3	11	10	1	13	19	6	18	7	5

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No. 15.*

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES														
	Firewood plus 20% * Truck loads			Special M * Truck loads			M * Truck loads			A.P. * Truck loads			Grain * Truck loads			S.A.P. Minimum 3 tons		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
361	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
362	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
363	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
364	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
365	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
366	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
367	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
368	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
369	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
370	3	6	0	4	18	8	5	3	4	3	10	10	3	3	10	4	5	10
371	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
372	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
373	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
374	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
375	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
376	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
377	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
378	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
379	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
380	3	6	4	5	0	10	5	5	8	3	11	8	3	4	4	4	8	0
381	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
382	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
383	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
384	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
385	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
386	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
387	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
388	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
389	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
390	3	7	2	5	2	4	5	7	4	3	12	0	3	5	2	4	8	6
391	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
392	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
393	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
394	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
395	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
396	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
397	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
398	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
399	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
400	3	8	4	5	2	8	5	8	0	3	12	8	3	5	8	4	9	0
401	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
402	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
403	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
404	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
405	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
406	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
407	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
408	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
409	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10
410	3	9	2	5	3	2	5	8	8	3	14	0	3	6	4	4	9	10

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																							
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Minimum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
361	6	11	7	4	8	11	8	10	7	6	18	3	11	11	4	13	19	6	18	7	11
362	6	11	7	4	8	11	8	10	7	6	18	3	11	11	4	14	0	5	18	9	3
363	6	11	11	4	9	7	8	11	1	6	18	7	11	11	4	14	1	6	18	9	11
364	6	11	11	4	9	7	8	11	1	6	18	7	11	12	5	14	1	6	18	10	10
365	6	11	11	4	9	7	8	11	4	6	19	4	11	12	5	14	1	9	18	11	11
366	6	12	6	4	9	7	8	11	4	6	19	4	11	12	5	14	2	4	18	12	7
367	6	12	6	4	9	7	8	11	9	6	19	5	11	12	7	14	2	10	18	13	8
368	6	12	6	4	9	7	8	11	9	6	19	5	11	12	7	14	3	4	18	14	7
369	6	13	6	4	10	2	8	12	5	6	19	8	11	12	7	14	4	10	18	15	1
370	6	13	6	4	10	2	8	12	5	6	19	8	11	13	8	14	4	10	18	15	8
371	6	14	3	4	10	11	8	13	1	7	0	8	11	13	8	14	5	4	18	16	2
372	6	14	3	4	10	11	8	13	8	7	0	8	11	14	7	14	6	3	18	16	7
373	6	14	3	4	10	11	8	13	8	7	0	8	11	14	7	14	6	3	18	17	7
374	6	14	9	4	11	1	8	14	5	7	1	0	11	14	7	14	6	8	18	18	5
375	6	14	9	4	11	1	8	14	5	7	1	0	11	15	1	14	7	7	18	19	1
376	6	14	9	4	11	1	8	14	9	7	1	7	11	15	1	14	7	7	18	19	6
377	6	15	11	4	11	7	8	15	1	7	2	0	11	15	1	14	8	8	19	0	5
378	6	15	11	4	11	7	8	15	1	7	2	0	11	16	1	14	9	1	19	1	6
379	6	16	5	4	12	3	8	16	0	7	2	9	11	16	1	14	9	1	19	1	11
380	6	16	5	4	12	3	8	16	0	7	2	9	11	16	1	14	9	7	19	2	10
381	6	16	5	4	12	3	8	16	8	7	2	11	11	16	7	14	10	8	19	3	5
382	6	17	0	4	12	6	8	16	11	7	3	8	11	16	7	14	10	8	19	4	3
383	6	17	0	4	12	6	8	16	11	7	3	8	11	17	2	14	11	1	19	5	4
384	6	17	0	4	12	6	8	18	1	7	4	4	11	17	2	14	11	6	19	5	10
385	6	17	4	4	13	1	8	18	1	7	4	4	11	17	2	14	11	6	19	6	3
386	6	17	9	4	13	1	8	18	5	7	4	11	11	18	4	14	12	0	19	8	2
387	6	17	9	4	13	1	8	18	5	7	4	11	11	18	4	14	13	0	19	8	8
388	6	17	9	4	13	1	8	18	11	7	5	0	11	18	4	14	13	0	19	9	2
389	6	18	4	4	13	3	8	18	11	7	5	0	11	18	8	14	14	0	19	9	8
390	6	18	4	4	13	3	8	19	4	7	5	3	11	19	9	14	14	6	19	10	8
391	6	19	6	4	14	5	9	0	1	7	6	3	12	0	5	14	14	6	19	11	1
392	6	19	6	4	14	5	9	0	1	7	6	3	12	0	5	14	14	11	19	12	0
393	6	19	6	4	14	5	9	0	8	7	6	5	12	0	5	14	15	4	19	12	7
394	6	19	11	4	14	7	9	1	0	7	6	7	12	1	6	14	15	4	19	13	0
395	7	0	1	4	15	1	9	1	0	7	6	7	12	1	6	14	15	10	19	14	0
396	7	0	1	4	15	1	9	1	11	7	7	1	12	1	6	14	16	11	19	14	6
397	7	0	1	4	15	1	9	1	11	7	7	1	12	2	2	14	17	10	19	15	0
398	7	0	10	4	15	4	9	2	3	7	7	8	12	2	2	14	18	3	19	15	11
399	7	1	8	4	15	9	9	2	3	7	7	8	12	2	2	14	18	10	19	16	11
400	7	1	8	4	15	9	9	3	0	7	8	7	12	2	7	14	18	10	19	17	5
401	7	1	8	4	15	9	9	3	5	7	8	8	12	2	7	15	0	3	19	17	10
402	7	2	3	4	16	0	9	3	5	7	8	8	12	4	2	15	1	2	19	18	10
403	7	2	3	4	16	0	9	4	0	7	9	1	12	4	2	15	1	2	19	19	5
404	7	2	8	4	16	7	9	4	0	7	9	1	12	4	2	15	2	2	20	1	3
405	7	2	8	4	16	7	9	4	4	7	9	4	12	4	7	15	2	9	20	1	9
406	7	2	8	4	16	7	9	4	4	7	9	4	12	4	7	15	2	9	20	2	10
407	7	3	6	4	16	11	9	4	9	7	9	11	12	4	7	15	3	7	20	3	3
408	7	3	6	4	16	11	9	4	9	7	9	11	12	5	5	15	4	1	20	4	2
409	7	3	10	4	17	2	9	6	0	7	10	5	12	5	5	15	4	1	20	4	7
410	7	3	10	4	17	2	9	6	0	7	10	5	12	5	5	15	4	7	20	5	6

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No. 15.*

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
411	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
412	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
413	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
414	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
415	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
416	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
417	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
418	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
419	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
420	3 9 8	5 4 8	5 9 8	3 14 10	3 7 2	4 10 4
421	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
422	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
423	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
424	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
425	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
426	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
427	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
428	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
429	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
430	3 10 4	5 5 8	5 11 2	3 15 8	3 7 10	4 12 2
431	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
432	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
433	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
434	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
435	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
436	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
437	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
438	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
439	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
440	3 12 0	5 6 6	5 11 6	3 16 2	3 8 6	4 12 10
441	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
442	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
443	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
444	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
445	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
446	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
447	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
448	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
449	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
450	3 13 2	5 8 8	5 14 0	3 16 8	3 8 10	4 13 4
451	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
452	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
453	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
454	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
455	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
456	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
457	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
458	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
459	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0
460	3 14 0	5 9 8	5 15 4	3 17 4	3 9 8	4 15 0

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES						
	A ‡ Minimum 2 tons	AA Smalls Minimum	B ‡ Minimum 1 ton	BB ‡ Minimum 1 ton	C ‡ Minimum 1 ton	1 Smalls Minimum	2 Smalls Minimum
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
411	7 3 10	4 17 2	9 6 3	7 10 8	12 5 8	15 5 0	20 6 1
412	7 4 9	4 17 8	9 6 11	7 11 4	12 5 8	15 5 0	20 7 1
413	7 4 9	4 17 8	9 7 1	7 11 9	12 7 5	15 5 6	20 7 6
414	7 5 2	4 18 3	9 7 1	7 11 9	12 7 5	15 7 0	20 8 0
415	7 5 2	4 18 3	9 7 8	7 12 5	12 7 5	15 7 0	20 8 7
416	7 5 2	4 18 3	9 7 8	7 12 5	12 7 9	15 7 5	20 9 5
417	7 5 6	4 18 7	9 8 4	7 12 9	12 7 9	15 8 0	20 10 5
418	7 5 6	4 18 7	9 9 5	7 13 9	12 7 9	15 8 0	20 11 1
419	7 6 4	4 19 1	9 9 5	7 13 9	12 9 2	15 8 5	20 11 4
420	7 6 4	4 19 1	9 9 9	7 14 0	12 9 2	15 9 0	20 11 9
421	7 6 6	4 19 1	9 10 3	7 14 0	12 9 6	15 9 0	20 13 4
422	7 6 6	4 19 1	9 10 3	7 14 0	12 9 6	15 9 5	20 14 5
423	7 7 10	5 0 1	9 10 7	7 14 1	12 9 6	15 10 5	20 14 10
424	7 7 10	5 0 1	9 10 7	7 14 1	12 10 7	15 10 5	20 15 3
425	7 7 10	5 0 1	9 11 4	7 15 1	12 10 7	15 11 4	20 15 9
426	7 8 1	5 0 1	9 11 4	7 15 1	12 10 7	15 11 9	20 17 2
427	7 8 1	5 0 1	9 11 9	7 15 4	12 11 3	15 12 5	20 17 9
428	7 8 8	5 0 5	9 11 9	7 15 4	12 11 3	15 13 3	20 18 8
429	7 8 8	5 0 5	9 12 3	7 16 0	12 11 3	15 13 9	21 0 1
430	7 8 8	5 0 5	9 13 3	7 16 7	12 11 9	15 13 9	21 0 8
431	7 9 1	5 0 11	9 13 11	7 17 4	12 11 9	15 15 3	21 0 11
432	7 9 1	5 0 11	9 13 11	7 17 4	12 12 9	15 15 9	21 1 7
433	7 9 4	5 1 2	9 14 4	7 17 11	12 12 9	15 15 9	21 2 0
434	7 9 4	5 1 2	9 14 4	7 17 11	12 12 9	15 16 7	21 3 7
435	7 9 4	5 1 2	9 14 8	7 17 11	12 13 10	15 17 2	21 4 0
436	7 10 6	5 1 6	9 14 8	7 17 11	12 13 10	15 17 2	21 4 7
437	7 10 6	5 1 6	9 15 1	7 18 0	12 13 10	15 17 7	21 4 11
438	7 11 4	5 2 7	9 15 1	7 18 0	12 14 10	15 18 3	21 5 5
439	7 11 4	5 2 7	9 15 5	7 18 4	12 14 10	15 18 3	21 5 11
440	7 11 4	5 2 7	9 15 5	7 18 4	12 14 10	15 18 7	21 7 10
441	7 11 8	5 2 7	9 15 11	7 18 8	12 15 5	16 0 0	21 8 4
442	7 11 8	5 2 7	9 17 3	7 19 11	12 15 5	16 0 0	21 8 9
443	7 12 3	5 2 11	9 17 3	7 19 11	12 16 0	16 0 6	21 9 10
444	7 13 1	5 3 9	9 17 9	8 0 4	12 16 0	16 1 7	21 10 4
445	7 13 1	5 3 9	9 17 9	8 0 4	12 16 6	16 1 7	21 10 9
446	7 13 1	5 3 9	9 18 0	8 0 9	12 17 4	16 2 0	21 12 2
447	7 13 6	5 3 9	9 18 0	8 0 9	12 17 4	16 2 5	21 13 2
448	7 13 6	5 3 9	9 18 8	8 0 9	12 17 4	16 2 5	21 13 9
449	7 13 9	5 3 11	9 19 3	8 1 7	12 18 3	16 3 5	21 14 2
450	7 13 9	5 3 11	9 19 3	8 1 7	12 18 3	16 4 0	21 14 6
451	7 13 9	5 3 11	9 19 11	8 1 8	12 19 6	16 4 0	21 15 1
452	7 14 3	5 4 4	10 0 1	8 2 1	12 19 6	16 4 11	21 16 6
453	7 14 3	5 4 4	10 0 1	8 2 1	12 19 6	16 5 5	21 17 0
454	7 15 2	5 4 11	10 0 9	8 2 11	12 19 9	16 5 5	21 17 6
455	7 15 2	5 4 11	10 0 9	8 2 11	12 19 9	16 6 3	21 18 0
456	7 15 2	5 4 11	10 1 5	8 3 5	12 19 9	16 6 9	21 18 5
457	7 16 1	5 5 4	10 1 5	8 3 5	13 0 7	16 7 4	22 0 0
458	7 16 1	5 5 4	10 2 1	8 4 1	13 0 7	16 8 9	22 0 4
459	7 16 7	5 6 1	10 2 1	8 4 1	13 0 7	16 9 10	22 2 0
460	7 16 7	5 6 1	10 2 8	8 4 1	13 0 9	16 9 10	22 2 4

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition* **No. 15.**

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
461	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
462	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
463	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
464	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
465	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
466	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
467	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
468	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
469	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
470	3 14 6	5 10 6	5 16 0	3 18 6	3 10 10	4 16 4
471	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
472	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
473	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
474	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
475	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
476	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
477	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
478	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
479	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
480	3 15 2	5 11 2	5 16 10	3 19 0	3 11 8	4 16 10
481	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
482	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
483	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
484	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
485	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
486	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
487	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
488	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
489	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
490	3 15 10	5 11 6	5 17 2	4 0 0	3 11 10	4 17 4
491	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
492	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
493	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
494	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
495	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
496	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
497	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
498	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
499	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
400	3 17 2	5 13 0	5 18 6	4 0 6	3 12 6	4 18 4
501	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
502	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
503	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
504	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
505	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
506	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
507	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
508	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
509	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0
510	3 18 2	5 15 2	6 1 0	4 1 2	3 12 10	5 0 0

* *For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																							
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Mini mum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
461	7	16	7	5	6	1	10	3	0	8	4	7	13	0	9	16	10	1	22	3	4
462	7	16	10	5	6	1	10	3	0	8	4	7	13	1	10	16	10	9	22	3	9
463	7	16	10	5	6	1	10	3	4	8	4	11	13	1	10	16	10	9	22	4	3
464	7	17	5	5	6	3	10	3	4	8	4	11	13	1	10	16	11	2	22	4	8
465	7	17	5	5	6	3	10	4	7	8	5	11	13	2	11	16	11	9	22	6	2
466	7	17	5	5	6	3	10	5	0	8	6	3	13	2	11	16	11	9	22	7	2
467	7	17	10	5	6	10	10	5	0	8	6	3	13	2	11	16	12	1	22	7	9
468	7	17	10	5	6	10	10	5	7	8	6	5	13	4	8	16	13	8	22	8	2
469	7	18	1	5	7	0	10	5	7	8	6	5	13	4	8	16	13	8	22	8	8
470	7	18	1	5	7	0	10	6	0	8	7	1	13	4	8	16	14	1	22	10	0
471	7	19	6	5	8	0	10	6	5	8	7	3	13	5	10	16	14	6	22	10	6
472	7	19	6	5	8	0	10	6	5	8	7	3	13	5	10	16	14	6	22	11	1
473	8	0	2	5	8	3	10	6	9	8	7	3	13	6	7	16	15	0	22	11	7
474	8	0	2	5	8	3	10	6	9	8	7	3	13	6	7	16	15	11	22	12	6
475	8	0	2	5	8	3	10	7	8	8	8	4	13	6	7	16	15	11	22	12	11
476	8	0	5	5	8	3	10	7	8	8	8	4	13	6	10	16	16	6	22	13	7
477	8	0	5	5	8	3	10	8	3	8	9	0	13	6	10	16	17	0	22	14	6
478	8	0	9	5	8	10	10	8	3	8	9	0	13	6	10	16	17	0	22	15	10
479	8	0	9	5	8	10	10	8	9	8	9	1	13	7	4	16	17	11	22	16	4
480	8	0	9	5	8	10	10	8	9	8	9	1	13	7	4	16	18	4	22	16	11
481	8	1	8	5	9	5	10	9	1	8	9	8	13	7	11	16	18	4	22	17	5
482	8	1	8	5	9	5	10	9	9	8	10	0	13	7	11	16	19	5	22	17	10
483	8	2	1	5	9	5	10	9	9	8	10	0	13	7	11	16	19	11	22	19	8
484	8	2	1	5	9	5	10	10	5	8	10	11	13	9	6	16	19	11	23	0	3
485	8	2	1	5	9	5	10	11	0	8	10	11	13	9	6	17	0	11	23	0	9
486	8	3	0	5	10	4	10	11	0	8	10	11	13	9	6	17	1	4	23	1	8
487	8	3	0	5	10	4	10	11	9	8	11	4	13	10	1	17	3	4	23	2	9
488	8	3	8	5	10	7	10	11	9	8	11	4	13	10	1	17	3	8	23	3	8
489	8	3	8	5	10	7	10	12	3	8	11	9	13	10	1	17	4	2	23	4	1
490	8	3	8	5	10	7	10	12	3	8	11	9	13	10	7	17	4	2	23	4	8
491	8	4	0	5	10	7	10	13	0	8	12	8	13	10	7	17	4	8	23	5	5
492	8	4	0	5	10	7	10	13	4	8	13	0	13	11	5	17	5	3	23	6	1
493	8	4	9	5	11	8	10	13	4	8	13	0	13	11	5	17	5	3	23	6	6
494	8	5	2	5	11	11	10	13	9	8	13	5	13	11	5	17	6	2	23	7	11
495	8	5	2	5	11	11	10	13	9	8	13	5	13	11	7	17	7	2	23	7	11
496	8	5	2	5	11	11	10	14	4	8	13	9	13	11	7	17	7	2	23	9	0
497	8	5	9	5	11	11	10	14	4	8	13	9	13	11	7	17	7	6	23	9	11
498	8	5	9	5	11	11	10	14	8	8	14	0	13	12	4	17	8	2	23	10	5
499	8	6	0	5	12	4	10	14	8	8	14	0	13	12	4	17	8	2	23	10	11
500	8	6	0	5	12	4	10	15	11	8	14	9	13	13	7	17	8	7	23	12	4
501	8	6	0	5	12	4	10	16	4	8	15	1	13	14	6	17	9	6	23	12	11
502	8	7	4	5	13	1	10	16	7	8	15	7	13	14	6	17	9	6	23	13	3
503	8	7	4	5	13	1	10	16	7	8	15	7	13	15	6	17	9	11	23	13	8
504	8	7	11	5	13	3	10	17	1	8	16	0	13	15	6	17	10	6	23	14	3
505	8	7	11	5	13	3	10	17	1	8	16	0	13	15	6	17	10	6	23	14	8
506	8	7	11	5	13	3	10	17	5	8	16	8	13	15	11	17	11	6	23	16	8
507	8	8	4	5	13	10	10	17	5	8	16	8	13	15	11	17	12	6	23	17	1
508	8	8	4	5	13	10	10	18	7	8	17	1	13	15	11	17	12	6	23	17	7
509	8	8	10	5	14	3	10	18	7	8	17	1	13	16	8	17	12	11	23	18	8
510	8	8	10	5	14	3	10	18	9	8	17	5	13	16	8	17	13	4	23	19	7

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No. 15.*

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
511	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
512	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
513	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
514	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
515	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
516	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
517	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
518	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
519	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
520	3 18 10	5 15 6	6 1 6	4 2 6	3 14 4	5 0 10
521	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
522	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
523	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
524	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
525	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
526	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
527	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
528	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
529	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
530	3 19 2	5 17 6	6 3 4	4 3 0	3 14 10	5 2 4
531	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
532	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
533	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
534	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
535	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
536	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
537	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
538	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
539	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
540	4 0 2	5 18 0	6 4 2	4 4 0	3 15 8	5 2 8
541	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
542	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
543	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
544	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
545	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
546	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
547	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
548	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
549	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
550	4 0 8	5 19 4	6 5 2	4 4 10	3 16 8	5 3 2
551	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
552	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
553	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
554	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
555	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
556	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
557	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
558	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
559	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8
560	4 2 2	6 0 4	6 6 2	4 5 10	3 17 4	5 4 8

** For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																	
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Minimum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
511	8	8	10	5	14	3	10	19	9	8	18	0	13	17	8	17	13	4	24	0	1
512	8	9	1	5	14	3	10	19	9	8	18	0	13	17	8	17	13	10	24	0	6
513	8	9	1	5	14	3	11	0	0	8	18	3	13	17	8	17	14	5	24	1	6
514	8	9	8	5	14	7	11	0	0	8	18	3	13	18	5	17	14	5	24	2	6
515	8	9	8	5	14	7	11	0	4	8	18	5	13	18	5	17	15	10	24	2	10
516	8	9	8	5	14	7	11	0	4	8	18	5	13	18	5	17	16	9	24	3	10
517	8	10	5	5	15	3	11	1	3	8	19	4	13	19	11	17	17	2	24	4	6
518	8	10	5	5	15	3	11	1	3	8	19	4	13	19	11	17	17	9	24	5	5
519	8	11	2	5	15	10	11	1	5	8	19	7	13	19	11	17	18	9	24	6	6
520	8	11	2	5	15	10	11	1	5	8	19	7	14	0	11	17	18	9	24	6	9
521	8	11	9	5	16	4	11	2	11	9	0	9	14	0	11	17	19	1	24	7	4
522	8	11	9	5	16	4	11	2	11	9	0	9	14	1	7	18	0	9	24	7	10
523	8	12	4	5	16	8	11	3	11	9	1	4	14	1	7	18	0	9	24	8	9
524	8	12	4	5	16	8	11	3	11	9	1	4	14	1	7	18	1	1	24	9	2
525	8	12	4	5	16	8	11	4	1	9	1	8	14	2	0	18	1	8	24	10	3
526	8	13	2	5	16	9	11	4	1	9	1	8	14	2	0	18	1	8	24	11	8
527	8	13	2	5	16	9	11	4	7	9	2	1	14	2	0	18	2	9	24	12	2
528	8	13	4	5	17	6	11	4	7	9	2	1	14	2	8	18	3	0	24	12	7
529	8	13	4	5	17	6	11	5	0	9	2	3	14	2	8	18	3	0	24	13	0
530	8	13	4	5	17	6	11	5	0	9	2	3	14	2	8	18	3	7	24	13	8
531	8	14	1	5	17	9	11	5	4	9	2	9	14	2	10	18	4	1	24	15	0
532	8	14	1	5	17	9	11	5	8	9	3	0	14	2	10	18	4	1	24	15	8
533	8	14	8	5	18	1	11	5	8	9	3	0	14	4	10	18	5	5	24	16	1
534	8	14	8	5	18	1	11	7	0	9	4	1	14	4	10	18	5	11	24	16	4
535	8	14	8	5	18	1	11	7	0	9	4	1	14	4	10	18	5	11	24	18	0
536	8	15	4	5	18	6	11	7	7	9	4	1	14	5	6	18	6	6	24	18	4
537	8	15	4	5	18	6	11	7	7	9	4	1	14	5	6	18	6	11	24	19	11
538	8	15	11	5	18	11	11	8	0	9	4	5	14	5	6	18	6	11	25	0	4
539	8	15	11	5	18	11	11	8	4	9	4	11	14	5	8	18	7	5	25	0	10
540	8	15	11	5	18	11	11	8	4	9	4	11	14	5	8	18	7	11	25	1	11
541	8	16	9	5	19	1	11	9	0	9	5	9	14	7	0	18	7	11	25	2	2
542	8	16	9	5	19	1	11	9	7	9	6	0	14	7	0	18	9	3	25	2	10
543	8	17	0	5	19	7	11	9	7	9	6	0	14	7	0	18	9	11	25	3	10
544	8	17	5	6	0	3	11	9	9	9	6	3	14	7	6	18	9	11	25	4	2
545	8	17	5	6	0	3	11	9	9	9	6	3	14	7	6	18	10	10	25	5	3
546	8	17	5	6	0	3	11	10	11	9	7	1	14	7	6	18	11	11	25	6	2
547	8	18	0	6	0	3	11	10	11	9	7	1	14	8	7	18	12	4	25	6	7
548	8	18	0	6	0	3	11	11	4	9	7	8	14	8	7	18	12	7	25	7	2
549	8	19	1	6	1	4	11	11	4	9	7	8	14	8	7	18	13	8	25	8	7
550	8	19	1	6	1	4	11	12	1	9	7	11	14	8	10	18	13	8	25	9	1
551	8	19	1	6	1	4	11	12	4	9	8	3	14	8	10	18	14	7	25	9	7
552	8	19	8	6	1	4	11	12	4	9	8	3	14	9	11	18	15	1	25	10	0
553	8	19	8	6	1	4	11	13	0	9	8	5	14	9	11	18	15	1	25	10	6
554	8	19	11	6	1	8	11	13	0	9	8	5	14	10	9	18	15	8	25	11	11
555	8	19	11	6	1	8	11	13	3	9	9	3	14	11	0	18	16	2	25	12	11
556	8	19	11	6	1	8	11	13	3	9	9	3	14	11	0	18	16	2	25	13	5
557	9	0	4	6	2	0	11	14	3	9	9	9	14	11	0	18	16	7	25	14	5
558	9	0	4	6	2	0	11	15	0	9	10	7	14	12	5	18	17	7	25	14	10
558	9	1	1	6	2	7	11	15	0	9	10	7	14	12	5	18	17	7	25	15	5
560	9	1	1	6	2	7	11	15	7	9	11	0	14	12	5	18	18	5	25	15	9

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No. 15.*

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Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES					
	Firewood plus 20% * Truck loads	Special M * Truck loads	M * Truck loads	A.P. * Truck loads	Grain * Truck loads	S.A.P. Minimum 3 tons
	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.	£ s. d.
561	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
562	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
563	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
564	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
565	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
566	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
567	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
568	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
569	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
570	4 2 6	6 1 6	6 7 8	4 6 10	3 18 4	5 5 0
571	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
572	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
573	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
574	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
575	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
576	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
577	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
578	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
579	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
580	4 3 2	6 2 10	6 9 0	4 8 0	3 18 10	5 6 6
581	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
582	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
583	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
584	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
585	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
586	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
587	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
588	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
589	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
590	4 4 0	6 3 4	6 9 8	4 8 6	3 19 10	5 7 4
591	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
592	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
593	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
594	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
595	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
596	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
597	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
598	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
599	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8
600	4 4 10	6 4 2	6 10 2	4 9 0	4 0 2	5 8 8

* *For minimum weights and alternative rates for smaller quantities, see General Condition No. 15.*

Mileage Rates Per Ton For Goods—*continued.*

Miles	CLASSES																							
	A ‡ Minimum 2 tons			AA Smalls Minimum			B ‡ Minimum 1 ton			BB ‡ Minimum 1 ton			C ‡ Mini mum 1 ton			1 Smalls Minimum			2 Smalls Minimum		
	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.	£	s.	d.
561	9	1	1	6	2	7	11	15	9	9	11	0	14	12	10	18	19	1	25	16	9
562	9	1	9	6	2	9	11	15	9	9	11	0	14	12	10	18	19	1	25	17	9
563	9	1	9	6	2	9	11	16	3	9	11	4	14	13	6	19	0	5	25	18	3
564	9	2	4	6	3	2	11	16	3	9	11	4	14	13	6	19	0	5	25	19	3
565	9	2	4	6	3	2	11	16	8	9	11	7	14	13	6	19	0	5	25	19	8
566	9	2	4	6	3	2	11	16	8	9	11	7	14	14	0	19	1	6	26	1	2
567	9	3	2	6	3	5	11	17	1	9	12	0	14	14	0	19	1	11	26	2	1
568	9	3	2	6	3	5	11	17	1	9	12	0	14	14	0	19	1	11	26	2	7
569	9	3	6	6	4	0	11	18	4	9	13	4	14	16	0	19	2	10	26	3	2
570	9	3	6	6	4	0	11	18	4	9	13	4	14	16	0	19	3	5	26	3	7
571	9	3	10	6	4	7	11	18	8	9	13	7	14	16	9	19	3	5	26	4	7
572	9	3	10	6	4	7	11	19	4	9	13	11	14	16	9	19	4	3	26	5	0
573	9	4	10	6	4	9	11	19	4	9	13	11	14	16	9	19	5	4	26	6	7
574	9	4	10	6	4	9	11	19	8	9	14	4	14	16	11	19	5	4	26	7	0
575	9	4	10	6	4	9	12	0	5	9	14	8	14	16	11	19	5	10	26	7	11
576	9	5	6	6	4	11	12	0	5	9	14	8	14	16	11	19	6	3	26	8	4
577	9	5	6	6	4	11	12	0	9	9	15	1	14	18	2	19	7	9	26	9	0
578	9	5	9	6	5	8	12	0	9	9	15	1	14	18	2	19	8	2	26	9	3
579	9	5	9	6	5	8	12	1	1	9	15	4	14	18	2	19	8	8	26	10	10
580	9	5	9	6	5	8	12	1	1	9	15	4	14	18	11	19	8	8	26	11	3
581	9	7	0	6	6	3	12	2	0	9	16	0	14	18	11	19	9	2	26	11	9
582	9	7	0	6	6	3	12	2	4	9	16	7	15	0	5	19	9	8	26	12	4
583	9	7	4	6	6	10	12	2	4	9	16	7	15	0	5	19	9	8	26	12	10
584	9	7	4	6	6	10	12	3	4	9	17	3	15	0	5	19	10	8	26	13	2
585	9	7	4	6	6	10	12	3	4	9	17	3	15	1	1	19	11	1	26	14	2
586	9	7	10	6	7	1	12	3	11	9	17	9	15	1	1	19	11	1	26	15	6
587	9	7	10	6	7	1	12	3	11	9	17	9	15	1	1	19	12	0	26	16	2
588	9	8	4	6	7	1	12	4	1	9	17	9	15	1	7	19	12	7	26	17	1
589	9	8	4	6	7	1	12	4	1	9	17	9	15	1	7	19	12	7	26	17	6
590	9	8	4	6	7	1	12	4	7	9	18	0	15	1	7	19	13	0	26	18	2
591	9	9	1	6	7	10	12	5	7	9	19	1	15	2	2	19	14	0	26	19	0
592	9	9	1	6	7	10	12	5	7	9	19	1	15	2	2	19	14	0	27	0	11
593	9	9	8	6	8	4	12	6	1	9	19	4	15	3	0	19	14	6	27	1	6
594	9	10	1	6	8	4	12	6	7	10	0	0	15	3	0	19	15	0	27	2	0
595	9	10	1	6	8	4	12	6	7	10	0	0	15	3	0	19	15	0	27	2	5
596	9	10	1	6	8	4	12	7	3	10	0	3	15	3	8	19	15	11	27	2	10
597	9	11	0	6	9	2	12	7	3	10	0	3	15	3	8	19	16	11	27	4	5
598	9	11	0	6	9	2	12	7	7	10	0	7	15	3	8	19	16	11	27	4	8
599	9	11	4	6	9	7	12	7	7	10	0	7	15	4	1	19	17	5	27	5	4
600	9	11	4	6	9	7	12	7	11	10	0	9	15	4	1	19	17	10	27	5	9

‡ *Unless otherwise specified. For alternative rates for smaller quantities, see General Condition No, 15.*

"SMALLS" MINIMUM TARIFF
(At Owner's Risk)

MINIMUM FREIGHT CHARGES FOR SMALL CONSIGNMENTS

Traffic Chargeable at Classes 'A' & 'AA'	Traffic Chargeable at Classes 'B' & 'BB'	Traffic Chargeable at Class 'C'	Traffic Chargeable at Classes '1' & '2'	Weight not Exceeding—											
				1 qr.	2 qrs.	3 qrs.	1 cwt.	1c. 1q.	1c. 2q.	1c. 3q.	2 cwt.	2c. 1q.	2c. 2q.	2c. 3q.	3 cwt.
Freight Rate not Exceeding—															
s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
11 9	11 0	10 9	10 6	2 9	2 9	2 9	2 9	2 9	2 9	2 9	2 9	2 9	2 9	2 9	2 9
17 0	16 0	15 9	15 0	2 9	2 9	2 9	2 9	2 9	2 9	2 10	3 2	3 3	3 4	3 7	3 10
22 3	21 0	20 6	19 9	2 9	2 9	2 9	2 9	2 10	2 10	3 2	3 3	3 4	3 7	3 11	4 3
28 0	26 9	26 0	25 0	2 9	2 9	2 9	2 10	3 2	3 3	3 4	3 10	3 11	4 4	4 8	4 11
33 3	31 9	31 0	29 9	2 9	2 9	2 10	3 2	3 4	3 7	3 11	4 4	4 8	5 0	5 2	6 1
38 3	36 6	35 6	34 3	2 9	2 9	2 10	3 3	3 7	3 11	4 4	4 11	5 2	6 1	6 5	6 10
43 9	41 9	40 9	39 3	2 9	2 9	3 2	3 7	3 11	4 4	4 11	5 5	6 3	6 7	7 2	7 7
55 0	52 6	51 0	49 3	2 9	2 10	3 3	3 11	4 5	5 2	6 1	6 10	7 4	8 2	8 7	9 6
65 3	62 0	60 6	58 3	2 9	3 2	3 10	4 5	5 2	6 3	7 2	7 11	8 7	9 6	10 7	11 5
76 6	72 9	71 0	68 3	2 9	3 3	4 3	5 0	6 3	7 2	8 2	9 0	10 4	11 1	11 11	12 11
87 0	82 9	80 9	77 6	2 9	3 4	4 5	5 9	7 1	7 11	9 0	10 5	11 6	12 4	13 7	14 11
109 0	103 9	101 3	97 3	2 10	3 11	5 5	7 1	8 3	9 8	11 5	12 7	14 1	15 5	17 0	18 4
130 3	124 0	121 0	116 3	3 2	4 5	6 5	8 2	10 0	11 6	13 3	15 1	16 8	18 4	20 1	22 1
152 0	144 9	141 3	135 9	3 3	5 0	7 4	9 3	11 5	13 3	15 5	17 4	19 3	21 4	23 6	25 4
173 3	165 0	161 0	154 9	3 7	5 9	8 2	10 7	12 9	15 1	17 4	19 9	22 3	24 2	26 8	28 11
195 6	186 0	181 6	174 6	3 10	6 5	9 0	11 8	14 4	16 8	19 5	22 3	24 8	27 2	30 0	32 6
217 0	206 9	201 6	193 9	4 3	7 1	10 0	12 9	15 9	18 8	21 5	24 7	27 5	30 1	33 2	36 1
238 6	227 0	221 6	213 0	4 5	7 6	10 10	14 1	17 4	20 3	23 7	26 10	30 1	33 2	36 6	39 8
260 3	247 9	241 6	232 3	4 11	8 2	11 10	15 2	19 0	22 3	25 11	29 0	32 11	36 1	39 9	43 3

"SMALLS" MINIMUM TARIFF—Continued.

Traffic Chargeable at Classes 'A' & 'AA'	Traffic Chargeable at Classes 'B' & 'BB'	Traffic Chargeable at Class 'C'	Traffic Chargeable at Classes '1' & '2'	Weight not Exceeding— 1 qr.	2 qrs.	3 qrs.	1 cwt.	1c. 1q.	1c. 2q.	1c. 3q.	2 cwt.	2c. 1q.	2c. 2q.	2c. 3q.	3 cwt.
Freight Rate not Exceeding—															
s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
282 3	268 9	262 0	252 0	5 2	8 7	12 9	16 3	20 4	23 9	27 10	31 5	35 9	39 0	43 3	46 8
—	289 0	281 9	271 0	5 9	9 3	14 0	17 7	22 3	25 9	30 1	34 0	38 6	41 11	46 6	50 3
—	309 9	302 0	290 6	6 3	10 0	14 11	18 10	23 7	27 5	32 3	36 5	41 2	45 0	49 11	53 9
—	330 0	321 9	309 6	6 7	10 7	15 9	19 11	25 3	29 0	34 5	38 7	44 0	47 11	53 4	57 5
—	351 0	342 3	329 3	7 1	11 1	16 8	21 2	26 10	31 0	36 7	40 10	46 8	51 0	56 5	60 9
—	—	362 6	348 6	7 4	11 8	17 11	22 5	28 6	32 10	38 9	43 7	49 6	53 9	59 11	64 4
—	—	382 3	367 6	7 7	12 2	18 10	23 6	30 0	34 5	40 10	45 9	52 3	56 9	63 3	67 11
—	—	401 9	387 0	8 2	12 9	19 9	24 8	31 5	36 5	43 3	48 1	55 0	59 8	66 8	71 6
—	—	—	406 9	8 5	13 5	20 8	25 9	32 10	38 6	45 6	50 7	57 10	62 10	70 0	75 1
—	—	—	426 3	8 9	14 1	21 8	26 10	34 3	40 6	47 9	53 1	60 8	66 0	73 4	78 8
—	—	—	446 0	9 1	14 9	22 8	27 11	35 8	42 7	50 1	55 7	63 6	69 1	76 8	82 3
—	—	—	465 9	9 5	15 5	23 7	29 0	37 1	44 8	52 4	58 1	66 4	72 3	80 1	85 10
—	—	—	485 6	9 9	16 0	24 7	30 1	38 6	46 8	54 7	60 7	69 1	75 5	83 5	89 5
—	—	—	505 0	10 1	16 8	25 7	31 2	39 10	48 9	56 11	63 1	71 11	78 7	86 9	92 11
—	—	—	524 9	10 5	17 4	26 7	32 3	41 3	50 10	59 2	65 7	74 9	81 8	90 2	96 6
—	—	—	544 6	10 9	18 0	27 6	33 4	42 8	52 10	61 5	68 0	77 7	84 10	93 6	100 1
—	—	—	564 3	11 1	18 8	28 6	34 5	44 1	54 11	63 8	70 6	80 5	88 0	96 10	103 8
			Each 19/9 or part thereof above 564/3 add	0 4	0 8	1 0	1 1	1 5	2 1	2 3	2 6	2 10	3 2	3 4	3 7

For Conditions see next page.

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CONDITIONS FOR "SMALLS" MINIMUM TARIFF.

(1) All goods shall, unless otherwise provided, be charged under the rates and conditions governing the carriage of goods for their respective classes, subject to a minimum charge for **each** class in accordance with the "Smalls" Minimum Tariff specified on the preceding page.

The "Smalls" Minimum Tariff is computed only up to 3 cwt., and above that weight the tonnage rates shall be charged, subject to a minimum charge as for 3 cwt. at the "Smalls" Minimum Tariff, provided that such minimum charge shall not be less than 6s. 9d. at Owner's Risk.

(2) When goods classified in two or more classes are forwarded by one or more *bona fide* consignors from the same forwarding station on the same day to one *bona fide* consignee at the same station of destination, such goods, if it be cheaper than to charge each class separately at the "Smalls" Minimum Tariff may be placed in one or more groups, and the total weight of each such group charged under the rates and conditions for the goods of the highest class in each such group, subject to a minimum charge for each group, in accordance with the "Smalls" Minimum Tariff. The total charge only shall be subject to the minimum charge of 6s. 9d. as prescribed in Clause (1).

(3) When the weight of a consignment of goods classified under Class 2, plus 25 per cent., Class 2 plus 50 per cent. or Double Class 2 does not exceed 3 cwt. the percentage concerned shall be added to the single Class 2 rate and charged in accordance with the "Smalls" Minimum Tariff for the rate so obtained.

(4) When the total weight of a consignment consisting of Goods, a portion of which is chargeable under Class "2," and the remainder under Class "2" plus 25 per cent., Class "2" plus 50 per cent., or Double Class "2," **exceeds 3 cwt.**, neither portion of the consignment shall be subject to "Smalls" minimum, but each portion shall be charged *pro rata* at the tonnage rate.

When the total weight of a similar consignment **does not amount to 3 cwt.**, each portion of the consignment shall be charged *pro rata* at its tonnage rate, but subject to the total charge not being less than the "Smalls" Minimum charge for the total weight under Class "2."

(5) **Commissioners' Risk**

(*a*) The Commissioners' Risk charge under the "Smalls" Minimum Tariff in respect of goods classified below Class 1 shall be computed in accordance with General Condition No. 4, page 10, but such charge shall not exceed the Owner's Risk rate prescribed in the "Smalls" Minimum Tariff for Class 1.

(*b*) When goods in Classes 1 and 2, whether carried at Commissioners' or Owner's risk are subject to "Smalls" minimum, or to the minimum charge per consignment specified in clause (1), the freight charges shall be computed at the minimum charges specified for Owner's risk.

(*c*) The "Smalls" Minimum Tariff for consignments classified at Class 1 plus 10 per cent., or Class 2 plus 10 per cent., shall be computed at the tonnage rate for Class 1 or Class 2 respectively at the O.R. "Smalls" Minimum plus 10 per cent., provided that the charges so ascertained shall not be less than 3s. 4d. per consignment in excess of the "Smalls" Minimum at such O.R. rates for Classes 1 and 2.

(6) The charges for small consignments forwarded by goods train must not exceed the charges under the Parcels Rates.

(7) (*a*) Any traffic which is subject to the "Smalls" Minimum Tariff and is charged at rates less than Class "A" shall be charged the "Smalls" Minimum provided for Class "A."

(*b*) Small consignments of fish charged at a rate per ton per mile and subject to "Smalls" Minimum Tariff shall be charged the "Smalls" Minimum provided for Class "C."

(*c*) Small consignments chargeable at a combination of rates under different classifications shall be charged the "Smalls" Minimum provided for the higher classification concerned.

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OWNER'S RISK RATES PER TON FOR MANURE.

For carriage at Commissioners' Risk, see General Condition No. 4.

Miles	Rate	Miles	Rate	Miles	Rate	Miles	Rate
	s. d.		s. d.		s. d.		s. d.
1–27	8 4	88–93	19 6	186–193	30 0	341–360	42 10
28–31	8 10	94–96	20 2	194–200	30 8	361–380	44 2
32–34	10 6	97–101	20 8	201–210	31 8	381–390	45 0
35–37	11 2	102–109	21 4	211–220	33 0	391–410	45 6
38–41	12 0	110–117	22 0	221–230	33 8	411–430	46 0
42–43	12 4	118–125	23 6	231–240	34 6	431–450	46 10
44–46	12 10	126–133	24 2	241–250	35 10	451–470	47 6
47–50	13 10	134–141	24 10	251–260	37 0	471–490	49 0
51–57	14 8	142–149	25 4	261–270	37 10	491–510	49 6
58–63	15 8	150–157	26 0	271–280	38 10	511–530	50 0
64–69	16 4	158–165	27 4	281–290	40 4	531–550	50 10
70–75	16 10	166–173	28 0	291–300	40 10	551–570	51 6
76–81	17 10	174–177	28 6	301–320	41 6	571–580	51 10
82–87	18 8	178–185	29 4	321–340	42 6	581–600	53 2

The above rates are subject to the undermentioned truckload minima :—

Per 11 ton truck ... ... 10 tons 9 cwt.

Per 16, 22 or 27 ton truck ... ... 15 tons 4 cwt.

Per Bogie truck ... ... 25 tons 0 cwt.

Smaller quantities shall be charged :—

Minimum 2 tons ... ... Firewood plus 20 per cent. rate.

Minimum 10 cwt. ... ... "A.P." rate.

Smaller quantities ... ... "A" rate "Smalls" Minimum.

OWNER'S RISK RATES PER TON FOR UNSEASONED HARDWOOD TIMBER

(See Footnotes.)

For carriage at Commissioners' Risk see General Condition No. 4.

Miles	Rate	Miles	Rate	Miles	Rate	Miles	Rate
	s. d.		s. d.		s. d.		s. d.
1–10	11 1	**90–93**	33 7	**187–191**	55 7	**322–326**	77 4
11–13	12 1	**94–97**	34 0	**192–196**	56 5	**327–331**	77 9
14–26	12 3	**98–99**	34 7	**197–200**	56 11	**332–336**	78 9
27–28	13 1	**100–101**	35 5	**201–204**	57 4	**337–341**	79 2
29–31	13 7	**102–103**	36 10	**205–206**	58 6	**342–350**	80 8
32–33	14 0	**104–107**	37 6	**207–210**	59 0	**351–360**	81 11
34–35	14 10	**108–110**	37 11	**211–212**	59 9	**361–370**	82 3
36–37	15 7	**111–112**	38 4	**213–216**	60 7	**371–380**	84 0
38–40	16 10	**113–115**	39 0	**217–221**	61 8	**381–390**	85 3
41–42	17 3	**116–117**	39 7	**222–226**	62 4	**391–400**	85 7
43–44	17 9	**118–121**	41 3	**227–231**	63 1	**401–410**	86 0
45–46	18 4	**122–123**	41 8	**232–236**	63 6	**411–420**	87 3
47	18 11	**124–126**	42 4	**237–241**	63 11	**421–430**	88 1
48–50	20 2	**127**	42 11	**242–246**	64 5	**431–440**	88 9
51–52	20 8	**128–130**	43 2	**247–249**	65 10	**441–450**	90 7
53–54	21 8						
55–56	22 9	**131–133**	44 4	**250–254**	66 8	**451–460**	91 5
57–58	23 4	**134–136**	44 10	**255–259**	67 1	**461–470**	92 1
59–61	23 9	**137–138**	46 3	**260–264**	67 8	**471–480**	92 8
62–63	24 5	**139–141**	46 8	**265–269**	68 9	**481–490**	92 11
64–65	25 0	**142–145**	47 8	**270–274**	69 2	**491–500**	94 2
66–67	26 5	**146–147**	48 4	**275–279**	70 5	**501–510**	96 0
68–69	27 1	**148–150**	48 7	**280–284**	70 8	**511–520**	96 3
70	27 6	**151–155**	49 2	**285–289**	71 6	**521–530**	97 11
71–72	28 1	**156–157**	49 10	**290–294**	72 4	**531–540**	98 4
73–74	28 9	**158–161**	51 8	**295–299**	73 4	**541–550**	99 5
75–76	29 10	**162–166**	51 11	**300–301**	73 9	**551–560**	100 3
77–78	30 5	**167–171**	52 6	**302–306**	74 2	**561–570**	101 3
79–80	31 6	**172–176**	53 2	**307–311**	75 3	**571–580**	102 4
81–84	32 1	**177–181**	53 7	**312–316**	76 1	**581–590**	102 9
85–89	32 4	**182–186**	54 9	**317–321**	76 10	**591–600**	103 6

The above rates are applicable to the undermentioned descriptions of Unseasoned Hardwood timber and shall be subject to the truckload minima specified hereunder :—

	Per 11 ton truck.	**Per 16, 22 or 27 ton truck.**	**Per "QR" truck.**	**Smaller quantities.**
	T. C. Q.	T. C. Q.	T. C. Q.	Class.
Sawn, dressed or undressed n.o.s.	8 10 0	13 17 3	20 0 0	"A" rate and conditions
Sleepers, sawn or hewn ...				
Palings, sawn or split ...	8 10 0	14 10 0	...	"A" rate and conditions
Pickets, Battens, Droppers ...				
Post and Rails				
Cut to sizes for case making ...	8 10 0	14 10 0	...	"A" rate Smalls Minimum (Maximum rate 98s. 7d. per ton)
Poles and Logs in short lengths loaded in 4-wheeled trucks without overhanging ...	8 10 0	14 10 0	...	"A" rate and conditions

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OWNER'S RISK RATES PER TON FOR SAND.

For carriage at Commissioners' Risk see General Condition No. 4.

Miles	Rate	Miles	Rate	Miles	Rate	Miles	Rate
	s. d.		s. d.		s. d.		s. d.
1–10	8 4	81–84	25 2	177–181	42 4	317–321	60 6
11–13	9 1	85–89	25 6	182–186	43 0	322–326	60 10
14–17	9 7	90–93	26 7	187–191	43 11	327–331	61 5
18–26	9 10	94–97	26 9	192–196	44 3	332–336	61 11
27–28	10 2	98–99	27 4	197–200	44 11	337–341	62 1
29–31	10 9	100–101	27 10	201–204	45 4	342–350	63 5
32–33	11 3	102–103	29 1	205–206	46 1	351–360	64 5
34–35	11 7	104–107	29 5	207–210	46 5	361–370	64 7
36–37	12 6	108–110	29 10	211–212	46 11		
38–40	13 0	111–112	30 2	213–216	47 10	371–380	66 1
				217–221	48 7	381–390	67 1
41–42	13 8	113–115	30 9	222–226	49 1	391–400	67 6
43–44	13 11	116–117	31 2	227–231	49 5	401–410	67 11
45–46	14 7	118–121	32 5	232–236	50 0	411–420	68 7
47	15 2	122–123	32 11	237–241	50 4	421–430	69 6
48–50	15 10	124–126	33 3	242–246	50 9	431–440	69 8
51–52	16 3	127	33 8	247–249	51 11	441–450	71 3
53–54	17 1	128–130	33 11	250–254	52 6	451–460	72 1
55–56	17 10	131–133	35 0	255–259	52 10	461–470	72 6
57–58	18 4	134–136	35 4	260–264	53 4	471–480	73 0
59–61	18 9	137–138	36 5	265–269	54 1	481–490	73 3
62–63	19 3	139–141	36 9	270–274	54 6	491–500	74 1
64–65	19 8	142–145	37 6	275–279	55 5	501–510	75 8
66–67	20 10	146–147	37 10	280–284	55 10	511–520	75 11
68–69	21 5	148–150	38 3	285–289	56 5	521–530	77 1
70	21 8	151–155	38 11	290–294	57 0	531–540	77 8
71–72	22 1	156–157	39 2	295–299	57 8	541–550	78 3
73–74	22 8	158–161	40 9	300–301	58 0	551–560	78 11
75–76	23 8	162–166	41 1	302–306	58 4	561–570	79 10
77–78	24 1	167–171	41 5	307–311	59 5	571–580	80 8
79–80	24 10	172–176	41 8	312–316	60 2	581–590	81 1
						591–600	81 5

The above rates are subject to the undermentioned truckload minima :—

Per 11 ton truck ... ... 10 tons 9 cwt.
Per 16, 22 or 27 ton truck ... ... 15 tons 4 cwt.
Per Bogie truck ... ... 25 tons.

Smaller quantities must be in bags and shall be charged Class "A" rate and conditions.

OWNER'S RISK RATES PER TON FOR WHEAT.

For carriage at Commissioners' Risk, see General Condition No. 4.

Miles	* Truck-loads Rate	Minimum 3 tons Rate	Miles	* Truck-loads Rate	Minimum 3 tons Rate	Miles	* Truck-loads Rate	Minimum 3 tons Rate
	s. d.	s. d.		s. d.	s. d.		s. d.	s. d.
1–10	10 0	10 0	75–77	34 8	38 10	291–300	60 5	74 1
11–13	10 11	10 11	78–81	35 1	41 5	301–310	61 6	75 6
14–17	11 6	11 6	82	35 1	42 9	311–320	62 4	76 2
18–20	12 10	13 5	83–85	36 3	42 9	321–330	62 6	77 5
21	13 5	13 5	86	36 3	43 10	331–340	63 0	77 10
22–23	13 9	14 11	87–89	38 0	45 3	341–350	64 10	78 9
24–25	14 11	14 11	90	38 0	45 11	351–360	65 8	79 6
26	15 1	17 4	91–93	38 10	46 3	361–370	66 6	80 5
27–29	15 10	17 4	94	38 10	46 6	371–380	67 0	82 6
30	15 10	19 4	95–99	39 7	47 7	381–390	67 11	83 0
31–33	17 6	19 4	100–104	39 7	49 11	391–400	68 5	83 5
34	17 6	20 8	105–109	41 4	50 5	401–410	69 1	84 2
35–36	19 6	20 9	110–114	42 6	52 1	411–420	70 0	84 8
37	20 9	20 9	115–124	43 3	53 8	421–430	70 8	86 6
38–39	20 10	22 10	125–134	44 6	54 8	431–440	71 4	87 0
40–41	21 8	22 10	135–144	45 8	56 1	441–450	71 8	87 6
42	21 8	24 5	145–154	47 1	58 2	451–460	72 7	89 1
43–45	22 11	24 5	155–164	47 5	59 1	461–470	73 9	90 4
46–48	23 8	26 8	165–174	48 9	59 10	471–480	74 8	90 9
49	25 10	26 8	175–184	50 3	61 7	481–490	74 10	91 3
50	25 10	27 10	185–190	51 3	62 6	491–500	75 6	92 2
51–53	26 1	27 10	191–200	52 4	64 1	501–510	75 10	93 9
54–55	27 5	30 3	201–210	53 8	65 10	511–520	77 5	94 6
56–57	28 8	30 3	211–220	54 0	67 2	521–530	77 11	95 11
58–61	28 8	32 3	221–230	55 5	68 2	531–540	78 10	96 3
62–65	29 8	33 10	231–240	56 1	69 5	541–550	79 11	96 9
66	29 8	35 3	241–250	56 6	70 1	551–560	80 6	98 2
67–69	31 3	35 3	251–260	58 0	70 11	561–570	81 7	98 5
70–71	32 0	37 9	261–270	58 8	71 6	571–580	82 1	99 11
72–73	33 8	37 9	271–280	59 5	72 6	581–590	83 2	100 8
74	33 8	38 10	281–290	59 7	73 8	591–600	83 7	101 11

* *Subject to undermentioned truckload minima* :—

In Bulk
- Per 27 ton "GZ" or "IZ" truck ... 18 tons
- „ 22 „ "GY" or "HY" „ ... 19 tons
- „ 22 „ "IY" or 16 ton "I" „ 15 tons 4 cwt.

In Bags
- Wheat n.o.s. ... ... ; Wheat, ground or crushed without addition of ingredients for stock food and so consigned... ... ; Wheat meal ... ...
  - Per 11 ton truck ... 10 tons 9 cwt.
  - „ 16, 22 or 27 ton truck... 15 tons 4 cwt.
  - „ higher capacity truck ... 25 tons
- Wheat offal and combings ...
  - Per 11 ton truck ... 7 tons
  - „ 16, 22 or 27 ton truck... 10 tons

Consignments of less than three tons shall be charged—

Minimum 2 tons ... Class "A" rate less 10 per cent.
Smaller quantities ... Class "B" rate less 10 per cent. "Smalls" minimum

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OWNER'S RISK RATES PER TON FOR FLOUR, BRAN, AND POLLARD.

For carriage at Commissioners' Risk, see General Condition No. 4.

Miles	* Truck-load Rate	Minimum 3 tons Rate	Miles	* Truck-load Rate	Minimum 3 tons Rate	Miles	* Truck-load Rate	Minimum 3 tons Rate
	s. d.	s. d.		s. d.	s. d.		s. d.	s. d.
1–10	8 9	10 0	110–114	34 0	41 8	401–410	55 3	67 4
11–13	9 4	10 11	115–124	34 7	42 11	411–420	56 0	67 9
14–16	9 4	11 0	125–134	35 7	43 9	421–430	56 6	69 2
17	10 3	11 6	135–144	36 6	44 10	431–440	57 1	69 7
18–20	10 3	12 3	145–154	37 8	46 6	441–450	57 4	70 0
21–23	11 0	13 4	155–164	37 11	47 3	451–460	58 1	71 3
24–26	12 1	14 8	165–174	39 0	47 10	461–470	59 0	72 3
27–30	12 8	15 6	175–184	40 2	49 3	471–480	59 9	72 7
31–34	14 0	16 6	185–190	41 0	50 0	481–490	59 10	73 0
35–36	15 7	18 7	191–200	41 10	51 3	491–500	60 5	73 9
37–39	16 8	19 6	201–210	42 11	52 8	501–510	60 8	75 0
40–42	17 4	21 0	211–220	43 2	53 9	511–520	61 11	75 7
43–45	18 4	22 0	221–230	44 4	54 6	521–530	62 4	76 9
46–48	18 11	23 0	231–240	44 10	55 6	531–540	63 1	77 0
49–50	20 8	24 5	241–250	45 2	56 1	541–550	63 11	77 5
51–53	20 10	25 3	251–260	46 5	56 9	551–560	64 5	78 6
54–55	21 11	25 11	261–270	46 11	57 2	561–570	65 3	78 9
56–61	22 11	27 9	271–280	47 6	58 0	571–580	65 8	79 11
62–66	23 9	28 11	281–290	47 8	58 11	581–590	66 6	80 6
67–69	25 0	29 1	291–300	48 4	59 3	591–600	66 10	81 6
70–71	25 7	30 7	301–310	49 2	60 5			
72–74	26 11	31 11	311–320	49 10	60 11			
75–77	27 9	33 2	321–330	50 0	61 11			
78–82	28 1	34 2	331–340	50 5	62 3			
83–86	29 0	35 1	341–350	51 10	63 0			
87–90	30 5	36 9	351–360	52 6	63 7			
91–94	31 1	37 2	361–370	53 2	64 4			
95–99	31 8	38 1	371–380	53 7	66 0			
100–104	31 8	39 11	381–390	54 4	66 5			
105–109	33 1	40 4	391–400	54 9	66 9			

The rates provided for flour in truckloads shall also apply to self-raising flour in the specified truckloads when forwarded in jute or calico bags.

* *Subject to the undermentioned truckload minima* :—

	Flour tons cwt.	Bran and Pollard tons cwt.
Per 11 ton truck	10 9 ...	9 10
Per 16, 22 or 27 ton truck	15 4 ...	14 5
Per "QR" truck...	25 0 ...	19 0
Per other bogie truck	25 0 ...	25 0

Consignments of less than three tons shall be charged :—

Minimum 2 tons ... ... Class "A" rate less 10 per cent.

Smaller Quantities... ... Class "B" rate less 10 per cent. "Smalls" minimum.

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OWNER'S RISK RATES PER TON FOR COAL.

For carriage at Commissioners' Risk, see General Condition No. 4.

Miles	Rate	Miles	Rate	Miles	Rate	Miles	Rate
	s. d.		s. d.		s. d.		s. d.
1–10	11 1	**71–72**	34 10	**142–145**	59 8	**250–254**	82 10
11–13	12 1	**73–74**	35 10	**146–147**	60 0	**255–259**	83 10
14–17	12 9	**75–76**	37 4	**148–150**	60 8	**260–264**	84 6
18–21	14 11	**77–78**	38 2	**151–155**	61 8	**265–269**	85 10
22–26	15 8	**79–80**	39 6	**156–157**	62 4	**270–274**	86 6
27–28	16 2	**81–84**	40 2	**158–161**	64 6	**275–279**	88 2
29–31	17 2	**85–89**	40 4	**162–166**	64 8	**280–284**	88 8
32–33	17 10	**90–93**	42 0	**167–171**	65 8	**285–289**	89 0
34–35	18 4	**94–97**	42 6	**172–176**	66 2	**290–294**	90 4
36–37	19 8	**98–99**	43 6	**177–181**	66 8	**295–299**	91 4
38–40	20 10	**100–101**	44 2	**182–186**	68 4	**300–301**	92 0
41–42	21 8	**102–103**	46 2	**187–191**	69 8	**302–306**	92 8
43–44	22 0	**104–107**	46 6	**192–196**	70 4	**307–311**	94 2
45–46	22 8	**108–110**	47 0	**197–200**	71 0	**312–316**	95 2
47	24 0	**111–112**	47 10	**201–204**	71 8	**317–321**	95 10
48–50	25 4	**113–115**	48 8	**205–206**	72 10	**322–326**	96 8
51–52	25 10	**116–117**	49 6	**207–210**	73 8	**327–331**	97 0
53–54	27 4	**118–121**	51 8	**211–212**	74 6	**332–336**	98 4
55–56	28 0	**122–123**	52 4	**213–216**	75 10	**337–341**	98 8
57–58	29 0	**124–126**	52 8	**217–221**	76 10	**342–350**	101 0
59–61	29 8	**127**	53 2	**222–226**	77 8	**351–360**	102 0
62–63	30 8	**128–130**	53 10	**227–231**	78 6	**361–370**	102 8
64–65	31 4	**131–133**	55 6	**232–236**	79 0	**371–380**	104 10
66–67	33 4	**134–136**	56 4	**237–241**	79 10	**381–390**	106 6
68–69	34 0	**137–138**	58 0	**242–246**	80 8	**391–400**	107 0
70	34 2	**139–141**	58 4	**247–249**	82 6		

Notes.

(*a*) The ordinary rates for the carriage of coal shall be the "Coal" mileage rates shown above.

(*b*) In respect of coal forwarded by rail on the "Up" journey from any coal-field in Victoria direct to any station, the rate for the carriage of such coal over the whole or any part of the line from the coalfield to Melbourne shall be 3.414 pence per ton per mile and the rate for the carriage of such coal on any other line shall be the difference between the "Coal" mileage rate, for the mileage of the journey from the coalfield to Melbourne, or any portion of such journey over which the coal has been hauled, and the "Coal" mileage rate for the total mileage from the coalfield to the station of destination, provided that the minimum charge to be paid by the consignor or consignee shall be 14s. 8d. per ton subject to "Coal" mileage rate as a maximum; and that the freight charge to be paid by the consignor or consignee for the carriage of coal from the coalfield to any station on any other line, except as shown in clause (*c*), shall not be less than the "Coal" mileage rate from Melbourne to such station and not less than 3.414 pence per ton per mile for the total mileage from the coalfield to the destination station. The "Coal" mileage rates shown above for the total mileage may be charged if in any instance it is cheaper than to compute the freight charges as prescribed in the preceding parts of this clause.

(*c*) The rate on Brown Coal forwarded direct from Victorian Brown Coal Mines at Yallourn, Winchelsea, Ballan, Gelliondale, Dean Marsh, Bacchus Marsh or Parwan to any station on the Victorian Railways System either on—

(i) Up journey,
(ii) Combined Up and Down journey, or
(iii) Down journey.

shall not exceed the Firewood plus 20 per cent. rate for the mileage from the forwarding to the destination station, provided, however, that the minimum charge for the carriage of any such coal shall be 14s. 8d. per ton subject to "Coal" mileage rate as a maximum.

(*d*) The above rates are subject to the undermentioned truckload minima :—

Per 11 ton truck	10 tons 9 cwt.
Per 16, 22 or 27 ton truck	15 tons 4 cwt.
Per "Q.R." truck	22 tons 0 cwt.
Per other bogie truck	25 tons 0 cwt.

Smaller quantities must be in bags and shall be charged Class "A" Rate and conditions.

OWNER'S RISK RATES PER TON FOR FIREWOOD.

For carriage at Commissioners' Risk, see General Condition No. 4.

Miles	Rate	Miles	Rate	Miles	Rate	Miles	Rate
	s. d.		s. d.		s. d.		s. d.
1–10	7 3	66–67	23 9	172–174	39 0	371–380	55 3
11–21	9 4	68–69	24 2	175–179	39 7	381–390	56 0
22–23	9 7	70	24 9	180–184	40 2	391–400	56 11
24–25	10 3	71–72	25 7	185–191	40 7	401–410	57 8
26	10 8	73–74	26 1	192–196	41 3	411–420	58 1
27–28	11 3	75–84	26 10	197–200	42 6	421–430	58 7
29–31	11 11	85–94	27 9	201–210	43 1	431–440	60 0
32–33	13 1	95–104	28 4	211–220	44 2	441–450	61 0
34–35	13 6	105–114	28 9	221–230	44 7	451–460	61 8
36–37	14 4	115–117	29 5	231–240	46 3	461–470	62 1
38–40	14 10	118–120	29 10	241–250	46 11	471–480	62 8
41–42	15 3	121–124	31 1	251–260	47 3	481–490	63 2
43–44	16 5	125–127	31 10	261–270	48 4	491–500	64 4
45–46	16 11	128–130	32 3	271–280	48 7	501–510	65 2
47	17 4	131–134	32 11	281–290	49 10	511–520	65 8
48–50	18 1	135–137	33 6	291–300	50 3	521–530	66 0
51–52	18 7	138–140	33 9	301–310	50 10	531–540	66 10
53–54	18 11	141–145	35 0	311–320	51 5	541–550	67 3
55–56	20 3	146–149	35 5	321–330	51 11	551–560	68 6
57–58	21 1	150–154	36 3	331–340	52 4	561–570	68 9
59–61	21 6	155–159	36 10	341–350	53 9	571–580	69 4
62–63	21 11	160–166	37 6	351–360	54 4	581–590	70 0
64–65	22 9	167–171	38 6	361–370	55 0	591–600	70 8

See Loading Scale, page 118 for tonnage to be charged.

Smaller quantities must be in bags and shall be charged Class "A" rate and conditions.

See page 121 for Conditions for the Conduct of Traffic at Suburban Firewood Stations.

LOADING SCALE AND TONNAGE TO BE CHARGED FOR SHORT FIREWOOD AND MALLEE ROOTS.

Class and Description of Truck	Capacity of Truck	Height of Loading	SHORT FIREWOOD (See Notes on page 120) BLOCKS Box, Apple, Black, Brown, Grey, Spotted, Yellow, Buloke (Sawn) under 3 ft. long	BLOCKS Red Box, Long leaf Box, Belar and Ironbark (Sawn) under 3 ft. long	SHORT FIREWOOD n.o.s. (Sawn or not sawn) under 3ft. long	MALLEE ROOTS	
			Tonnage to be charged	Tonnage to be charged	Tonnage to be charged	Height of Loading	Tonnage to be charged
	Tons	ft. ins.	tons cwt.	tons cwt.	tons cwt.	ft. ins.	tons cwt.
I. and I.A. High Sided ...	16	Water level	12 0	11 5	10 5	Up to 2 ft. 6 in. above water level	10 0
I.A. High Sided when used as	11	2 8	7 0	6 10	6 0	4 11	6 0
I. and I.A. Swing Door	16	Water level	12 0	11 5	10 5	Up to 2 ft. 6 ins. above water level	10 0
I.A. Swing Door when used as	11	2 2	7 0	6 10	6 0	4 0	6 0
I. and I.A. Drop-door ...	16	Water level	12 0	11 5	10 5	Up to 2 ft. 6 ins. above water level	10 0
I.A. Drop Door when used as	11	2 0	7 0	6 10	6 0	3 7	6 0
I.Y.	22	Water level	12 0	11 5	10 5	Up to 2 ft. 6 ins. above water level	10 0
I.Z. and G.Z.	27	Water level	14 5	13 8	12 5	Up to 2 ft. 6 ins. above water level	11 7
I.Z. and G.Z. when used as ...	16	3 6	12 0	11 5	10 5	Up to 1 ft. 9 ins. above water level	10 0
G.Y. and H.Y.	22	Water level	15 10	14 10	13 5	Up to 2 ft. 6 in. above water level	12 0
G.Y. and H.Y. when used as ...	16	3 6	12 0	11 5	10 5	Up to 1 ft. 4 in. above water level	10 0
Q.R. Nos. 1 to 376 and number 407	26 & 30	Water level	14 5	13 5	12 5	Up to 2 ft. 6 in. above water level	12 0
Q.R. numbered 377 to 406 inclusive	30	Water level	16 10	15 5	14 5	Up to 2 ft. 6 in. above water level	14 0
"E"	44	Water level	29 5	27 10	25 0	Up to 2 ft. 6 in. above water level	22 10
N.Q. (narrow gauge) ...	11	2 8	7 0	6 10	6 0	—	—
N.Q. from and to stations on narrow gauge lines only.	11	3 4	—	—	7 10	—	—

LOADING SCALE AND TONNAGE TO BE CHARGED FOR LONG FIREWOOD, VINE STAKES AND ROUGH AND SPLIT TIMBER SPECIFIED.

Class and Description of Truck	Capacity of Truck.	LONG FIREWOOD AND VINE STAKES (See notes on page 120)								ROUGH and SPLIT TIMBER, specified. (See notes 2, 7 and 8 on page 120)		
		3 ft. and 3 ft. 6 in. long		4 ft. and 4 ft. 6 in. long		5 ft. long		6 ft. long		Height of Loading		
		Height of Loading	Tonnage to be Charged	Height of Loading	Tonnage to be Charged	Height of Loading	Tonnage to be Charged	Height of Loading	Tonnage to be Charged	Split Staves for Cask Making	Other Timber	Tonnage to be Charged
	Tons	ft. ins.	tons cwt.	ft. ins.	tons cwt.	ft. ins.	tons cwt.	ft. ins.	tons cwt.	ft. ins.	ft. ins.	tons cwt.
I. and I.A. High Sided	16	6 0	11 10	6 0	11 10	6 0	11 10	Water level	11 10	6 0	Water level	14 10
I.A. High Sided when used as ...	11	4 0	7 10	4 7	9 0	5 0	10 0	5 4	10 12	3 7	2 8	8 10
I. and I.A. Swing Door ...	16	5 0	12 0	5 9	13 10	5 10	14 0	6 0	14 8	4 10	Water level	14 10
I.A. Swing Door when used as ...	11	3 3	7 10	3 9	9 0	4 1	10 0	4 4	10 12	2 11	2 2	8 10
I. and I.A. Drop Door	16	5 0	12 10	5 7	14 0	5 7	14 0	6 0	15 0	4 7	Water level	14 10
I.A. Drop Door when used as ...	11	3 0	7 10	3 7	9 0	4 0	10 0	4 2	10 12	2 8	2 0	8 10
I.Y.	22	5 0	12 0	5 9	13 10	5 10	14 0	6 0	14 8	4 10	Water level	14 10
I.Z. and G.Z.	27	5 0	12 14	5 9	14 5	5 10	14 16	6 0	15 4	4 10	— Water level	15 6 17 5
I.Z. and G.Z. when used as ...	16	4 9	12 0	5 5	13 10	5 6	14 0	5 8	14 8	4 7	3 6	14 10
G.Y. and H.Y.	22	5 0	12 14	5 9	14 5	5 10	14 16	6 0	15 4	—	—	—
G.Y. and H.Y. when used as ...	16	4 9	12 0	5 5	13 10	5 6	14 0	5 8	14 8	—	—	—
Q.R. Nos. 1 to 376 and number 407	26 & 30	4 6	16 0	5 0	18 0	5 0	18 5	Water level 5 0	13 5 18 5	4 2	Water level	18 0
Q.R. numbered 377 to 406 inclusive	30	4 6	18 10	5 0	20 15	5 0	20 18	Water level 5 0	15 5 20 18	4 2	Water level	20 15
"E"	44	5 0	25 0	5 9	26 15	5 10	27 15	6 0	28 10	—	—	—
N.Q. (narrow gauge)	11	3 11	7 10	4 8	9 0	5 2	10 0	5 6	10 12	3 6	2 10	8 10
N.Q. from and to stations on narrow gauge lines only	11	—	—	5 2	10 0	—	—	—	—	—	—	—

NOTES.

1. Narrow-gauge truck numbered 80, which is fitted with grated floor, may be loaded 4 inches higher than the height specified in the loading scale.

2. The loading of trucks containing long firewood and split staves for cask making must be secured by a fence consisting of pieces of timber placed upright along the ends and sides of truck. When the timber is not long enough to obtain the requisite height otherwise, a foundation must first be made by placing sufficient wood along the floor of the truck to attain the necessary height, and the fence built on such foundation (see note 10).

3. Rough firewood of varying lengths which cannot be safely secured above water level according to the loading scale for long firewood shall be loaded to water level and charged the scale weights applicable to short firewood, N.O.S.

4. The tonnage to be charged in respect of box, red box, and ironbark firewood cut from limbs of trees and axed into lengths under 3 feet long shall be computed under the scale prescribed for other short firewood.

5. Truck loads of mixed descriptions of firewood shall be charged the highest tonnage minimum prescribed for any of the descriptions of firewood contained in the truck.

6. Waste ends and chips bagged from firewood cutting mills shall be charged a minimum of 9½ tons per 11-ton truck, and 15 tons per 16, 22 or 27 ton truck. If sent loose they shall be charged as short firewood N.O.S.

7. Rough and split timber comprises—(*a*) droppers, pickets, shingles, fodderboards, battens, billets for case-making, blackwood billets, kanooka and needlewood billets (for pipe making), spoke billets, sawn staves for cask making, props, lids and caps ; (*b*) palings, mining laths and mining slabs with least possible number standing up.

8. Rough and split timber for mining.—exceeding 8 feet in length shall, when forwarded in "Q.R." trucks, be loaded to a height of 49 inches, and the freight computed as for 18 tons per "Q.R." trucks numbered 1 to 376 and number 407, and 20 tons 15 cwt. per "Q.R." truck numbered 377 to 406. A sufficient number of upright stays and lashings is to be used to secure loading.

9. Trucks loaded in excess of the heights specified will not be permitted to go forward, but will be detained until adjusted at the expense of the owner.

10. The loading in 26 and 30-ton "QR" trucks of 6 ft. firewood with a 6 ft. fence around the side of the truck, **is not permitted**. Any 6 ft. firewood loaded in these types of trucks shall be loaded within the truck to water level or to a height of 5 feet within a fence of 5 ft. firewood standing on the floor of truck. Five feet firewood may be loaded in these types of trucks to a height of 5 ft. by means of a **fence of 5 ft. firewood standing on the floor of the truck.**

Conditions for the Conduct of Traffic at Suburban Firewood Stations.

1. In every instance in which the area abutting on sidings is divided into allotments, the limit of each allotment, or of contiguous allotments held by one and the same person, will be marked by a post fixed at each of the four corners of the area.

The rents of allotments will require to be paid monthly in advance.

2. Storage at the rate of 6s. 0d. per day per allotment of 25 feet frontage will be charged upon all consignments allowed to remain on the ground on other than leased allotments over twelve hours after the truck is unloaded, and payment will be required on demand before the consignments are removed off the ground.

Any portion of a day exceeding twelve working hours after a truck has been unloaded is to be computed as one day, and any portion of an allotment to be computed as an allotment. This charge applies to each foot of frontage, which at any portion of its depth is occupied by consignments—that is to say, this charge for storage will not be increased or reduced by reason that the cubic capacity for stacking on the said frontage may or may not be made fully available.

3. On all allotments abutting on the siding, a space of 4 feet 9 inches from the rails must always be kept clear by the consignee immediately a truck is discharged, or in his default, the work will be done by the Commissioners, at a cost to the consignee of 6s. 0d. per hour per man for the time so employed.

4. A receipt must be given for each truck or consignment when the truck is placed at the proper discharging point. Should the lessee or the consignee not be present himself or his agent to give a receipt, the unloading of each truck shall nevertheless proceed, and the lessee who is not present, or neglects to give a receipt within twelve hours after the truck is discharged, shall be held as infringing the conditions of his lease. And upon other than leased allotments, consignments for which no receipts have been given, or for which the railway charges have not been paid on demand, or which have not been removed from the ground within twelve hours after the truck is discharged, shall be liable to the storage charges aforesaid, or to be warehoused, or to be sold at the cost and risk of the said consignee. And no claim shall be recognized by the Commissioners on account of trucks unloaded for which no receipts have been given ; and after receipts have been given the lessee or consignee shall be bound to observe the conditions as to gate passes, where such are necessary.

5. The standard height to which firewood may be stacked on any allotment is ten (10) feet, and a charge of 4s. 0d. per truck will be made for any firewood stacked above that height.

If necessary for the quick and economical discharge of trucks, the consignee must stack up or level down consignments placed on allotments, and must also trim stacks if required by the Commissioners.

A charge of 6s. 0d. per hour per man will be made for trimming stacks if it be done by the Commissioners, and consignees will be informed prior to the work of trimming being commenced.

6. All suburban Firewood Sidings will be open for the removal of firewood, coal and briquettes during the hours and on days specified in General Condition No. 27, page 17.

Storage of Stone, Sand, Metal Screenings, etc. at Suburban Sidings.

The storage rates and conditions specified in clause 2 above shall also have effect in respect to stone, sand, metal screenings, &c., at suburban sidings.

Rates for the Carriage of Empties.

(*a*) The rates for the carriage of **Empty Returns** only apply to containers which have on the outward journey been carried over the railways full, and are being consigned by the original consignee at the original receiving station to the original consignor at the sending station. Consignors of returned empties must declare on the consignment note that the identical returned empties were conveyed full by rail on the forward journey, otherwise the higher rate provided in clause (*c*) shall operate.

(*b*) **Empty petrol and kerosene cases** which have been used for the carriage of petrol or kerosene by rail on the forward journey, and are so declared on the consignment note, may be returned from the original destination station at the empty return rates, irrespective of whether they are sent to the original despatching station or to the original consignor.

(*c*) **Empty Containers,** which are sent on the forward journey by rail to be despatched, when full by road, boat, or by any method of transportation other than rail or **empty containers** which when full were forwarded by road, boat, or any method of transportation other than rail but are returned by rail shall be charged under Class 2 rate subject to the Tariff for "Smalls."

(*d*) **Empty Containers** which have contained poisonous chemicals must be thoroughly washed out by senders before being accepted by rail. The bungs and lead washers of empty sulphuric acid drums must be securely seated. Consignors will be required to give a certificate on the consignment note that all trace of poison has been removed.

(*e*) **Repayment of Freight.**

(1) The freight charges or empties marked * shall be prepaid in cash or freight stamps at the option of the Commissioners.

(2) The freight charges on all empties except those specified in clause (3) forwarded to the Melbourne Goods Depot or to Dynon must be prepaid.

(3) Exceptions : Barrels, casks, crates and cases for all Breweries in Melbourne and gas cylinders for any consignees in Melbourne may be sent to Melbourne freight "to pay."

Description.	Class.	Minimum. ††	
		s. d.	
Barrels—			
*Fruit Pulp	B.	2 0	
Baskets—			
Bread	C.	2 0	
*Fish, new or R.E. in packages, for each 28 lbs. or part thereof	—	—	Up to 101 miles 5d. each, for each succeeding 100 miles or part thereof 3d. each additional or if cheaper Class B minimum 2s. 0d.
Strawberry, including punnets ...	C.	2 0	
Boxes or Cases—			
Ammunition viz—Wooden or Iron, ... new or R.E. such as for Cordite, Fuse, Shells for Defence Services and Munitions Department	B.	S.M.	
*Butter, new or R.E. (including wrapping paper up to 1 lb.)—			
Not exceeding 28 lbs. each ...	—	—	Up to 101 miles 5d. each for each succeeding 100 miles or part thereof 3d. each additional or if cheaper Class B minimum 2s. 0d.
Exceeding 28 lbs. each	—	—	Double above package rates or Class "B," minimum 2s. 0d. when cheaper
Cheese, new or *R.E.	A.	2 0	
Collapsible			
Angle Brace, when collapsed, new or R.E.	A.	2 0	

Empties—continued.

Description.	Class.	Minimum. ††	—
		s. d.	
Boxes or Cases—*continued.*			
*Egg, new or R.E. (including Fillers) ...	...	...	Same as butter boxes,
*Fish, new or R.E., irrespective of weight, each box	...	...	Up to 101 miles 5d. each, for each succeeding 100 miles or part thereof 3d. each additional or if cheaper Class B minimum 2s. 0d.
Fruit—			
New	A.	2 0	
*R.E. (truck loads may be accepted freight "to pay")	A.	2 0	
*Kerosene, R.E.	A.	2 0	
Milk, concentrated, new or R.E. ...	A.	2 0	
Motor Body	2	...	
New, N.O.S.	A.	2 0	
Petrol Pump	C.	S.M.	
Piano	C.	S.M.	
*R.E., N.O.S.	A.	2 0	
Sweat—			
New or *R.E.	A.	2 0	
Test for Milk or Cream	...	...	See page 0
Buckets, in nests or crated—			
*Fruit	B.	2 0	
*Raspberry, R.E.	...	...	Up to 101 miles 5d. each, for each succeeding 100 miles or part thereof 3d. each additional or if cheaper Class B minimum 2s. 0d.
Cans—			
Milk and Cream and Cream Billies new	C.	2 0	
" " " " *R.E.	...	...	Up to 101 miles 5d. each, for each succeeding 100 miles or part thereof 3d. each additional or if cheaper Class B minimum 2s. 0d.
*Mineral Water, small, R.E.	...	...	Same as butter boxes
Carboys, protected by bagging or wicker-work in cases or crates—			
Acid, R.E.	B.	...	
New or not returned	C.	...	
Cartons—			
Cardboard in crates *R.E.	C.	2 0	
Cardboard, collapsed, R.E., in bags, crates or cases	A.	2 0	
Cases. See Boxes.			
Caskets, wooden, for protecting coffins, R.E.	C.	S.M.	
Casks (see page 127).			
Cones and Tubes, paper, R.E.	A.	2 0	
Containers, metal, collapsible, new, or returned empty	A.	2 0	

Empties—continued.

Description.	Class.	Minimum. ††	—
		s. d.	
Coops—			
Collapsible, wooden and wire—			
In packages, N.O.S.	C.	2 0	
,, Used at agricultural shows	...	...	Free return at Owner's Risk to sending station on production of usual certificate from the Secretary of the Show.
*Poultry, R.E.—			Not over 101 miles each: s. d. / 102 to 150 miles each: s. d. / Over 150 miles each: s. d.
Not exceeding 19 cubic feet ...	...	...	1 1 / 1 11 / 2 9
Exceeding 19 cubic feet	...	...	1 11 / 2 9 / 3 4
			(or B Class minimum 2s.0d. if cheaper.)
*New, sent to the country to be filled for return journey	...	...	Same as Coops, Poultry R.E.
New, N.O.S.	2	...	
Crates—			
Aerated water, R.E. to Victorian Country Industries	A.	2 0	
Beer, new or R.E.	B.	2 0	
Cheese, new or R.E.	A.	2 0	
Collapsible—			
Wooden and Wire—			
In packages, N.O.S.	C.	2 0	
Used at agricultural shows ...	...	...	Free return at Owner's Risk to sending station on production of the usual certificate from the Secretary of the Show.
For packing concrete pipes on return to original forwarding station	A.	2 0	
Fittings for packing asbestos sheets, returned to original forwarding stations	A.	2 0	
For drying potatoes	A.	2 0	
Stove	B.	2 0	
Angle Brace, new or R.E., when collapsed	A.	2 0	
Cordial new or R.E.	B.	2 0	
Flower and plant	A.	2 0	
*Fruit, new or R.E.	A.	2 0	
*Honey, new or R.E.	A.	2 0	
*New, sent to the country to be filled for return journey	...	...	Same as Coops, Poultry R.E.
N.O.S.—			
New	2	...	
R.E.	C.	2 0	
Onion, new or R.E.	A.	2 0	
*Pig, R.E.	...	...	each s. d.: Not over 50 miles ... 1 11; 51 to 101 miles ... 3 4; 102 to 150 miles ... 4 9; Over 150 miles ... 6 5
Pottery, new or R.E.	C.	2 0	
*Poultry, R.E.	...	...	Same as Coops, Poultry R.E.

Empties—continued.

Description.	Class.	Minimum. ††	—
		s. d.	
Crates—*continued.*			
Rabbit—			
*N.O.S. new or returned ...	...	...	
Not exceeding 8 cubic feet, irrespective of weight ...	...	...	Up to 101 miles 5d. each, for each succeeding 100 miles or part thereof 3d. additional or if cheaper Class B minimum 2s.0d.
Exceeding 8 cubic feet in measurement	B.	2 0	
Export, new or R.E., or in shooks ...	A.	2 0	
*Sheep, R.E....	...	...	Same as Crates, pig, R.E.
*Vegetable, R.E.	...	...	Same as Coops, Poultry, R.E.
Wine, R.E.	C.	2 0	
Cylinders, new or R.E.—			
Gas	C.	2 0	
Mineral Water	A.	2 0	
Cylinders for Explosives, R.E. ...	...	...	*Free at Owner's Risk*
Drums—			
N.O.S. (see page 127)	C.	S.M.	
Hampers—			
Boot, not returned	2	...	
Boot, R.E.	1	...	
For Woollens and Clothing, R.E. ...	1	...	
Vegetable	C.	2 0	
Jars, in cases or crates—			
Acid, R.E.	B.	S.M.	
Confectionery, R.E.	C.	S.M.	
Fruit Juice	B.	2 0	
Mineral Water, R.E.	A.	2 0	
Weed Eradicator, R.E.	A.	...	
Wine, R.E.	B.	2 0	
Magazines—Portable, explosives, R.E. ...	...	...	Free, at Owner's Risk
Reels, wooden, for hose	A.	2 0	
Spools, Yarn, R.E.	A.	2 0	
Tins, new or *R.E., as under—			
may be accepted at Commissioner's Risk when in cases or crates—			
Biscuit — *Fruit*			
Boot Polish — Honey			
Butter — †Kerosene			
Confectionery — Lino Polish			
Custard — †Lubricating Oil			
Disinfectant — †Petrol			
Egg Preservative — Preserves			
Eucalyptus — Saline			
In cases, crates or bags	A.	2 0	
In bundles	B.	2 0	
Loose—			
In truck loads, minimum 30 cwt.	B.		
Smaller lots	1		

† *Empty kerosene, lubricating oil and petrol tins in cases, in truck loads may be sent freight "to pay."*—

To—	At—
Vacuum Oil Co.	Spotswood.
Shell Co.	Spotswood or Newport.
Neptune Oil Co.	Spotswood or Newport.

Empties—continued.

Description.	Class.	Minimum. ††	—
		s. d.	
Tins—*continued.*			
Biscuit Tins, R.E., packed in Transport Vans, fully loaded	A.	2 0	For the total weight of the transport and contents
Milk, condensed or concentrated in cases, crates or bags	A.	2 0	
Fruit Dip	C.	S.M.	
N.O.S., in cases, crates, or bags ...	C.	2 0	
N.O.S., loose	1	...	
Transports, cake (boxes on wheels used as containers for cake) R.E. ...	B.	S.M.	
,, for fruit tins, R.E.	B.	S.M.	
,, cases or boxes N.O.S. ...	1	...	
Trays—			
Drying, new	A.	2 0	
Egg	...	...	Same as Butter Boxes
Trunks—Boot	...	...	Same as Hampers, Boot
Tubes and cones, paper, R.E.	A.	2 0	
Tubs—			
*Butter	...	...	Same as Butter Boxes
Ice Cream	C.	2 0	
Wrappers—			
Bacon and Meat, New or *R.E. ...	A.	2 0	

†† Where no entry appears in this column the ordinary class weight minimum shall apply (see *General Condition* No. 15.) "*S.M.*" *indicates* "*Smalls*" *Minimum.*
"*R.E.*" *indicates Returned Empty.*

For carriage at Commissioners' Risk see General Condition No. 4.

Empties—continued.

—	New or not Returned Empty "Smalls" Minimum	Returned Empty. Not Exceeding— Miles 101	Miles 200	Miles 300	Miles 400	Miles 500	Miles 600	
	Class	each s. d.	each s. d.	each s. d.	each s. d.	each s. d.	each s. d.	
†Demijohns, in wickerwork	2							
†Firkins	C.	1 2	1 2	2 2	2 10	3 11	5 0	
†Kegs, not over 10 gallons ...	C.							
†Barrels	C.							
†Quarter Casks	C.							
†Kilderkins	C.	2 2	2 10	3 11	4 5	5 6	7 1	
†Octaves	C.							
†Casks, egg, small ...	C.							
†Hogsheads	C.	2 10	3 11	5 6	7 7	9 4	11 1	Or Class "C" "Smalls" Minimum if cheaper
†Pipes, tallow	Same as Butts							
***Pipes, wine								
*Casks, large, for crockery ...		3 11	5 6	7 7	9 4	10 10	12 7	
***Puncheons, wine ...	C.							
† ,, tallow ...	C.							
†Drums, capacity not exceeding—								
140 gallons		Class C		"Smalls"		Minimum		
90 gallons	C.	5 0	7 2	9 9	11 11	14 0	16 2	
70 gallons	C.	3 11	5 6	7 7	9 4	10 10	12 7	
45 gallons	C.	2 2	2 10	3 11	4 5	5 6	7 1	
25 gallons	C.	1 8	2 2	3 3	3 10	5 0	6 3	

Butts, vats (wooden), pipes, N.O.S. set up, new or returned empty :—

		Class
Not over 100 gallons capacity	...	C. "Smalls" Minimum
101-400 gallons capacity ...	...	2
Over 400 gallons capacity	...	2 plus 50 per cent.
,, ,, ,, ,, and casks, dismantled, in packages	...	C. and conditions

† New or not returned empty when consigned to the country shall be charged at the same rate as empty returns only when a declaration is made on consignment note, that the empties when full will be returned by rail.

*** Pipes and Puncheons (wine).—The returned empty rate only applies when not exceeding 112 gallons—over 112 gallons charge as Butts.

For carriage at Commissioners' Risk see General Condition No. 4.

Rates for the Carriage of Fish.

1. The rates for the carriage of Fish,—fresh, smoked, salted, or dried (Produce of Commonwealth)—also oysters and other shell fish, fresh, shall, except otherwise provided, be as follows :—

	Per ton per mile. Owner's Risk	
From 1 to 100 miles	9d.	Plus 20%
,, 101 ,, 200 ,, ...	5d. additional	
Over 200 miles	3d. ,,	

3s. 6d. per ton terminal (which includes loading and unloading) shall be added, and the charge shall be subject to the tariff for "Smalls."

When sent in consignments of 3 tons or over, a reduction of 20 per cent. shall be allowed off the foregoing rates provided the fish be loaded in the least possible number of trucks.

Fish, Produce of Commonwealth in refrigerator trucks shall be charged tariff rates plus 25% subject to a minimum of the full weight carrying capacity of the truck used.

2. Fish from Anderson, Bairnsdale, Drysdale or Orbost to Melbourne Goods Depot shall be charged as shown hereunder :—

From	Minimum 3 tons per consignment per ton	Smaller Quantities "Smalls" Minimum per ton
	£. s. d.	£ s. d.
Anderson	(a) 2 16 8	3 9 10
Bairnsdale	4 19 6	6 9 6
Drysdale	1 15 6	2 3 7
Orbost	5 11 10	7 13 1

(a) Minimum 1 ton.

The above rates include 3s. 6d. per ton terminal charge.

3. Fish consigned from one consignor at one station (who prepays the freight charges) to various consignees at the same destination station shall be treated and charged as one consignment.

4. Fish carried portion of the journey by Mixed train and the balance of the journey by Passenger train shall be charged Goods rates.

5. Consignments must be delivered at the station 30 minutes prior to the starting time of the train by which they are to be forwarded, and shall be consigned on a Goods consignment note.

6. For carriage at Commissioners' Risk, see General Condition No. 4.

Rates for the Carriage of Cream.

Cream in cans by Goods and Mixed trains shall be charged under Class BB and conditions subject to the following charges for distances not exceeding 90 miles being applied if cheaper.

Miles	Per Can, not exceeding 126 lb. Owner's Risk
	s. d.
1 to 30	1 11
31 to 50	2 9
51 to 70	3 5
71 to 90	4 7
Over 90	Class BB and conditions

See also special rates for single packages of Country Produce, Schedule 2, page 133.

2. **Transfer Charges for Cream at Break of Gauge Stations—**

When waybilling cream from stations on narrow-gauge lines to stations on broad-gauge lines, debit for the transfer charges at the break of gauge station, both for the forward and return journeys shall be raised by the outwards narrow gauge station when waybilling the full cans on the forward journey.

The transfer charges for the both journeys shall be entered on a separate line of the waybill.

3. **Empty Return Cream Cans.**—When waybilling cans of Cream on the forward journey the charge for the return of the empty can shall be added to the freight for the carriage of the full can and included in the debit on the outwards waybill. The empty can shall be returned free to the original forwarding station, and so waybilled.

4. **Sample Cream, or Cream Test Boxes,** shall be charged Cream rates when full and Returned Empty Can rates on the return journey.

5. For carriage at Commissioners Risk, see General Condition No. 4.

Rates for the Carriage of Milk.

BY GOODS AND MIXED TRAINS.

Miles	Per 30-qt. Can (100 lbs.)	Per 40-qt. Can (126 lbs.)	Per 50-qt. Can (160 lbs.)
	Owner's Risk	Owner's Risk	Owner's Risk
	s. d.	s. d.	s. d.
1 to 15	1 1	1 3	1 5
16 to 30	1 5	1 11	2 5
31 to 50	2 3	2 5	2 11
51 to 70	2 11	3 3	3 11
71 to 100	3 7	3 11	5 0
101 to 150	4 6	4 6	5 7
151 to 200	4 9	5 0	6 4

1. Milk forwarded from Glen Forbes to Dandenong shall be charged the 50 mile rate. Milk from Clyde and Officer to Melbourne and Balaclava shall be charged the 30 mile rate.

2. When cheaper than the rates per can shown above, the following rates shall be charged for milk forwarded by Mixed or Goods trains :—

	Class
Minimum 4 tons per 4-wheeled "I" or 11-ton louvre truck	A less 10%
Minimum 6 tons per 15-ton louvre truck	A less 10%
Minimum 1 ton	BB
Smaller quantities	C less 10% "Smalls" Minimum

3. **Milk sent by Mixed trains** on branch lines, thence by Passenger or fast Mixed trains on main lines, shall be charged Passenger train rates for the whole distance.

4. **Transfer Charges for Milk at Break of Gauge Stations.**—When waybilling milk from stations on narrow gauge lines to stations on broad gauge lines, debit for the transfer charges at the break of gauge station, both for the forward and return journeys, shall be raised by the outwards narrow gauge station when waybilling the full can on the forward journey.

The transfer charges for the both journeys shall be entered on a separate line of the waybill.

5. **Empty Return Milk Cans.**—When waybilling cans of Milk on the forward journey the charge for the return of the empty can shall be added to the freight for the carriage of the full can and included in the debit on the outwards waybill. The empty can shall be returned free to the original forwarding station, and so waybilled.

6. When Milk sent to Melbourne or suburban passenger stations is charged Goods train rate on the forward journey, the empty cans shall be charged the Returned Empty Can Goods rates, whether sent by Passenger or Goods train.

7. **Sample Milk Test Boxes** shall be charged milk rates when full, and Returned Empty Can rates on the return journey.

Rates for the Carriage of Boats.

(Owner's Risk)

1. ROWING AND N.O.S., INCLUDING FOR RACING AND SAILING (see (*a*) and (*b*).)

Loaded in	Per truck Per mile	Minimum Charge per truck
Truck	s. d.	£ s. d.
Per 4-wheeled	2 2	4 8 11
Per "Q.R."	3 4	6 14 5
Per 50-ft. "Q." "E.", "S." or "Q.B." *	6 8	13 8 10
Each additional 4-wheeled	1 1	2 4 10

* Add 59s. 9d. for use of "Q.B." truck.

(*a*) When one boat is conveyed in a truck fully loaded with other goods, half the rates per truck per mile shown in clause 1 shall be charged for the boat.

(*b*) An additional charge of £6 3s. 1d. shall be imposed in respect of each placing of trucks for loading or unloading boats at Flinders-street Yard (Jolimont).

2. CANOES AND SIMILAR SMALL BOATS.

When conveyed in trucks loaded with other goods.— Quarter boat rates for each canoe or small boat, subject to the full boat rate per truck per mile as a maximum for any number loaded in the same truck.

N.O.S.—The same rates and conditions as for Rowing Boats apply.

3. MOTOR.

Accessories and removable parts, such as batteries and magnetos, to be consigned and each item shown on the Consignment Note.

Loaded in	Per truck Per mile	Minimum Charge per truck
Truck	s. d.	£ s. d.
Per 4-wheeled	4 7	8 16 11
Per bogie	6 8	13 8 10
Per 50-ft. "Q.", "E.", "S." or "Q.B." *	11 6	22 0 10

* Add 59s. 9d. for use of "Q.B." truck.

New or secondhand boats of all descriptions may be accepted for carriage at Commissioners' Risk at the above rates plus 10%, subject to the following conditions :—

(1) Racing Boats, Canoes and similar small boats shall be properly protected by packing.

(2) Motor Boats must have propeller, rudder and shafting removed and engine wholly encased.

Rates for the Carriage of Vehicles.

The Owner's Risk rates for the carriage by goods train of motor vehicles and other vehicles of all descriptions also motor chassis, trailers, motor bodies or cabs for motor trucks shall be as shown hereunder :—

1. Rate per Railway Truck containing one or more of above road vehicles etc. :—

Per 4-wheeled Railway Truck	Per "QR" Bogie Railway Truck	Per "Q" "E" or "S" Railway Truck
Per mile s. d. 3 4	Per mile s. d. 6 8	Per mile s. d. 10 0

Minimum charge, 107s. 6d.

2. Any Vehicle etc., weighing not more than 7 cwt. not requiring a separate truck and loaded with other goods shall be charged at 1s. 8d. per mile, minimum charge 53s. 9d.

	Weight not exceeding	Per vehicle per mile
3. Trailers loaded in trucks with other goods ...	5 cwt.	10d.
	Minimum charge £2/13/9d.	
Trailers or Floats loaded in trucks with other goods	12 cwt.	1/8d.
	Minimum charge £2/13/9d.	

Full Vehicle Rates and Conditions to apply when cheaper.

4 Trotting Sulkies in pieces (wheels off and securely fastened to seat) shall be charged parcels rates plus 50 per cent.

NOTES.

(*a*) New or secondhand vehicles may be accepted at Commissioners' Risk at the above rates plus 10% when they are in cases or set up protectd by a calico or hessian cover. All moveable lamps and tools to be enclosed in case and all accessories to be secured to vehicles and declared on consignment note. Secondhand vehicles set up are subject to inspection by Stationmaster and secondhand motor vehicles to the proviso that they are driven to the railway station under their own power. When these conditions are not complied with, the vehicles shall be accepted for carriage at Owner's Risk only.

(*b*) Trotting sulkies which have accompanied trotting horses by rail in connexion with Race Meetings shall be returned at half tariff rates, and wherever practicable, afforded similar transport on the return journey as was given on the forward journey, in order that, as far as possible, the sulkies may accompany the horses.

(*c*) Motor Vehicles.—No person shall, without the written authority of the Commissioners, send by railway any motor vehicle—

(1) carrying or accompanied by any petrol or volatile oil or other volatile inflammable substance ; nor

(2) unless all petrol, volatile oil, and other volatile inflammable substance aforesaid be removed from any tanks, piping, carburetters, or other receptacles on the vehicle; nor

(3) unless the connecting cables and wires of all batteries have been disconnected from the battery terminals and the said terminals have themselves been so protected that they cannot be connected inadvertently whilst the vehicle is in the care of the Commissioners.

Declaration to this effect to be lodged by the sender with the consignment note in every instance, and filed therewith at the sending station.

Any person guilty of a breach of this By-law shall be liable to a penalty not exceeding twenty pounds.

Special Owner's Risk Rates for Single Packages of Country Produce.

1. Country Produce.

With the exception of consignments forwarded from Melbourne or from any station situated within 20 miles of Melbourne, single packages of country produce forwarded to any station which is open for Goods traffic in less than truckloads shall be charged at the rates shown hereunder :—

Mileage	Schedule 1		Schedule 2	
	Dried Fruit, Vegetables, Cider, Honey, Perry, and Wine, Produce of the Commonwealth		Butter, Eggs, Cream, Cheese, Ham and Bacon	
	Per Package not exceeding 30 lb. (Honey, 33 lb.) (Dried Fruit, 36 lb.)	Per Package exceeding 30 lb., but not exceeding 60 lb. (Honey, 67 lb.) (Dried Fruit, 67 lb.)	Per Package not exceeding 30 lb.	Per Package exceeding 30 lb., but not exceeding 60 lb.
	s. d.	s. d.	s. d.	s. d.
Up to 25 miles	1 3	2 0	1 3	2 0
26 ,, 50 ,, ...	1 6	2 7	1 6	2 7
51 ,, 101 ,, ...	2 0	2 7	2 3	2 11
102 ,, 150 ,, ...	2 3	2 11	2 7	3 5
151 ,, 200 ,, ...	2 3	2 11	2 11	3 9
201 ,, 250 ,, ...	2 7	3 5	3 0	4 2
251 ,, 300 ,, ...	2 7	3 7	3 5	4 7
301 ,, 400 ,, ...	2 11	4 2	3 7	5 0
401 ,, 500 ,, ...	3 0	4 4	4 1	6 5
501 ,, 600 ,, ...	3 5	5 0	4 2	7 1

Should there be more than one package in a consignment each package may be charged as above if cheaper than the tariff rate.

2. FRESH FRUIT AND TOMATOES.

With the exception of consignments forwarded from suburban passenger stations, single packages of Fresh Fruit and Tomatoes may be forwarded between all stations which are open for either Goods or Passenger business at the rates shown below :—

Mileage	Per Package not exceeding 30 lb.	Per Package exceeding 30 lb., but not exceeding 60 lb.
	s. d.	s. d.
Up to 25 miles...	1 3	2 0
26 ,, 50 ,, ...	1 6	2 7
51 ,, 200 ,, ...	2 0	2 7
201 ,, 300 ,, ...	2 0	2 11
301 ,, 400 ,, ...	2 0	3 5
401 ,, 500 ,, ...	2 0	3 9
501 ,, 600 ,, ...	2 0	4 0

When more than one package is consigned at the same time, each package shall be charged at the rate per package, with the exception that ordinary Goods rates and conditions shall apply when cheaper to consignments forwarded to stations open for the receipt of light goods.

For carriage at Commissioners Risk see General Condition No. 4.

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DELIVERY IN MELBOURNE BY CARTAGE CONTRACTOR OF SINGLE PACKAGES OF COUNTRY PRODUCE AND FRESH FRUIT AND TOMATOES AND OTHER SMALL CONSIGNMENTS OF GOODS.

(1) Single packages of Country Produce and Fresh Fruit and Tomatoes, fully addressed and not marked "To be called for," when consigned in packages not exceeding the weights specified below, will be delivered from the Melbourne Goods Depot by the Departmental cartage contractor to any address in the districts shown in clause (3) at the following rates :—

	District "A" (City)	District "B"	District "C"
	s. d.	s. d.	s. d.
Per package not exceeding 30 lb. (honey, 33 lb., and dried fruit, 36 lb.)	1 0	1 9	2 0
Per package, exceeding 30 lb., but not exceeding 60 lb. (dried fruit 67 lb., and honey 67 lb.)	1 6	2 3	2 6
Per jar of wine	2 6	2 9	3 6

(2) Consignments of goods other than those mentioned in clause (1), fully addressed, and marked for delivery per carrier, will be delivered from the Melbourne Goods Depot by the Departmental cartage contractor at the following scale of charges :—

		District "A" (City)	District "B"	District "C"
	lb.	s. d.	s. d.	s. d.
For each consignment whether consisting of one or more packages not exceeding—	28	1 9	2 0	2 6
	56	1 9	2 6	3 0
	cwt. 1	2 6	3 6	4 6
	2	4 6	6 0	6 6
	3	6 3	9 0	10 6
	5	10 0	15 6	16 6
Each additional 56 lbs.		1 6	2 0	2 3
Furniture or Pianos		Double Rates	Double Rates	Double Rates
Potatoes, Vegetables and Grain per Bag		3 0	3 6	3 6

Railway weights to be taken.

The freight and delivery charges which must be prepaid shall be shown separately on waybills.

(3) **The Districts concerned are :—**

DISTRICT "A"	DISTRICT "B" (Inner Suburbs.)	DISTRICT "C" (Outer Suburbs.)
Within the City Area, bounded by Flinders, Spring, Victoria, Peel, William, Latrobe and Spencer Streets.	Abbotsford, Albert Park, Alphington, Armadale, Auburn, Ascot Vale, Balaclava, Balwyn to Belmore-rd., Brunswick, Burnley, Camberwell, Camberwell East, Canterbury, Carlton, North Carlton, Caulfield, Clifton Hill, Collingwood, Coburg to Bell-street, Croxton, Deepdene to Belmore-rd., East St. Kilda, East Kew, Elsternwick, Elwood, Essendon, Essendon North, Fairfield, Fitzroy, Fitzroy Nth., Flemington, Footscray, Footscray West to Kingsville-rd., Gardenvale, Gardiner, Glenferrie, Glen Iris, Hawksburn, Hawthorn, Ivanhoe, Jolimont, Kensington, Kew, Kooyong to Glenferrie-rd., Malvern, Maribyrnong, Melbourne North, South, East, West and Port, Middle Park, Moreland, Moonee Ponds, Newmarket, Newport, Northcote, Parkville, Prahran, Preston to Bell-street, Richmond, St. Kilda, South Yarra, Spotswood, Thornbury, Toorak, Windsor, Yarraville	Balwyn (beyond Belmore-rd.), Beaumaris, Bentleigh, Black Rock, Box Hill, Brighton, Burwood, Carnegie Cheltenham, Coburg (beyond Bell Street), Deepdene (beyond Belmore-rd.), East Burwood, East Oakleigh, Footscray West (beyond Kingsville-rd.), Glenhuntly, Hampton, Heidelberg, Highett, Kooyong (beyond Glenferrie-rd.), Malvern East, McKinnon, Mentone, Merlynston, Mont Albert, Moorabbin, Mordialloc, Murrumbeena, Oakleigh, Ormond, Preston (beyond Bell-street), Reservoir, Rosanna, Sandringham, Surrey Hills, Sunshine, Williamstown

Commodity Special Rates.

The commodity and concession rates specified hereunder are in all cases subject to the following conditions :—

1. That such commodity or concession rates shall apply only where the person firm or body corporate or unincorporate obtaining the benefit of such rates uses the railways solely for the transportation of goods where in the opinion of the Chief Commercial Manager railway facilities are available and might reasonably be used.

2. That if at any time the Chief Commercial Manager is satisfied that any goods are being or have been forwarded by road or any method of transportation other than by rail by for or on behalf of any person firm or body corporate or unincorporate obtaining the benefit of the commodity or concession rates when railway facilities were available and might reasonably have been used for such transportation the Chief Commercial Manager may direct that the rates specified shall not apply to any goods of the nature described consigned to or on behalf of or by or on behalf of any such person firm or body and in the event of the Chief Commercial Manager so directing the ordinary full tariff rates and conditions shall thenceforth be charged and paid in respect of such goods.

3. Rebates.—Unless otherwise provided all claims for rebates must be lodged with the Chief Commercial Manager within six (6) months from the date the consignments are forwarded.

ADVERTISING MATERIALS (INTERSYSTEM).

Tourist Literature, Time Tables and Railway Pamphlets consigned to Australian Railway administrations and Government Tourist Bureaux shall be carried "Free" on each railway system.

Commercial advertising matter (including show cases and enamel plates) for display on railway premises and consigned to the Railway Advertising Division shall be charged at ordinary tariff rates unless cheaper to charge one-quarter parcels rates over all systems except New South Wales and Commonwealth Railways, on which the charge shall be half rate. Freight charges on consignments to Queensland and Western Australia must be prepaid.

ARSENIC (see Volume 11 for conditions of carriage).

Arsenic in kegs and drums from the undermentioned stations shall be charged as follows:—

From—	To—	Classification to be Charged	
		Truck Loads, (a)	Smaller Quantities Minimum 1 ton
Spotswood and Yarraville ...	Port Melbourne or Williamstown Pier	Class B.	Class C.
Any Country Station ...	Melbourne, Port Melbourne, Yarraville or Williamstown Pier ...	B. less 17 per cent.	C. less 19 per cent.

(a) Subject to Minimum of 10 tons 9 cwt. per 11-ton truck, 15 tons 4 cwt. per 16, 22 or 27-ton truck.

COMMODITY SPECIAL RATES—*continued.*

ARSENIC AND ARSENICAL COMPOUNDS FOR ERADICATION OF NOXIOUS WEEDS — (See Volume II for conditions of carriage.)

Arsenic and Arsenical Compounds consigned to any Victorian Municipal Council for the eradication of noxious weeds shall be carried free of charge, subject to the following conditions :—

Application for the concession to be made by the Municipal Council to the Chief Commercial Manager at least fourteen (14) days prior to the date when the consignment is to be forwarded, specifying the name of the consignee, and the station from and to which the Arsenic or Arsenical Compounds will be consigned, together with a statutory declaration from the Municipal Council that the consignments will be used solely for the eradication of noxious weeds.

This concession will only be granted on the authority of the Chief Commercail Manager, and in such cases the freight at " Manure " rate and conditions is to be waybilled " paid," and debited to the Hon. the Treasurer, the authority from the Chief Commercial Manager being attached to the account as a voucher.

BENZOL — (See Volume II for conditions of carriage.)

Benzol declared on the consignment note to be used for prevention of blue mould in tobacco growing districts shall be charged the ordinary tariff rates in the first instance, but on production by the consignee of a Statutory Declaration to the effect that the identical consignments have been so used, the charges may be reduced to the rate for Orchard Pests Specifics (Class " A " and conditions) by the receiving station.

The way-billing station shall endorse the waybills " declared to be used for the prevention of blue mould " when benzol is so consigned.

BOOKS AND PERIODICALS manufactured at Warburton, and railed on the Up journey from Warburton to Melbourne shall be charged Class 2 rate less 15 per cent.

BRICKS, ordinary, in truck loads of 10 tons 9 cwt. per 11-ton truck, or 15 tons 4 cwt. per 16, 22, or 27-ton truck, forwarded as shown hereunder, shall be charged at the following rates :—

Station from—	Station to—	Rate Per ton s. d.
Ballarat ... ... Ballarat East ... ... Selkirk's Siding	Melbourne	29 8
Ballarat ... ... Ballarat East ... ... Selkirk's Siding	Geelong ... ... North Geelong ... South Geelong ...	20 2
Melbourne ... ... Jewell ... ... Northcote ... ... Thornbury ... ... Bell	Geelong ... ... North Geelong ... South Geelong ...	17 10

District Rate conditions shall apply in respect of any consignments forwarded to suburban stations within 10 miles of Spencer Street or Flinders Street.

BUTTER AND CHEESE.

The mileage rates for the carriage of butter or cheese classified under Class BB or Class B plus one eighth, or any maximum rate applicable to these rates shall apply to the carriage of these commodities in refrigerator "T" trucks only where any person firm or body corporate or unincorporate who obtains or seeks to obtain the benefit of any such rates for such transportation in refrigerator "T" trucks has used the railways and no other means whatsoever for the transportation of goods in cases where railway facilities were available for such transportation. If at any time the Chief Commercial Manager or such Officer as may be acting in the office of Chief Commercial

COMMODITY SPECIAL RATES—*continued.*

Manager is satisfied upon reasonable inquiry that any goods have been transported by road or by any method of transportation other than by rail by for or on behalf of any such person firm or body corporate or unincorporate when railway facilities were available for such transportation the Chief Commercial Manager or such officer acting as aforesaid may direct that from and after the date of such direction the rates to be charged and paid for the carriage of butter or cheese in refrigerator "T" trucks consigned to or by or on behalf of any such person firm or body corporate or unincorporate shall be the mileage rates for the carriage of butter or cheese classified under Class BB or Class B plus one eighth, with the addition of 25 per cent. and subject to a minimum charge for 10 tons per truck in addition to the cost of icing the truck for the journey and freight at tariff rates on the quantity of ice so used.

CELERY forwarded from Mile End to Melbourne or Sydney in non-refrigerated trucks shall be charged the following rates for carriage at owner's risk :—

	Per ton £	s.	d.
(i) To Melbourne—			
Victoria	2	0	4
South Australia	2	19	8
Total ...	5	0	0
(ii) To Sydney via Albury—			
South Australia	2	2	1
Victoria	4	4	5
New South Wales	4	18	0
Counting	0	1	6
Transhipping	0	5	0
Uniform rating charge at Darling Harbour	0	0	6
Total ...	11	11	6

The above rates shall be subject to the following minimum tonnages in South Australia and Victoria :—

9 tons per Victorian 11 ton "U" truck ;
14 tons per Victorian 15 ton "U" truck ;
10 tons per other 4-wheeled truck ;
20 tons per bogie truck.

CEMENT.

(*a*) When not less than 1,000 tons of cement per annum are forwarded in the prescribed truck loads from one sender at one station to one consignee at one destination station, the tariff rate for the journey shall be subject to a maximum of 82s. 2d. per ton, which shall be granted by rebate. Application for rebate must be lodged with the Chief Commercial Manager within six months of the despatch of the 1,000 tons.

(*b*) Cement forwarded in truck loads from Fyansford or Melbourne to Irrigation Trusts in the Mildura district for cementing and concreting channels shall be subject to a maximum rate of 106s. 1d. per ton, provided that a declaration is made by the Secretary of the Trust on the original waybill at the inwards station, that the cement is to be used for the purpose mentioned.

CIRCUSES.

Circuses which can be conveyed by ordinary goods trains when times are suitable will be provided with special trains as may be approved, in consequence of no suitable goods trains running, at the following rates :—

£1 19s. 0d. per mile, subject to a minimum charge of £40 11s. 10d., and in addition a charge of 10s. 0d. per mile for any mileage incurred in running light engines in order to provide for such special trains, and for the hire of carriage stock required for the sole use of the circus £4 1s. 1d. per car per day, the Department reserving the right to use such special trains as it may require.

COMMODITY SPECIAL RATES—*continued.*

CONCESSION RATES ON CONSIGNMENTS FROM OR TO VICTORIAN COUNTRY INDUSTRIES.

(Applicable to such industries situated outside a radius of 20 miles from Melbourne (Flinders Street or Spencer Street)).

1. RAW MATERIAL TO BE USED IN THE MANUFACTURE OF PRODUCTS OF VICTORIAN COUNTRY INDUSTRIES.

(*a*) The following Raw Materials, consigned as indicated hereunder to the Victorian Country Industries specified shall, except where otherwise provided, be charged at tariff Rates, less the following percentages (see also clauses (*b*) and (*c*) and page 163, *clause (e) and note (a) on page 145* :—

Class	Percentage
M	33
A	28½
B	28½
C	31½
1	26½
2	26½

Country Industry consigned to	Commodity	—
Agricultural Implement Works (see also clause (*c*))	Iron and Steel, viz.— Bar, rod, plate, angle, hoop, pig, polished, sheet and galvanized Mouldboards Piping, black Discs and ploughshares Bolts, nuts and rivets Shafting, bright steel *Rims, wooden, for* harvester wheels	On Down journey
Aerated Water Manufacturers	Carbon Dioxide Gas	On Down journey
Bacon Factories	Salt (refined)	On Down journey
Bark Mills	Bark, bundles or in sheets	
Biscuit and Confectionery Factories (see also clause (*b*))	Confectionery starch Nulomoline	On Down journey
Blind Manufacturers	Duck	On Down journey
Boot Manufacturers	Leather	On Down journey
Canned Fruit Manufacturers (see also clause (*b*))	Caustic Soda	On Down journey
Cardboard Box Manufacturers	Strawboard Paperboard	On Down journey
Cement Pipe Manufacturers	Cement Reinforcements, fabricated iron or steel Iron and steel rod Iron, wire	On Down journey
Cement Tank Manufacturers	Cement Reinforcements, fabricated iron or steel Iron and steel rod Iron, wire	On Down journey

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Commodity Special Rates—continued.

Concessions to Victorian Country Industries—Raw Materials, etc.—continued.

Country Industry Consigned to.	Commodity.	—
Cement Tile Manufacturers ...	Cement	On Down journey
Cement Wash-trough Manufacturers	Cement	On Down journey
Coachbuilders ... ... (see also clause (b)).	Axles arms and boxes ... Felloes ... ... Iron and steel—bar rod, plate, angle, hoop, sheet, and galvanized Naves, rough, turned, and mortised Poles ... ... Rims buggy, finished and unfinished Shafts ... ... Spokes ... ... " rough ... ... Springs ... ... Tyres, iron ... ... Wheels, cart, dray, waggon, rough and unfinished	On Down journey
Concrete Building Block Manufacturers	Cement ... ... Iron and Steel Rod ... Reinforcements, fabricated iron or steel	On Down journey
Concrete Manufacturers (Terrazzo)	Cement ... ... Reinforcements, fabricated iron or steel Oxide, red ... ... Plaster ... ... Hemp and Sisal	On Down journey
Country Factories	Tin plate ... ... Silicate of soda	On Down journey
Disinfectant Manufacturers ...	Tar oil ... ... Resin ... ... Caustic soda	On Down journey
Distilleries	Dried Fruit	When consigned direct to distillers and accompanied by a declaration that it is to be used for distilling purposes
	" "	Returned to the original forwarding station for distillation purposes on production of declaration
	Wine off, for distillation	
Fibrous Plaster	Cocoanut Fibre ... ... Coir Fibre ... ... Hemp and Sisal ... ... Plaster of Paris ... ... Tow	On Down journey

Commodity Special Rates—continued.

Concessions to Victorian Country Industries—Raw Materials, etc.—continued.

Country Industry Consigned to.	Commodity.	—
Foundries ... ... (see also clause (*c*))	Boiler tubes ... ... Castings and forgings, rough and finished Piping, black and wrought iron Channels and rolled joists ... Iron and Steel— Bar, rod, plate, angle, pig, hoop, scrap, polished, sheet and galvanized Bolts, nuts and rivets ... Blooms, ingots Metal, scrap including castings and ingot Gunmetal scrap	On Down journey
Fruit Dip Buckets or Tins, Frost Pots	Iron, sheet, black, galvanized and hoop, wire, washers and steel rods	On Down journey
Grocers' Sundries Manufacturers ... (see also clause (*b*))	Acetic Acid ... ... Resin ... ... Caustic soda ... ... Soda ash ... ... Tar, stock	On Down journey
Insect Exterminator Manufacturers	Orris root, powdered ... Pepper	On Down journey
Knitting Mills and Cotton Factories	Formic Acid ... ... Ammonia Solution ... Cotton, raw ... ... Cotton and worsted yarn ... Fibre Silk ... ... Hydro-chloric Acid ... Hydro-peroxide ... ... Laces and braids ... ... Dyes ... ... Soda Ash ... ... Soap, Common ... ... Tapes ... ... Wool Tops ... ... Yarn, Silk, Artificial ... ,, Woollen ... ... Sodium Hypochlorite ... Sulphur ... ... Solvent T. ... ... Tetracarnit	On Down journey
Mattress Manufacturers ... ... (see also clause (*b*))	Bolts, iron ... ... Wire, woven, for wire mattresses Wire in coils, packed in casks, for mattress making on declaration	On Down journey
Milk Condenseries	Carbon Dioxide Gas ...	On Down journey

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Commodity Special Rates—continued.

Concessions to Victorian Country Industries—Raw Materials, etc.—continued.

Country Industry Consigned to.	Commodity.	—
Motor Bodies, Motor Vehicles, and Chassis—Manufacturers and Assemblers	Tin plates ... ... Accumulators and Storage Batteries Bolts and Washers ... Bumper Bars ... ... Castings ... ... Felt ... ... Iron and Steel Wire ... Iron, sheet ... ... Lacquers ... ... Paints ... ... Rubber manufactures ... Tacks, nails, screws, and rivets Tow ... ... Tyres and Tubes, Rubber ... Wooden parts ... ... Motor Chassis parts ... Motor Wheels without tyres	On Down journey
Paper Mills	Glass paper ... ... Wood pulp ... ... Waste paper ... ... Silicate of Soda ... ... Hoop Iron ... ... Acid, sulphuric ... ... Alumina, sulphate of (water purifier)	On Down journey
Polish Manufacturers	Methylated spirits ... Resin	On Down journey
Pottery Works	Lead, red ... ... Oxide, red ... ... Canes ... ... Plaster of Paris ... ... Whiting	On Down journey
Rope and Twine Works ...	Yarn—Hemp, flax, jute, coir Hemp—Fibre flax... ... Cotton yarn	On Down journey
Sauce and Pickle Manufacturers ...	Acetic Acid	On Down journey
Sheep Dip Manufacturers ...	Acid, sulphuric and muriatic Arsenic, refined ... ... Sulphur	On Down journey
Soap Manufacturers	Resin ... ... Caustic soda ... ... Soda ash ... ... Coconut oil	On Down journey
	Tallow	On Up or Down journey
Tanneries	Bark extract " ground or chopped " bundles or in sheets	

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Commodity Special Rates—continued.

Concessions to Victorian Country Industries—Raw Materials, etc.—continued.

Country Industry Consigned to.	Commodity.	——
Tanneries—*continued.*	*Hides, in less than 2-ton lots Myrabolams ... ... Oil, mineral ... ... Pelts, pickled ... ... Tanning compounds ...	On Down journey
Textile Machinery Manufacturers	Lead, and bar iron ...	On Down journey
Underclothing Manufacturers ...	Braids ... ... Buttons ... ... Cotton (sewing) ... ... Laces ... ... Tapes ... ... Biege ... ... Calico ... ... Woollen cloth	Rebate shall be granted to Class B and conditions less 28½ per cent. — On Down journey
Vineyards	Spirits for fortifying wine Wine or Grape Spirit ,, stone	
Vinegar Manufacturers	Wine off, for vinegar making Acetic Acid	
Water Heater Manufacturers ...	Copper, sheet ... ... Iron, galvanized ... ... ,, black sheet	On Down journey
Windmills Manufacturers ...	Iron, bar, rod, plate, angle, and galvanized ... ... Galvanized iron piping ...	On Down journey
Woolwashing Establishments (including Albury)	Sheepskins Wool Soda ash	On Down journey
Woollen Mills	Wool tops ... ... Acid, acetic ... ... ,, formic ... ... ,, hydrochloric ... ,, muriatic ... ... ,, sulphuric ... ... Ammonia Solution ... Caustic Soda ... ... Cotton, raw ... ... Dyes ... ... Hydro peroxide ... ... Oil, Neatsfoot, Batching, and Textile ... ... Salts, Glauber, crude or refined ... ... Soap, common ... ... ,, Extract of ... ... Soda ash ... ... Sodium Hypochlorite ... Solvent T ... ... Tetracarnit ... ... Sulphur ... ... Woollen Noils ... ... Washing fluid ... ... Yarn, worsted or woollen...	On Down journey

* *Hides, minimum 2 tons, shall be charged Class AP, less 20 per cent.*

Commodity Special Rates—continued.

Concessions to Victorian Country Industries—Raw Materials, etc.—continued.

(*b*) The under-mentioned Raw Materials on Down journey consigned to the Victorian Country Industries shown hereunder, shall be charged the ordinary Tariff rates in the first instance, and a REBATE to the extent of the percentages shown below (see page 163, clause (*e*)) shall, except where otherwise specified, be granted on production of a statutory declaration that such raw materials have been converted into manufactured products, which have been subsequently conveyed by rail from the place of manufacture (except as otherwise specified hereunder) :—

Class.	Rebate Percentage.	Class.	Rebate Percentage.
M	33	C	31⅓
A	28½	1	26⅔
B	28½	2	26⅔

Country Industry Consigned to.	Commodity.	——
Biscuit and Confectionery Factories	Glucose Sugar	
Canned Fruit Manufacturers ...	Sugar	See †
Coachbuilders	Lamps, Leather, Motor Tyres, Paints and colours	
Dried Fruit Mixtures	Dried Fruit	Rebate shall be granted to Class A and conditions, less 17 per cent. on consignments of dried fruit forwarded from Melbourne and subsequently returned thereto as Dried Fruit Mixture
Furniture Manufacturers... ...	Duck, Flock, Kapok, Frames (chair, couch and settee, set up and partly polished) Springs, furniture... ... Hessian ... ... Fibre, coconut ... ... Leather ... ... Leather cloth ... ... Imported oak ... ... Wadding...	See *
Grain Storage, and Sheep Feeding Bin Manufacturers	Iron, corrugated ... ... ,, sheet and angle ... ,, piping	See *
Grocers' Sundries Manufacturers ...	Sugar Tartaric acid Gelatine Tar oil Turpentine	
Jam Manufacturers	Sugar	See †

* *The rebate shall be granted irrespective of whether the manufactured products are subsequently railed outwards or not.*

† *The freight may be computed and waybilled in the first instance at tariff rates, less the percentage above-mentioned, as the case may be, subject to the authority of the Chief Commercial Manager being obtained by the forwarding station.*

Commodity Special Rates—continued.

Concessions to Victorian Country Industries—Raw Materials, etc.—continued.

Country Industry Consigned to.	Commodity.	—
Joinery Works	Glass used in manufacture of windows	See *
Mattress Manufacturers	Flock, Kapok, Ticking ...	See †
Metal Ceiling and Wall Manufacturers	Sheet iron or steel Galvanized sheet iron	
Milk Condenseries	Sugar	On production of a Statutory Declaration that such sugar has been converted into condensed milk or dried milk which has been subsequently conveyed by rail from the place of manufacture to the seaboard and consigned direct for export beyond the Commonwealth a rebate shall be granted to 28½ per cent. less than the classification rate, provided that the net rate on sugar shall not be thereby reduced below 17s. 4d. per ton
Foundries	Cylinders and Flywheels	
Manufacturers of Iron Chimneys, Furnace Frames, and Stove and Tobacco Kiln Pipes	Galvanized and black sheet iron	See *
Manufacturers of Motor Tractors ...	Motor engines, transmissions, wheels, and gears	Rebate shall be granted to Class "C" and conditions, less 31½ per cent.
Manufacturers of Tanks, Sanitary Pans, Spouting, Ridging, Down Piping, Baths, Bath Screens, Milk and Cream Cans, Iron Wash or Horse Troughs, Iron Chimneys, Rubbish and Flour Bins and Trunks	Iron or Steel ... ... Corrugated (galvanized) Sheet (galvanized or tinned)	See *
Sewing Machine Manufacturers ...	Rough castings, bar iron and steel, sheet iron, bolt and nuts	

NOTE.—(*a*) *When materials for a country industry are chargeable under Class 1 less 26½ per cent., loading and unloading charges are not to be imposed.*

* *The rebate shall be granted irrespective of whether the manufactured products are subsequently railed outwards or not.*

† *The freight may be computed and waybilled in the first instance at tariff rates, less the percentage above-mentioned, as the case may be, subject to the authority of the Chief Commercial Manager being obtained by the forwarding station.*

COMMODITY SPECIAL RATES—*continued.*

Concessions to Victorian Country Industries—Raw Materials, etc.—*continued.*

(c) **Raw Materials to be used in the manufacture of Agricultural Implements and Machines and Products of Victorian Country Foundries.**

Raw materials, consisting of steel, iron (bar, rod, plate, angle, and galvanized), mould-boards, discs, bolts and rivets used in the manufacture of agricultural implements and machines and products of country foundries, and sheet-iron or steel and galvanized sheet-iron used in the manufacture of metal ceilings and walls shall be charged the ordinary tariff rates in the first instance, and a rebate subsequently granted to Class "M" rate, less 33 per cent., and conditions, on production of a statutory declaration that the raw materials were subsequently converted into implements, machinery, products of country foundries, metal ceilings or walls, conveyed by rail from the place of manufacture direct for export, or to places outside the State, either by water or rail (see page 163, clause (e)).

2. PRODUCTS OF VICTORIAN COUNTRY INDUSTRIES.

The following products of Victorian Country Industries, so described on the Consignment Note shall, with the exception of those for which provision is made in clauses 4 and 5 hereof and on page 163 clause (e), and where otherwise specified, be charged on the " Up " journey from the place of manufacture at Tariff Rates, less the percentages shown hereunder—(see note (*a*) on page 147).

Class	Percentage
M	21
A	17
B	17
C	19
1	15
2	15
A.P.	4¾
S.A.P.	4¾

Aerated Waters.
Ale, Beer, and Stout, bottled.
Ale, Beer, and Stout, bulk.
Bacon.
Baking Powder.
Baths and Basins, enamel, cast and galvanized iron.
Baths and Basins, Terrazzo Cement.
Bath Heaters.
Bedding and Bedsteads.
Beeswax.
Biscuits.
Blankets, Flannels and Woollens, including Woollen Rugs, and Woollen Cloths
Boots and Shoes
Butchers Small Goods, viz., Black and White Puddings, Brawn, Frankfurts, Germans, Sausages, Saveloys.
Brandy.
Chalk.
Chutney.
*Cider.
Citrus Health Saline Powder.
Clothes Pegs.
Clothing.
Coffee Essence.
Coffins.
Concrete Building Blocks.
Confectionery.
Cordials.
Cotton Goods.
Cotton Yarn.
Crucibles, clay.
Culverts, box, cement.
Curry Powder.
Disinfectants.
*Earthenware.
Egg Preservative.
Envelopes, straw.
Felloes and Spokes.
Fibre flax.
Fibrous Plaster Sheets.
Finings, Beer.
Fire Bricks, Fire Clay and Fire Lumps.
Flock.
Fruit, dried*, canned, bottled, preserved.
Fruit dried mixture.
Fruit Juices.
Fuller's Earth.
Furniture, new.
Gates, iron.
Hams.
Handles, seasoned.
Honey.
Jams and Jellies.
Jelly Crystalls.
Joinery.
Lard.
†Leather.
Legs, Chair and Table, sawn or turned in the rough.

COMMODITY SPECIAL RATES—*continued.*

Concessions to Victorian Country Industries—*continued.*

2. Products of Victorian Country Industries—*continued.*

Lemon Peel, candied.
Linerite.
Liquor, black, by-product of wood pulping.
Macaroni.
Mattresses.
Meat, extract of and preserved.
Mineral Waters.
Motor-cycle side-cars.
Nescafe.
Nescory.
Noils
Ochre.
Oil, eucalyptus.
Oil, olive.
Oxide, Red.
Pedestals, Terrazzo Cement.
*Perry.
Pickles.
Pipes, Concrete.
,, wrought iron or steel
,, earthenware.
,, vent.
Plaster of Paris.
Polish, wax, for boots, furniture and linoleum
Polish, marble and stove.
Potato, flour.
*Potteryware, including sanitary pedestal pans.
Pulp, tomato and fruit for export.
Rabbits, canned
Rayon Goods.
Retorts, clay.
Rope, hempen.
Salt, refined.
*Sanitaryware.
Sauces.
Sinks, Terrazzo Cement.
Slates, roofing.
Slippers.
Sheep Dip.
Spirits.
Spirit, Wine or Grape.
Soap.
Soup Tablets and Powders.
Starch.
,, potato.
Strawboard.
Strawpaper.
Tanks, Cement.
Temperance Drinks.
Tiles.
Tow.
Troughs, Cement, Washing, Sheep, Pig, Cattle or Horse.
Troughs, Terrazzo Cement, Washing.
Twine and Lines.
Twine, reaper and binder and seaming.
Vegetables, preserved, dehydrated or dried.
Vinegar.
Water Heaters.
Whiting.
*Wine.
Wine lees.
Woollen and Knitting Mills, products of.
Wool Tops.
Yarn, Woollen, Worsted.

Note.—(*a*) When products of a Victorian Country Industry are chargeable under Class 1 less 15%, loading and unloading charge are not to be imposed.

* The reduced rates shall not apply in respect of earthenware, potteryware, and sanitaryware, for which a special rate is provided on page 158, nor in respect of wine, cider, perry and dried fruit consigned direct for export.

† Leather charged under "A.P." or "A" less 25 per cent. shall be allowed a reduction of 4¾ per cent.

3. STONE, PRODUCT OF VICTORIAN COUNTRY INDUSTRIES, FOR EXPORT.

Granite and marble, wholly polished or carved, loose or in cases (specified in the classification under Classes 1 and 2), which are the product of Victorian Country Industries consigned by rail direct for export from the place of manufacture to Melbourne, Geelong, or Portland, shall be charged the full tariff rates in the first instance and, except as provided on page 163, clause (e) a rebate to half rates less 15 per cent. shall be granted on satisfactory proof of export being furnished.

4. PRODUCTS OF VICTORIAN COUNTRY FOUNDRIES.

Products (new) of Victorian country foundries, including Metal Ceilings and Walls, Stoves and Stove Pipes, Scaffolding Clips, Vent Pipes, Windmills, Agricultrual Implements and Agricultural Machines forwarded on the "Up" journey from the place of manufacture, shall be charged one rate lower than the tariff rate, less the percentage shown in clause 2 for the respective classes, provided that such reduced rate shall not be applicable in respect of consignments which are subject to rebate, as prescribed in clause 5 hereof and as provided on page 163, clause (e).

When consignments ordinarily scheduled under Class 1, which are entitled to the reduction of one class, weigh less than 1 ton, they may be charged at the actual weight at Class "C," less 19 per cent., subject to Tariff for "Smalls" instead of being subject to the weight conditions of Class "C."

The provisions of this clause 4 shall not apply to Wrought Iron or Steel Pipes.

COMMODITY SPECIAL RATES—*continued.*

Concessions to Victorian Country Industries—*continued.*

5. MACHINERY, MACHINES, VEHICLES, ETC., NEW, MANUFACTURED IN THE COMMONWEALTH.

The following new articles, viz., agricultural implements and machines and parts, steel balls for grinding purposes, boilers, castings (rough), crab winches, fly wheels, forgings (rough), gates (iron), machinery and parts including small petrol or oil engines, metal ceilings and walls, motor bodies and parts, motor chassis, motor tractors and parts, motor vehicles, picks and pickeyes, saw benches, sole plates, wrought iron or steel pipes, stoves and copper frames, plate iron petrol or oil tanks, vent pipes, windmills and vehicles, manufactured in the Commonwealth, and sent by rail to Melbourne, Geelong, or Portland from the place of manufacture for immediate export beyond the State, or sent from country stations to any station not less than 50 miles from Melbourne, shall, except as provided on page 163, clause (e), subject to the conditions specified hereunder, be charged half the tariff rates and the following percentage reductions also allowed in respect of consignments which are the products of Victorian country industries and which are classified under the classes shown hereunder, viz.:—

Class	Percentage
M	21
A	17
B	17
C	19
1	15
2	15

(*a*) In respect of machinery, &c., intended for export, the full tariff rates shall be charged in the first instance, and a rebate to half rates, or half rates less the percentages shown above, as the case may be, granted on satisfactory proof of immediate export beyond the State being furnished.

(*b*) In respect of machinery, &c., sent from country stations to stations distant 50 miles or more from Melbourne, the reduced rates from the forwarding station to the destination station shall be computed via Melbourne, and shall not be less than the full tariff rates from Melbourne to the destination station.

(*c*) In respect of machinery, &c., sent from country stations to stations situated outside a radius of 20, but within 50 miles of Melbourne, the rates from the forwarding station to the destination station shall not be more than the reduced rates from the forwarding station to a station 50 miles from Melbourne and not less than the full tariff rates from Melbourne to a station 50 miles therefrom.

(*d*) In respect of motor vehicles and other road vehicles sent from country stations to stations situated within 20 miles of Melbourne, the rate shall not be more than the rate to a station 50 miles beyond Melbourne.

(*e*) The Tariff for "Smalls," where applicable, shall be applied on the actual tonnage rate charged.

(*f*) In computing the charges on any of the abovementioned machinery etc. which may be consigned for the full journey by rail from a country industry in Victoria to Adelaide, Sydney, Brisbane, Kalgoorlie or Perth, when such charges are based on the sum of the local rate from the country station to Melbourne, plus the Special Intersystem Through Goods Rate, half the full tariff rate, less the percentages specified from the country station to the radius of 20 miles from Melbourne, (Exception—For Kalgoorlie consignments read "to Melbourne" instead of the underlined words), shall be charged plus the full Special Intersystem Through Rate thence to destination.

CONSIGNMENTS FOR EXPORT.

A reduced rate is provided in the Classification of Goods for the following commodities when consigned direct for export, viz.:—Milk (condensed, concentrated or dried), Chutney, Fruit (canned, bottled and preserved), Fruit (fresh), Fruit juice or cordials, Glaxo, Jams and Jellies, Lactogen, Lemon Skins in brine, Oil (Eucalyptus), Pickles, Potteryware, Pulp (Fruit and Tomato), Sauces, Wine, Canned Vegetables.

(*a*) When these commodities are consigned to Melbourne for export they shall be charged the ordinary Classification Rates in the first instance and a rebate to the export rate provided shall be subsequently granted on production of a statutory declaration that the identical goods railed have been exported. Satisfactory proof of export must also be provided.

(*b*) When these commodities are declared on the consignment note to be for export at the Victoria Dock, by a specific ship, the name of which must be shown on the waybill, the Export Rate and conditions shall be charged in the first instance. Should the whole or part of any consignment not be shipped, the Goods Superintendent shall adjust the freight charges at the full ordinary rate, and report the matter to the Chief Commercial Manager.

COMMODITY SPECIAL RATES—*continued.*

CONSTRUCTION MATERIAL.

Rates and conditions for the carriage of materials—

(*a*) consigned to or by the Chief Engineer for Railway Construction for the construction of new lines ;

(*b*) used in the construction or alteration of private sidings, irrespective of whether the work is done by the Department or not ;

(*c*) used by other than the Department in the maintenance of private sidings ; shall be as *follows, viz.*:—

Rate per Ton per Mile	Capacity of Truck Tons	Subject to the following Minimums Tons
Truck Loads—3¼d.	11	9
	16, 22 or 27	14
	Q.R. Bogie	18
	50 ft. Q.	24
Smaller Lots—4¾d.	Actual weight, where cheaper than the truck load rate and conditions	

Minimum rate 8s. 8d. per ton, and minimum charge 23s. 6d., otherwise classification rate, subject to "Smalls" minimum. This traffic shall not be subject to haulage charges.

In computing the charges to be made for gravel, road-metal, &c., in full truck loads the freight shall be calculated on the basis of the rates and conditions charged for similar materials for Borough and Shire Councils, page 154, if cheaper than the Construction rates and conditions shown above.

In cases where construction material is loaded or unloaded by the Traffic staff, a charge of 4s. 0d. per ton shall be imposed for the service. Debits to be included on the waybill and cleared *by voucher account.*

(*d*) **St. Kilda, Brighton and Black Rock.**—All materials and supplies for the maintenance, or working of the St. Kilda–Brighton Electric Street Railway, and the Sandringham–Black Rock Electric Street Railway shall be charged under the above rates and conditions.

In the event of a special train being run with equipment from Newport to Elsternwick or St. Kilda on account of the Electric Street Railway, an additional charge of £8 1s. 6d. shall be made for the special service.

DOORS, HARDWOOD, *product of Victorian Country Joinery Works, railed from* the place of manufacture on the "Up" journey, shall be charged under Class "C" and conditions.

EXHIBITS FOR GOVERNMENT TOURIST BUREAUX shall be carried "free" for either Intersystem or local journeys.

Heavy or bulky consignments shall be carried by goods train at the discretion of the Systems concerned.

COMMODITY SPECIAL RATES—*continued.*

FLOUR AND MALT.

1. MILLING IN TRANSIT REBATES.

A.—Rebates on wheat railed to flour mills specified below and manufactured there, and railed thence to destinations shown below as flour, bran, pollard or sharps, will be allowed in accordance with the following provisions :—

WHEAT CONSIGNED BY RAIL			shall be charged the separate ordinary mileage rates for each journey, *i.e.*, as wheat and as flour, etc., in the first instance, and a rebate will be granted to the continuous mileage rate for the total distance from the wheat forwarding station to the flour, etc., destination station, plus a stopover charge of—
FROM	TO any flour mill at	and manufactured at such flour mill and carried thence by rail as flour, bran, pollard or sharps to—	Per ton.
1. Any Country station	Any Country station	(i) Melbourne and Suburban stations (ii) Portland, Geelong, Warrnambool or Port Fairy or to any station intermediate between Geelong and Port Fairy (iii) any station on the Queenscliff, Forrest, Crowes, Timboon, Alvie or Mortlake lines	s. d. 3 11
2. Any Country station	Any Country station with the exception of G'long (see Clause 5)	Any station on the Eastern or South Eastern line or any branch thereof outside a radius of 20 miles of Melbourne	3 11
3. Melbourne and Subn. stations	Any Country station	Any station	3 11
4. Any Country station	Any Country station	Any station other than those specified in Clauses 1 and 2	9 7
5. Any Country station	(i) Melbourne and Suburban stations (ii) Geelong	Any station on the Eastern or South-Eastern line, or any branch thereof outside a radius of 20 miles of Melbourne	9 7

The following conditions will apply to such rebates :—

(*a*) The quantity of wheat railed from the most distant despatching station to the flour mill and the quantity of flour, bran, pollard and sharps railed from the flour mill to the most distant destination station will first be taken into account, and then the quantity of wheat similarly railed from each succeeding despatching station and the quantity of flour, bran, pollard and sharps railed to each destination station in the order of greatest distance.

(*b*) The tonnage of the flour, bran, pollard and sharps on which rebate is granted shall not exceed the tonnage of the wheat railed—

(i) from any country stations to the flour mills mentioned in respect of the wheat/flour movements specified in Clauses 1, 2, 4 and 5.

(ii) to the flour mills mentioned from any station on the "Up" side thereof in respect of the wheat/flour, etc., movements specified in Clause 3.

COMMODITY SPECIAL RATES—*continued.*

Flour and Malt—*continued.*

(c) The 20 miles radius of Melbourne will include stations within 20 miles of Flinders Street or Spencer Street.

(d) The rebate provisions above prescribed shall apply in respect of wheat railed from Victorian stations to Albury, Wahgunya (for Corowa), and Tocumwal, and subsequently railed therefrom as flour, bran, pollard and sharps to Victorian stations.

B.—The provisions of Clause A will also apply in every respect to—

(i) barley and wheat railed to country Malthouses and manufactured there and railed thence in the shape of malt ;

(ii) wheat railed to flour mills or wheat crushing or grinding plants and subsequently railed therefrom in the form of wheatmeal or crushed or ground wheat provided that these commodities contain no ingredient except wheat ;

(iii) Rye railed to flour mills and subsequently railed therefrom in the form of rye flour.

C.—All claims for rebate must be lodged with the Chief Commercial Manager within 18 months of the railing of the wheat, rye or the barley inwards and within 6 months of the railing of the flour, bran, pollard, sharps, malt, wheatmeal, crushed or ground wheat outwards.

2. FLOUR FOR EXPORT REBATE.

In respect of flour, white or brown, and self-raising flour consigned by rail from Victorian country flour mills direct to the seaboard for immediate export beyond the State, a rebate shall be granted, subject to the production of satisfactory proof of export, of 15 per cent. below the rate applicable to the journey between the flour mill and the port of export.

This rebate shall also apply to flour, white or brown, and self-raising flour gristed at Albury, Corowa or Tocumwal, and forwarded to Victorian ports direct for export.

FORMALIN FOR PICKLING WHEAT shall be charged the ordinary rate in the first instance, and a reduction to Class "A" rate and conditions arranged by the receiving station on production by the consignee of a statutory declaration that the formalin has been used for pickling wheat.

FRUIT, DRIED, FOR EXPORT.

The rates for the carriage of Dried Fruit, Produce of the Commonwealth, on the "Up" journey consigned from the stations shown below, direct to the places at the seaboard shown hereunder, for immediate export beyond the Commonwealth, and provided it is forwarded on a continuous rail journey from the point of production to the point of shipment, shall be as follow:—

Station from—	To Melbourne See note (b)		To Victoria Dock (incldng. haulage)		To Pt. Melbourne Pier		To Williamstown Pier	
	Truck loads See note (a) Per ton	Minimum 3 tons Per ton	Truck loads See note (a) Per ton	Minimum 3 tons Per ton	Truck loads See note (a) Per ton	Minimum 3 tons Per ton	Truck loads See note (a) Per ton	Minimum 3 tons Per ton
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Redcliffs	65 7	74 4	67 8	76 5	66 10	75 9	66 10	75 9
Irymple	66 10	75 9	68 11	77 10	67 2	76 5	67 2	76 2
Mildura	67 2	76 5	69 3	78 6	67 8	77 0	67 8	77 0
Merbein	68 3	77 8	70 4	79 9	69 2	78 6	69 2	78 6
Yelta	69 5	78 6	71 6	80 7	69 5	78 9	69 5	78 9
Swan Hill	51 3	59 3	53 4	61 4	51 5	59 9	51 5	59 9
Woorinen	52 2	60 4	54 3	62 5	52 9	60 9	52 9	60 9
Nyah West ...	53 5	61 11	55 6	64 0	53 7	62 1	53 7	62 1
Piangil	54 3	62 9	56 4	64 10	54 11	63 4	54 7	63 4
Robinvale	60 5	69 5	62 6	71 6	60 9	70 3	60 5	69 5
Mooroopna ...	35 6	41 1	37 7	43 2	35 11	41 4	36 8	42 7
Shepparton ...	35 11	41 4	38 0	43 5	36 8	42 7	36 10	42 8
Kyabram	37 3	43 2	39 4	45 3	38 0	44 1	38 9	44 10
Tresco	49 10	57 9	51 11	59 10	50 5	58 1	49 11	57 9

(a) Minimums—10 tons 9 cwt. per 11-ton truck, or 15 tons 4 cwt. per 16, 22 or 27-ton truck.

(b) When Dried Fruit is consigned to Melbourne for export, it shall be charged under the ordinary classification in the first instance, and a rebate to the export rate shall be subsequently granted on production of satisfactory proof of export and a statutory declaration that the identical dried fruit railed has been exported beyond the Commonwealth.

COMMODITY SPECIAL RATES—*continued.*

FRUIT, DRIED, FOR DISTILLATION, Produce of Commonwealth, forwarded to distilleries for distillation, and so declared on the consignment note, shall be charged under Class "A" less 10 per cent. and conditions in the first instance, and on production of a certificate from the Customs officer that the identical dried fruit has been distilled, the charges may be reduced to Class "M," less 10 per cent.,* subject to minimums of 10 tons 9 cwt. per 11-ton truck, and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

* For concessions to Victorian Country Industries see page 140.

FRUIT, FRESH, FOR EXPORT, forwarded from country stations to Cool Stores at Melbourne for storage or treatment and subsequently exported, shall be charged under Class "AA" "Smalls" Minimum, in the first instance, and waybilled accordingly ; but on production of a statutory declaration that the identical fruit has been exported beyond the Commonwealth, supported by proof of shipment, a rebate may be arranged by the Goods Superintendent to the rate and conditions provided for fruit consigned direct for export.

FRUIT JUICE AND FRUIT PULP forwarded from Mildura to Melbourne shall be subject to a maximum rate of 87s. 3d. per ton, subject to the Tariff for "Smalls."

FRUIT AND VEGETABLES CANNED, JAMS, JELLIES, SAUCES AND PICKLES, FRUIT JUICE OR FRUIT JUICE CORDIALS (manufactured from fruit grown in the Commonwealth), products of Victorian Industries exported by rail from the place of manufacture direct to destinations beyond the State of Victoria shall be charged the ordinary classification rates in the first instance, but on production of a statutory declaration that the consignments have been so railed and disposed of for use outside of the State of Victoria, a rebate will be made to the following classification, viz.:—

In truck loads—minimum 10 tons 9 cwt. per 11 ton truck, or 15 tons 4 cwt. per 16, 22 or 27 ton truck ... ... Class A plus 25%

In smaller quantities, subject to the Tariff for "Smalls" ... ... Class B plus 10%

FYANSFORD—

CEMENT, COAL AND OTHER TRAFFIC a/c. Australian Portland Cement Pty. Ltd., to and from Fyansford, which is 3¼ miles from North Geelong and open for traffic inwards and outwards in truck loads (minimum 3 tons) and for empty returns inwards a/c. the aforementioned Company, shall be charged as follows :—

Commodity		Rate to be Charged
		Per ton s. d.
Cement ...	Fyansford to Melbourne	20 0 (*a*) (*b*)
" ...	Fyansford to Other Stations	Tariff rates from North Geelong plus 9d. per ton
Other Traffic ...	Forwarded to or from Fyansford	Tariff rates to or from North Geelong plus 9d. per ton (*b*)
Coal	Between Geelong Station or Pier or Corio Quay and Fyansford	2 0 (*a*)
Other Traffic ...	Between Geelong Station or Pier or Corio Quay and Fyansford	2 0

(*a*) In truckloads subject to a minimum of 10 ton 9 cwt. per 11 ton truck and 15 tons 4 cwt. per 16, 22 or 27 ton truck.

(*b*) Clause 3 of the Instructions governing the application of the District rate shall be applicable to consignments forwarded between Fyansford and suburban stations within a radius of 20 miles of Melbourne (either Flinders Street or Spencer Street).

GRAIN CONSIGNED FOR EXPORT AT PORTS OUTSIDE OF MELBOURNE.

In respect of any Grain in truckloads carried to any port outside of Melbourne which is situated nearer to the forwarding station than Williamstown Pier, for export beyond the State, a rebate shall in such cases as it may be necessary, and subject to the production of satisfactory proof of export, be granted of sufficient extent to cause the net rate from the forwarding station to such port to be 1s. 3d. per ton less than the rate under the mileage scale from the forwarding station to Williamstown Pier.—

Provided that in respect of any Grain in truckloads carried from **Horsham and Stations North and West of Horsham to Portland** for export beyond the State, a rebate shall, in such cases as it may be necessary, and subject to the production of satisfactory proof of export, be granted of sufficient extent to cause the net rate from the forwarding station to Portland to be 1s. 3d. per ton less than the mileage rate from the forwarding station to Geelong, or 2s. 6d. per ton less than the mileage rate from the forwarding station to Williamstown Pier, as the case may be.

COMMODITY SPECIAL RATES—*continued.*

GRAIN, FLOUR, BRAN, POLLARD AND COMPRESSED FODDER, as under, in consignments of not less than 30 tons in trucks loaded to the minimum tonnage prescribed for the commodities concerned, shall be charged the following rates per ton (not including loading or unloading) for the journeys shown :—

	s.	d.
Wheat	7	0
Grain N.O.S. and Compressed Fodder	6	8
Flour, Bran and Pollard	5	7

Commodity	From	To
Grain for export (previously carried by rail)	Melbourne ... ... South Kenington ... Arden Street ... ... Burnley ... ... Glenroy ... ... Kensington ... ... Newmarket ... ... Macaulay ... ... Moreland ... ... Thomas' Mill (Newport) Fitzroy ... ... *North Fitzroy* ... Victoria Park ... ... Sunshine ... ... West Footscray Australian Barley Board's Siding (Newport–Sunshine Loop)	Williamstown Pier Port Melbourne Pier Victoria Dock
	Williamstown Pier ...	Port Melbourne Pier Victoria Dock
	Port Melbourne ...	Williamstown Pier Victoria Dock
	Victoria Dock ...	Williamstown Pier Port Melbourne Pier
Export Flour, Bran, Pollard and Compressed Fodder manufactured from grain or produce previously carried by rail	Melbourne ... ... Kensington ... ... South Kensington ... *Burnley* ... ... Glenroy ... ... Arden Street ... ... Macaulay ... ... Newport ... ... Fitzroy ... ... Victoria Park ... ... St. Albans ... ... Sunshine ... ... West Footscray ...	Williamstown Pier Port Melbourne Pier Victoria Dock
	Williamstown Pier ...	Port Melbourne Pier Victoria Dock
	Port Melbourne ...	Williamstown Pier Victoria Dock
	Victoria Dock ...	Williamstown Pier Port Melbourne Pier
Grain previously carried by rail (only at the close of the export season each year, the date of which shall be determined by the Chief Commercial Manager)	Williamstown Pier ... *Port Melbourne Pier* ...	Melbourne or suburban stations within 10 miles of Melbourne .

NOTES.—The above rates will not be granted in respect of agricultural produce rejected at the shipping ports during the currency of the export grain season.

For conditions under which goods may be forwarded to or from private sidings at Commissioners' or Owner's Risk see General Conditions Nos. 4 and 5, page 10.

COMMODITY SPECIAL RATES—*continued.*

GRAVEL, METAL, METAL SCREENINGS (DRY), RUBBLE, STONE DUST, TOPPINGS AND SPALLS.

The rates for the carriage in truck loads (see (*a*)) of Gravel, Metal, Metal Screenings (Dry), Rubble, Stone Dust, Toppings and Spalls, **consigned to Victorian Borough and Shire Councils and the Country Roads Board, for the construction and maintenance of roads, including footpaths, culverts and bridges,** shall be as prescribed hereunder :—(See Addenda for list of Borough and Shire Councils).

Miles	Rate per ton	Miles	Rate per ton
	s. d.		s. d.
1–10	11 1	141–142	55 6
11–13	12 1	143–144	56 0
14–17	12 9	145–147	57 0
18–21	14 11	148–150	58 2
22–26	15 8	151–153	59 10
27–28	16 4	154–157	61 8
29–31	17 2	158–165	64 6
32–33	18 0	166	65 8
34–35	18 6	167–173	66 2
36–37	20 0	174–176	66 8
38–40	20 4	177–181	67 8
41–42	21 10	182–186	68 10
43–50	22 0	187–191	70 2
51–60	22 6	192–196	70 10
61–70	24 10	197–200	71 10
71–72	25 0	201–204	72 6
73–74	26 4	205–206	73 8
75–76	27 4	207–210	74 4
77–78	28 2	211–212	75 0
79–80	29 4	213–216	76 6
81–82	30 6	217–221	77 8
83–84	30 10	222–226	78 6
85–86	31 4	227–231	79 0
87–88	31 8	232–236	80 0
89–90	34 2	237–241	80 6
91–92	34 10	242–246	81 2
93–94	35 6	247–249	83 0
95–96	35 10	250–254	84 0
97–98	37 0	255–259	84 6
99–100	38 0	260–264	85 4
101–102	38 8	265–269	86 6
103–104	39 10	270–274	87 2
105–107	40 6	275–279	88 8
108–110	42 0	280–284	89 4
111–112	42 8	285–289	90 2
113–114	43 8	290–294	91 2
115–116	44 2	295–299	92 2
117–118	45 2	300–301	92 10
119–120	46 10	302–306	93 4
121–122	47 6	307–311	95 0
123–124	48 2	312–316	96 4
125–127	49 0	317–321	96 10
128–130	49 2	322–326	97 4
131–132	50 10	327–331	98 4
133–134	51 10	332–336	99 0
135–136	53 0	337–341	99 4
137–138	53 6	342–360	101 6
139–140	53 10		

(*a*)—Subject to minimum tonnages of 10 ton 9 cwt. per 11 ton truck, 15 tons 4 cwt. per 16, 22 or 27 ton truck and 25 tons per bogie truck.

COMMODITY SPECIAL RATES—*continued.*

HAMS AND BACON (including Lard and Pigs' Feet), product of Victorian country industries on the Up journey consigned direct from the point of production to the seaboard for immediate export beyond the State, shall be charged under Class "AP" rate less 4¾ per cent., minimum 8 tons per 11-ton truck, and 12 tons per 16, 22 or 27-ton truck.

Consignment Notes and Waybills must be endorsed "For Export" and the name of vessel shown thereon.

When these commodities are consigned *to Melbourne* for export they shall be charged the ordinary Tariff rates in the first instance, and a rebate to the export rate provided shall be subsequently granted on production of a statutory declaration that the identical goods railed have been exported. Satisfactory proof of export must also be provided.

HOPS.

Hops forwarded from Eurobin to Melbourne shall be charged 110s. 0d. per ton, including unloading.

HOSE REELS, MANUAL FIRE ENGINES AND EQUIPMENT FOR FIRE BRIGADES, forwarded by rail to and from fire brigade demonstrations shall be charged ordinary rate on the forward and return journeys.

On production of a statutory declaration or statement from the Stationmaster at the original forwarding station that all the competing members of the brigade travelled by rail to and from the demonstration, free carriage of the hose reels, equipment and engines on the backward journey will be allowed.

ICE.

(*a*) *Ice, supplied for the purpose of cooling fish* carried by rail, shall be charged under Class "A" rate, subject to the tariff for "Smalls," provided that it be supplied for that purpose. The maximum charge for any distance shall be 45s. 2d. per ton Owner's Risk if cheaper than the tariff rate, and provided that the ice is forwarded to destination in empty trucks going down for fish or butter. Stationmasters must see that ice carried at this rate is applied for the sole purpose of Fish Refrigerating.

When sent in VANS of Passenger, Mixed or Goods trains, the tariff rates shall be charged.

When ice is supplied by the Commissioners, £4 17s. 6d. per ton shall be charged on the waybill for the ice, in addition to the freight charge as above.

(*b*) *Ice from Melbourne to Stony Point* shall be charged 27s. 5d. per ton, Owner's Risk, subject to the tariff for "Smalls."

(*c*) *Ice, supplied by the Department to the lessees of Refreshment Rooms* shall, when forwarded on the Down journey in "T" trucks being used in the butter traffic, be charged at the rate of 8s. 6d. per block (about 1 cwt.), including freight, at Owner's Risk.

LACTOGEN, Produce of the Commonwealth, railed from country districts in Victoria on the "Down" journey to Victorian Coastal Ports for carriage by sea to another Victorian Port for immediate trans-shipment there to an Interstate or Overseas vessel for export beyond the State, shall be charged Class "2" rate in the first instance. On production of a statutory declaration and satisfactory proof that the identical goods have been exported, a rebate to Class "BB" shall be granted in respect of consignments exported beyond the State, and to Class "BB" less 25 per cent, in respect of consignments exported beyond the Commonwealth. The latter rate is subject to a minimum of 7 tons per 11-ton truck, and 10 tons per 16, 22 or 27-ton truck.

LIME AND LIMESTONE (Owner's Risk, see Volume II. for conditions of Carriage).

The following special rates and conditions shall be charged :—

From—	To—	Special rate (see (*d*))
Stations and Sidings on the Timboon line	Melbourne and Suburbs within a radius of 20 miles of Melbourne	4d. per ton per mile
" " " "	Other Stations (see (*a*))	Per ton s. d.
Kawarren	Melbourne	40 10 (*b*), (*c*)
Lara	Burnley	15 2
Nowa Nowa	Melbourne	59 8 (*c*)

(*a*) Consignments to other stations shall be charged the ordinary tariff rate unless cheaper to charge the sum of the special rate to Melbourne thence tariff rate to destination.

(*b*) Plus transfer charges, see page 176.

(*c*) Clause 3 of Instructions governing the District Rates shall apply to suburban stations within a radius of 20 miles of Melbourne (either Flinders Street or Spencer Street).

(*d*) All these rates shall be subject to minimum tonnages of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

COMMODITY SPECIAL RATES—*continued.*

MALT COMBINGS AND BREWERS' GRAINS FOR MANURE shall be charged the ordinary tariff rates in the first instance, but if used for manurial purposes, the rate may be reduced to Manure Rate (subject to minimums specified below) on production of a Statutory Declaration by the consignee that the total quantity of Malt Combings or Brewers' Grains, as the case may be, in each particular consignment has been used for manure.

	Per 11-ton truck	Per 16, 22 or 27-ton truck
Minimums—Brewers' Grains	8 tons	12 tons
Malt Combings	6 tons	9½ tons

In every case where a reduction to Manure rate is made, the Chief Commercial Manager's authority must be obtained.

MEAT.

Carried at Owner's Risk **only**. Owner to load and unload.

	Per Louvre Truck	Per TT Truck	Minimum per Special Trip
(1) MEAT, FRESH (Melbourne and Suburbs)—			
From Newport, Borthwick's Siding, N. S. Smorgan and Sons' Siding, Newmarket, Angliss' Siding, the City Abattoirs, or other suburban stations within 9 miles of Melbourne to Port Melbourne Pier ...	s. d. 99 9	s. d. 147 11	s. d. 598 7
From Newport, Borthwick's Siding, N. S. Smorgan and Sons' Siding, Newmarket, Angliss' Siding, the City Abattoirs, or other suburban stations within 9 miles of Melbourne to Government Cool Stores at the Victoria Dock, Victoria Dock, or Melbourne ...	66 6	—	398 9
From Newmarket, Angliss' Siding, the City Abattoirs, or other suburban stations within 9 miles of Melbourne (Newport excepted), to Williamstown Pier	99 9	—	598 7
From Newport, and Borthwick's Siding, or N. S. Smorgan and Sons' Siding to Williamstown Pier ...	66 6	—	398 9

	Per 12-ton Truck	Per 15-ton Truck	Minimum per Special Trip
(2) MEAT, FROZEN (Melbourne and Suburbs)—			
Between Melbourne, Government Cool Stores at the Victoria Dock, Angliss' Siding, the City Abattoirs, or Suburban Freezing Works within 9 miles of Melbourne, and the Victoria Dock, Newport, or the Port Melbourne and Williamstown Piers ...	s. d. 99 9	s. d. 143 11*	s. d. 598 7
From Victoria Dock to Port Melbourne or Williamstown Pier	99 9	143 11*	598 7
From Government Cool Stores at the Victoria Dock, or the Victorian Butter Factories Siding to ships berthed at the Victoria Dock	s. d. 53 9	s. d. 79 2	s. d. —
Between Government Cool Stores, Victoria Dock, and the Victorian Butter Factories Siding ...	53 9	79 2	—
From Port Melbourne Siding to Port Melbourne Pier ...	53 9	79 2	—

* The charge for trucks numbered 343 to 392 inclusive shall be 126s. 6d. per truck.

COMMODITY SPECIAL RATES—*continued.*

(3) **RABBITS** (see pages 188 and 189).

(4) **MEAT, FROZEN** (from country stations to Melbourne, or Piers, by special train)—The tariff rates shall be charged, subject to a minimum of 15 "T" trucks for each special train.

(5) **SPECIAL TRIPS NOT OTHERWISE PROVIDED FOR—**

(*a*) Frozen produce loaded at the Government Cool Stores at the Victoria Dock, Victorian Butter Factories Company's Siding, or the Melbourne Goods Depot, shall be conveyed to Port Melbourne Pier or Williamstown Pier by ordinary trains under the ordinary tariff rates, if loaded and ready for despatch, as under (subject to alteration from time to time and subject to sufficient local loading being available to justify the running of such trains) :—

For Port Melbourne Pier—At 4 a.m. and 2.30 p.m. daily, Saturdays, 1 p.m.

For Williamstown Pier—At 4.30 a.m., 9,30 a.m.. 1 p.m. and 8 p.m. daily.

Consignors to notify the Superintendent of Melbourne Yards at least two hours before time of clearance.

(*b*) The Commissioners shall not be under obligation to remove trucks loaded with frozen produce from the Government Cool Stores at the Victoria Dock, Victorian Butter Factories Company's Siding, or Melbourne Goods Depot, at hours other than those specified in the preceding clause, or forward such trucks to Port Melbourne Pier or Williamstown Pier (as the case may be), by special train unless there is, at the place requiring clearance, a minimum of six fully-loaded trucks, or the freight charge on the goods is at least £29 16s. 1d. ; but if it is desired that frozen produce in any quantity less than the stated minimum be forwarded by a special train already arranged to run, and notice of at least two hours is given to the Superintendent of Melbourne Yards accordingly, such produce may be cleared and forwarded by such train, subject to a charge of 42s. 8d. in each instance to cover the cost of the special clearance.

(*c*) For special trips not otherwise specified, a minimum charge shall be imposed as may be arranged by the Chief Commercial Manager.

(6) **FROZEN MEAT SHUT OUT OR REJECTED AT PORT MELBOURNE OR WILLIAMSTOWN PIERS.**

Frozen meat shut out or rejected at the Piers and returned to Melbourne or suburban sidings, to be subsequently railed back to the Piers for shipment, shall be charged half rate for both journeys, provided full rate has been charged on the first journey to the Piers.

MILK POWDER (SKIM) AND CONDENSED OR POWDERED BUTTERMILK, Produce of the Commonwealth, shall be charged ordinary rate in the first instance, *viz.*:—Class "BB" rate and conditions, and—

(*a*) On production of a statement from the consignee that the skim milk powder or the condensed or powdered buttermilk is to be used as poultry or animal food, the charges may be reduced to Class "AP" rate and conditions by the receiving station.

(*b*) When consigned to a storekeeper, an application for rebate to Class "AP" and conditions, accompanied by a statement from the actual user of the skim milk powder or condensed or powdered buttermilk that it has been utilized for poultry or animal food, must be submitted to the Chief Commercial Manager.

MOTOR ENGINE REPLACEMENTS.

The Ford Motor Company of Australia Pty Ltd. operates a replacement scheme under which they will forward from Melbourne or Geelong a "reconditioned" engine to be fitted to a secondhand car or truck by the Company's Agents in country towns. The engine taken out "replaced" will be returned to the Company at Melbourne or Geelong. The following regulations will apply to this traffic :—

	Freight to be charged.
1. "Reconditioned" motor engine forwarded by the Company from Melbourne or Geelong to a country station.	Full ordinary Tariff rates and conditions.

COMMODITY SPECIAL RATES—*continued.*

Motor Engine Replacements.—*continued.*

2. "Replaced" motor engine sent from a country station by the person who receives the "reconditioned" motor engine to the original forwarding station account the original consignor of the "reconditioned" engine (Ford Motor Company of Australia Pty. Ltd.).

Freight to be charged.

Provided full tariff rate has been paid on the "Reconditioned" motor engine (clause 1) **half the full ordinary tariff rate** shall be charged for the "Replaced" motor engine, provided the consignment note is endorsed "Replacing 'Reconditioned' engine received on .../..../19...., for which freight charges amounting to £—s—d. have been paid" (see Note A). The waybill for the replacement engine issued at the country station shall be endorsed when issued as follows :—"Replacing 'Reconditioned' engine received on ...../...../19...., Waybill No....., Abstract No..... from..... station to..... station."

Note "A."—If it is cheaper to charge Freight Contract or the District rates shown on pages 161 and 163, for either of the consignments referred to in clauses 1 and 2, the half rate provided in clause 2 shall not be allowed for the "Replaced" motor engine.

MOTOR ENGINE OR CYLINDER BLOCKS forwarded by goods train from country stations to Melbourne for re-boring purposes and returned to the original despatching station, are to be charged half the ordinary tariff rates on the return journey, provided the consignment note is endorsed "forwarded by rail and returned after re-boring."

OIL, PARAFFIN, used for the purpose of cleaning dried fruits, shall be charged at the ordinary rate in the first instance, but on production of a written statement by the consignee to the Stationmaster at the inwards station that the Paraffin Oil will be used for the purpose mentioned, the freight charges may be reduced to Class "A" rate and conditions. (See Volume II for conditions of carriage).

OWNER'S TANK TRUCKS.

The free return of empty tank trucks is allowed only when a tank truck is returned direct from the destination station to which it was sent loaded to the station from where it was forwarded full.

In all other instances a charge of 7s. 11d. per mile shall be made for the haulage of the empty tank truck.

PAPER railed from Melbourne to Warburton to be used in the manufacture of books, periodicals, &c., shall be charged as follows :—

In truck loads of 8 tons per 11-ton truck, or 12 tons per 16, 22 or 27-ton truck ... ... Class C, less 31½ per cent.
Smaller quantities ... ... Class I, less 26½ per cent.

POTTERYWARE, product of Victorian Country Industries consigned direct for export shall be charged Class "M" less 21 per cent. subject to a minimum of 5 tons per 11-ton and 7½ tons per 16, 22 or 27-ton truck.

When the traffic is consigned to *Melbourne* for export it shall be charged the ordinary Classification Rates in the first instance, and a rebate to the export rate provided shall be subsequently granted on production of a statutory declaration that the identical goods railed have been exported. Satisfactory proof of export must also be provided.

POTTERYWARE, EARTHENWARE AND SANITARYWARE, SPECIAL RATES.

From	To	Owner's Risk Rate Per ton †*	—
		s. d.	
Epsom	Melbourne	40 10	Subject to a minimum of 5 tons per 11-ton truck and 7½ tons per 16, 22 or 27-ton truck
,, ...	Yarraville or Deer Park...	42 7	

* May be accepted at Commissioners' Risk—

(ii) when properly packed as prescribed in the Classification, and

(ii) subject to the additional charges and conditions shown in General Conditions Nos. 4 and 5, page 10.

† The reduction provided on page 146 for Products of Victorian Country Industries is not applicable in respect to the rates specified above.

COMMODITY SPECIAL RATES—*continued.*

PRINTED MATTER FOR COMMONWEALTH DEPARTMENTS, forwarded by the Government Printer from Melbourne to Canberra and other Capital Cities, or from Canberra to other Capital Cities, by goods train, shall be charged two-thirds of the ordinary mileage rate on each system, unless it is cheaper to charge Intersystem rates where the latter are applicable.

PUBLICITY MATTER for Australia National Publicity Association shall be conveyed free over all Australian Systems.

" QB " TRUCKS.

Goods of exceptional dimensions, which require to be carried in these trucks shall be charged the tariff rates subject to a minimum weight as for four 4-wheeled trucks plus 59s. 9d. for the use of the special truck.

RAILS.

Old Iron and Steel Rails which are intended for use in the construction of timber tramways running into railway stations will be carried in full truck loads at Class " M " rate. This reduced rate will, however, be granted only when specially authorized by the Chief Commercial Manager.

RAILWAY REFRESHMENT ROOMS.

1. **Rooms Managed by the Department.**—Consignments for Railway Refreshment Rooms managed by the Superintendent of Refreshment Services shall be waybilled free on the ordinary goods waybill, which must be numbered and abstracted in the usual manner.

The number of packages and the weight must be shown on the waybill, and particular care is to be exercised that proper receipts are obtained on delivery. Any shortage, ullage or damage must be immediately reported to the Claims Agent.

Urgent consignments not exceeding 3 cwt., whether perishable or otherwise, may be forwarded by any passenger train except the Sydney or Adelaide Express trains.

These regulations also apply to consignments account the Refreshment Services Poultry Farm, Noble Park, Ouyen Rest House, Refreshment Services Store and The Chalet, Mt. Buffalo National Park.

2. **Rooms Leased to Private Persons.**—Freight charges must be imposed in respect of consignments forwarded to Railway Refreshment Rooms, or fruit or other stalls which are leased from the Department.

Urgent consignments of fruit and other perishable goods, not exceeding 3 cwt. per consignment, for such lessees of Refreshment Rooms, may be forwarded at goods rates by any passenger train except the Sydney or Adelaide express trains.

SALT EX CHEETHAM COMPANY'S SIDING SOUTH GEELONG.

(*a*) Salt in truck loads of 11 tons per 11-ton truck or 16 tons per 16, 22 or 27-ton truck forwarded by the company from the siding to Melbourne shall be charged for at the rate of 19s. 4d. per ton, such rate to include unloading at Melbourne, but the loading at the siding shall be done by the company. Clause 3 of the Instruction governing the application of District rates (page 162), shall also be applicable in respect of salt from the siding to suburban stations.

(*b*) Any consignment of refined salt forwarded to stations other than Melbourne and suburban stations within 20 miles of Melbourne, shall be charged the ordinary tariff rates and conditions as from Geelong, plus an additional charge of 1s. 2d. per ton for carriage for the portion of the journey between the siding and Geelong.

Unrefined salt forwarded to stations other than Melbourne and suburban stations within 20 miles of Melbourne shall be charged the ordinary tariff rates and conditions as from the siding.

(*c*) The reduction provided for Products of Victorian Country Industries is not applicable in respect to these rates.

(*d*) Ordinary rates and conditions shall apply where cheaper than the foregoing rates.

SALT, UNREFINED, FOR DESTROYING NOXIOUS WEEDS IN VICTORIA AND SALT AND LIME FOR MANURE PURPOSES shall be charged the ordinary rate in the first instance, and a reduction to Manure rate granted as follows :—

(*a*) On production of a statement from the consignee that the salt is to be used for destroying noxious weeds in Victoria, or the salt or lime is to be used for manurial purposes, the charges shall be reduced to Manure rate and conditions by the receiving station.

(*b*) When consigned to a Storekeeper, an application for rebate, accompanied by a statement from the actual user of the salt or lime that it **has been used** for manure or for destroying noxious weeds must be submitted to the Chief Commercial Manager.

SAND forwarded from Bayles to Metropolitan stations situated within a radius of 20 miles of Spencer Street shall be charged the SAND rate for the mileage as from Koo-wee-Rup.

COMMODITY SPECIAL RATES—*continued.*

SEED WHEAT, OATS, BARLEY AND MILLET.

(1) Pedigreed seed wheat, oats and barley forwarded from the following Government Agricultural Colleges and Farms, viz.:—

Longerenong Agricultural College, Werribee State Farm, Rutherglen State Farm, Dookie Agricultural College, Mallee Research Station, Walpeup.

to consignees at Victorian country stations shall, when forwarded in less than truck loads, be charged under the appropriate truckload rate for the commodity concerned subject to "Smalls" Minimum in the first instance, provided a certificate is furnished from the Principal of the Agricultural College or the State Farm Manager to the effect that the consignment consists of pedigreed seed wheat, oats, or barley, as the case may be, and the consignment note is made out accordingly.

(2) Wheat, Oats, Barley and Millet for seed, consigned by senders other than the Agricultural Colleges and State Farms specified in clause (1) to consignees at Victorian country stations, shall be charged the ordinary tariff rates and conditions in the first instance, but on production of a written statement from the consignee that the identical wheat, oats, barley or millet has been sown, the charges shall be adjusted under the appropriate truckload rate for the commodity concerned subject to "Smalls" Minimum.

SHELL GRIT forwarded from Portland to Melbourne in truck loads, minimum 10 tons 9 cwt. per 11-ton truck or 15 tons 4 cwt. per 16, 22 or 27 ton truck, shall be charged 74s. 4d. per ton.

Clause 3 of the Instructions governing the application of District Rates (page 162), shall also be applicable in respect of Shell Grit forwarded from Portland to suburban stations.

STONE.

Granite in rough blocks consigned from Harcourt by two or more consignors to two or more consignees at two different destination stations in Melbourne and the suburbs, shall be charged at the truck load rate, viz., Class "M," provided that the stone is loaded into one truck to the prescribed tonnage minimum.

Granite, undressed, forwarded from Casterton to West Footscray or Melbourne shall be charged 65s. 2d. per ton, subject to a minimum tonnage of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27 ton truck.

Marble, undressed, and Marble Chips, forwarded from Nowa Nowa to Melbourne shall be charged 59s. 8d. per ton, subject to a minimum tonnage of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

STRAWBOARD railed from Melbourne to Warburton to be used in the manufacture of books, periodicals and cartons shall be charged :—

In truck loads of 8 tons per 11-ton truck and 12 tons per 16, 22 or 27-ton truck	Class A, less 28½ per cent.
Smaller quantities	Class C, less 28½ per cent.

TELEGRAPH POLES, SPECIAL TRAINS FOR DISCHARGING.

(*a*) **Service of Special train.**

20s. 8d. per mile shall be charged for the forward journey, and 10s. 3d. per mile for the return journey, minimum charge £11 1s. 10d. plus supervision charges (if any).

(*b*) **Freight Charge.**

In addition to the special service charge, the ordinary freight charges shall be imposed for the full mileage that each loaded truck is hauled on the forward journey, and if it is impracticable owing to train arrangements to complete the distribution of the poles on the forward journey, and part of the work has to be done on the return journey, no further freight charge shall be made.

WATER SUPPLIED TO DRY DISTRICTS.

The ordinary rate to be charged for water carried by rail is Class "M," minimum 9 tons per 2,000 gallon water tank truck.

Exception.—In circumstances such as the supply of water for domestic and stock use becoming exhausted, lower special rates may be authorised by the Chief Commercial Manager on each occasion that a truck load of water is forwarded.

In submitting applications to the Chief Commercial Manager, Stationmasters must furnish the following particulars :—

(*a*) Name of consignee.
(*b*) Station to and from which water is to be railed.
(*c*) Number of trucks of water required.
(*d*) Is water required for domestic use or for stock or for some other purpose (to be explained).
(*e*) Have all local supplies of water failed.
(*f*) Distance from nearest alternative source of supply of water.
(*g*) How are other settlers in district obtaining supplies of water.
(*h*) Stationmaster's recommendation to supply water at special rates.

When authority is received from the Chief Commercial Manager to charge special rates, the authority must be attached to the waybill and a copy of the waybill sent immediately to the Chief Commercial Manager's Office.

District Rates.

1. The District rates specified hereinafter are in all cases subject to the following conditions:—

(i) That such District rates shall apply only where the person, firm, or body corporate or unincorporate obtaining the benefit of such rates uses the railways solely for the transportation of goods where in the opinion of the Chief Commercial Manager railway facilities are available and might reasonably be used.

(ii) That if at any time the Chief Commercial Manager is of opinion that any goods are being or have been forwarded by road or any method of transportation other than by rail for or on behalf of any person, firm or body corporate or unincorporate obtaining the benefit of the District rates when railway facilities were available and might reasonably have been used for such transportation, the Chief Commercial Manager may direct that the rates specified shall not apply to any goods consigned to or on behalf of or by or on behalf of any such person, firm or body, and in the event of the Chief Commercial Manager so directing the ordinary full tariff rates and conditions, shall thenceforth be charged and paid in respect of such goods.

2. The District rates per ton between Melbourne and the stations named hereafter shall, in respect of the classes of goods and commodities specified, be as follow :—

Between Melbourne and—	Class B Minimum 1 ton	Class BB Minimum 1 ton	Class C Minimum 1 ton	Class 1. "Smalls Minimum	Class 2. "Smalls Minimum	Sugar— Minimum 20 tons	Sugar— Minimum 5 tons	Kerosene— Minimum 2 tons
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
North Geelong	29 0	23 5	29 0	29 0	35 10	...	...	...
Geelong	29 0	23 5	29 0	29 0	35 10	...	...	...
South Geelong ...	31 7	26 0	31 7	31 7	38 2	...	...	...
Colac	...	...	...	83 8	96 8	...	...	...
Ballarat / Ballarat East ...	...	...	59 7	60 6	74 11	55 4	...	...
Bendigo	...	...	...	...	...	76 0	...	...
Nunga	...	...	...	235 11	278 5	...	...	...
Ouyen	...	...	...	231 2	272 2	...	...	...
Kiamal	...	...	...	226 2	265 11	...	185 7	...
Trinita	...	...	...	217 11	257 1	...	180 8	204 1
Hattah	...	...	208 4	208 4	245 6	...	174 0	197 1
Nowingi	...	...	197 10	197 10	230 8	...	164 3	185 7
Boonoonar	...	...	193 5	193 5	226 2	...	159 7	182 3
Carwarp	...	...	188 0	188 0	219 0	...	156 9	178 3
Yatpool	...	...	181 3	181 3	210 9	...	149 1	170 8
Redcliffs / Irymple / Mildura / Merbein / Yelta ...	154 4	125 1	163 10	163 10	190 6	...	131 9	151 5
Thurla	...	...	183 8	183 8	213 3	...	151 8	172 11
Benetook	...	...	190 2	190 2	196 6	...	156 9	179 10
Pirlta	...	...	194 10	194 10	227 9	...	161 7	184 6
Merrinee	...	...	201 2	201 2	234 6	...	166 3	191 1
Karrawinna	...	...	206 4	206 4	242 9	...	170 11	198 2
Werrimul	...	...	213 9	213 9	249 6	...	176 8	202 6
Bambill	...	...	217 11	217 11	257 1	...	180 8	206 1
Yarrara	...	...	222 11	222 11	262 6	...	184 1	208 8
Meringur	...	...	227 9	227 9	268 10	...	188 5	214 1
Karween	...	...	232 7	232 7	273 2	...	190 7	219 8
Morkalla	...	...	236 4	236 4	279 6	...	195 4	223 11

DISTRICT RATES—*continued.*

3. The rates prescribed in Clause 2 hereof shall unless otherwise specified also have application under the following provisions (see sub clauses (*a*) and (*b*)) to similar traffic between suburban stations within 20 miles of Melbourne (either Flinders Street or Spencer Street), and the stations specified, but shall not otherwise have any application whatsoever, except as provided in Clauses (*c*), (*d*), (e) and (*f*). hereof

(*a*) *If the suburban station be of equal or less distance than Melbourne (Spencer Street), from the District Rate Station,* the same rates shall be charged as between the District Rate Station and Melbourne if less than the mileage rates.

(*b*) *If the suburban station be of greater distance than Melbourne (Spencer Street) and is situated—*

(i) *beyond and via Melbourne.*—The rates as between the District Rate Station and Melbourne shall be charged plus the difference between the mileage rates from the District Rate Station to Melbourne and from the District Rate Station to the suburban station.

(ii) *not beyond or via Melbourne*—The rates as between the District Rate Station and Melbourne shall be charged plus the difference between the mileage rate from the District Rate Station to the junction station on the main line and the mileage rate from the District Rate Station to the suburban station *via* such junction station on the main line, provided that such combined rates be less than the mileage rates for the whole distance. The Broadmeadows–Albion loop line is not to be used in the computation of freight charges under the conditions of this clause.

(*c*) The District rates prescribed in Clause 2, in respect of the traffic between Melbourne and Ouyen, and all stations thence to Yelta and Morkalla, shall be the rates between such stations and stations on the direct line from Melbourne on either the Ballarat, Castlemaine, or Geelong route, if less than the mileage rates.

(*d*) The rates for traffic between Bendigo and Mildura, Merbein, Irymple, Redcliffs, Yelta and stations on the Morkalla line, shall be the same as the District rates provided in clause 2 hereof, between such stations and Melbourne, when cheaper than the mileage rates.

(e) Goods between Harcourt, Kangaroo Flat, Golden Square, or Epsom and Mildura, Merbein, Irymple, Redcliffs or Yelta shall be charged mileage rates between Harcourt, Kangaroo Flat, Golden Square, or Epsom and Bendigo, plus the District rates between Bendigo and Mildura, Merbein, Irymple, Redcliffs, or Yelta, when cheaper than charging mileage rates for the through journey.

(*f*) The District rates prescribed in Clause 2, in respect of traffic between Melbourne and North Geelong shall also apply between Melbourne and certain private sidings situated at Corio, Geelong or North Geelong as provided in Volumn III.

DISTRICT RATES—*continued.*

The District Rates per ton between Melbourne and the stations named hereunder shall, in respect of the classes of goods and commodities specified, be as follow :—

Between Melbourne and—	**General Merchandise (not otherwise herein specified) which is ordinarily chargeable at higher rates per ton, with the exception of vehicles and wool** (a) Per ton	Furniture		Beer	Galvanised Iron	Sugar		Cement	
		Packed (a) Per ton	Not Packed (a) Per ton	(a) Per ton	(a) Per ton	Truck Loads (b) Per ton	Smaller Quantities (a) Per ton	Truck Loads (c) Per ton	Smaller Quantities (a) Per ton
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Albury	188 1	188 1	258 7	114 5	137 4	93 4	123 6	74 11	102 11
Wodonga	186 6	186 6	255 3	111 7	133 11	92 5	122 8	72 1	100 4
Wahgunyah	173 4	173 4	241 7	105 10	127 0	88 0	115 10	70 4	94 1
Oaklands	193 0	193 0	267 2	128 8	...	97 8	128 8	76 8	104 8
Wangamong	191 5	191 5	262 11	...	...	94 1	128 8	75 8	103 11
Sangar	186 6	186 6	257 0	...	...	93 4	128 8	72 1	100 4
Rennie	181 6	181 6	249 5	...	...	88 11	128 8	71 1	97 8
Warragoon	176 6	176 6	244 1	...	...	88 0	128 8	70 4	95 1
Sloane	173 4	173 4	237 2	...	...	84 6	128 8	66 6	93 4
Mulwala	168 4	168 4	232 0	...	...	83 7	128 8	65 8	89 8
Cobram	161 10	161 10	224 3	98 8	118 5	79 2	105 7	63 8	87 1
Tocumwal	163 5	163 5	225 8	98 8	118 5	81 11	106 4	64 8	88 0
Deniliquin	188 1	188 1	258 7	114 5	137 4	93 4	123 6	74 11	102 11
Gulpa	178 4	178 4	246 6	110 1	132 1	88 11	128 8	70 4	97 8
Mathoura	171 7	171 7	237 10	105 10	127 0	87 1	112 6	69 4	93 4
Moira	166 7	166 7	228 8	102 11	123 6	82 8	112 6	65 8	93 4

(a) Minimum charge 2s. 4d. per consignment note.

	Per 11-ton Truck Tons. cwt.	Per 16, 22 or 27-ton Truck Tons. cwt.
(b) Sugar, subject to a minimum of	10 0	15 0
(c) Cement	10 9	15 4

(d) Consignments between the above country stations and suburban stations within 20 miles of Melbourne shall be charged these District rates subject to the conditions and additional charges where applicable shown in clause 3, page 162.

(e) The Concessions to Victorian country industries provided on pages 139 to 148 (clauses 1, 2, 3, 4 and 5) shall not apply to these rates.

DISTRICT RATES—*continued.*

BETWEEN MELBOURNE AND COWES (PHILLIP ISLAND) AND TANKERTON (FRENCH ISLAND).

1. Through traffic carried by rail between Melbourne and Stony Point, thence by boat by the Cowes Ferry Company shall be charged at the rates shown hereunder for the carriage by rail and boat.—

—	Rice, Salt, Sugar, Gal. Iron, Kerosene (at owner's risk only), Softwood, or Mixed Consignments thereof. Rail min. 1 ton. Chicory roots, dried. Rail Minimum 10 cwt. *	Butter Rail Minimum 1 ton *	Class M. Rail Minimum 2 tons *	Class AP. Rail Minimum 2 tons *	Class Grain, Rail Minimum 2 tons *	Class A. Rail Minimum 1 ton *	Class B. Rail Minimum 1 ton *	Class C. Rail "Smalls Minimum	Class 1. Rail "Smalls Minimum	Class 2. Rail "Smalls Minimum
Per Ton	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
RAIL rates at Owner's Risk †	21 8	18 5	20 8	20 8	18 11	20 8	20 8	30 6	30 6	37 10
BOAT rate minimum 20 cwt. ... ... (See clauses 2 and 3)	15 0	15 0	15 0	15 0	15 0	15 0	15 0	15 0	15 0	15 0

* Smaller quantities shall be charged Class C rail "Smalls Minimum" specified above when cheaper than charging ordinary mileage rate for rail proportion.

† The conveyance of goods at the Commissioners' Risk in accordance with General Condition No. 4 page 10, applies only to the rail portion of the journey.

For wharfage and transfer charges see clauses 4 and 5.

Consignments between Stony Point and suburban stations within 20 miles of Melbourne shall be subject to the District Rate conditions shown in Clause 3, page 162.

2. BOAT CHARGES FOR CONSIGNMENTS WEIGHING LESS THAN ONE TON (except as shown in clause 3).

The following boat charges shall be added to the rail proportion shown in clause 1 instead of the boat proportion shown in that clause :—

Weight not over— cwt.	qr.	s. d.	Weight not over— cwt.	qr.	s. d.	Weight not over— cwt.	qr.	s. d.
0	1	1 0	2	0	3 0	10	0	10 0
0	2	1 6	3	0	4 0	15	0	12 6
1	0	2 0	5	0	6 0	20	0	15 0
1	2	2 6						

DISTRICT RATES—*continued.*

Between Melbourne and Cowes, etc.—*continued.*

3. BOAT CHARGES FOR SPECIAL CONSIGNMENTS AS UNDER :—

The following boat charges shall be added to the rail charges for through traffic for the undermentioned commodities instead of the boat rates shown in clauses 1 and 2 :—

		s.	d.
Chicory	... per ton	20	0
Furniture	... per ton	25	0
Superphosphate	... per ton	10	0
Motor Spirit	per drum	4	0
Explosives	per cylinder	3	0
Seaweed	per bale	1	9
Wool	per bale	6	0
Cream	per large can	1	9
Cream	per small can	1	6
Vealers	per carcase	2	0
Beef	per quarter	3	6
Mutton	per carcase	1	6
Rabbits	per crate	2	6
Bicycles or Perambulators	... each	1	0
Motor Cycles	... each	5	0

4. WHARFAGE CHARGES.

(*a*) Wharfage charges shall be imposed on all goods (except as shown in sub-clause (*b*)), consigned to Cowes or Tankerton to be forwarded via Stony Point as under :—

General cargo—1/- per ton, with a minimum of 1d. per package.

Returned empties (not new, including casks, cases, boxes, crates and baskets, packages of biscuits and confectionery tins)—3d. per ton, with a minimum of 1d. for single return empties.

(*b*) (Exceptions : Goods belonging to Her Majesty's Government, Artificial Manures for agricultural purposes, Fresh Fish, Fishing and Secondhand Boats, Ice landed for preservation of fish being sent to Market, Firewood, Agricultural Implements.)

5. TRANSFER CHARGES.

A transfer charge of 1s. 10d. per ton, minimum 7d. per consignment, shall be imposed on all goods to or from Phillip Island or French Island per the boats of the Cowes Ferry Company forwarded by rail to or from Stony Point.

6. PREPAYMENT OF CHARGES.

Rail, boat and incidental charges shall be prepaid on goods consigned to Tankerton on French Island, and to Cowes on Phillip Island, for despatch from Stony Point by the boats of the Cowes Ferry Company.

7. WAY-BILLING.

Goods consigned to or from Phillip Island or French Island per the boats of the Cowes Ferry Company shall be way-billed through on Old and New lines way-bills showing Rail and Transfer charges and Boat and Wharfage charges separately.

The Boat's proportion of charges is to be computed separately for each sender on consignments from two or more consignors to one consignee.

THROUGH TRAFFIC BETWEEN MELBOURNE AND FRENCH ISLAND.

Through consignments between Melbourne and French Island, conveyed by boats other than those of the Cowes Ferry Company shall be charged the rail proportion of the rates shown in clause 1 for carriage by rail to or from Stony Point.

Intersystem Goods Rates.

1. The Intersystem Through Goods Rates shown hereafter are special Through Rates, which shall be charged between the cities specified, and shall not apply between any other stations except as shown in paragraphs 2 and 6.

2. These special Intersystem Rates shall also be charged to and from stations and sidings situated in the vicinity of Melbourne, Sydney, Newcastle and Brisbane (Roma Street only) within the maximum distance zone prescribed in the footnotes to the respective rates.

3. Each System's Classification of Goods and all other conditions of carriage on such System, shall apply to its appropriate proportion of the Through Intersystem Rates, with the following exceptions :—

(i) Goods specified in the respective Systems' classification under class rates not provided for in the Intersystem Through Rates shall be charged as follows :—

System	Class rate specified in Goods Rates Book	Intersystem Through Class rate to be charged
	Class	Class
Victoria	"Grain"	"AP"
	"SAP"	"A"
	"AA"	"A"
	"BB"	"B"
South Australia ...	"SAP"	"AP"
	"AA"	"A"
	"BB"	"B"
	"A" + 10/-	"A" + 10/-
	"B" + 10/-	"B" + 10/-
Commonwealth ...	"AA"	"A" + 12/-
	"BB"	"B" + 12/-
	"CC"	"C" + 12/-

Wherever in the classifications of the Systems concerned a rate is provided less a percentage or plus a percentage the rates to be charged shall be as shown hereunder :—

(*a*) Less a percentage—

N.S.W., Queensland Commonwealth ... The percentage shall be deducted from these Systems' proportion of the Intersystem Rates.

Victoria, South Australia, Western Australia The percentage shall not be deducted from these Systems' proportion of the Intersystem Rates.

(*b*) Plus a percentage—

All Systems... ... ... The proportion for any System plus the additional percentage which is provided for that System shall be charged.

(ii) Goods (including fruit) carried via Kyogle between Border Tunnel and South Brisbane chargeable at truck load rates shall be subject to the New South Wales truck load minima.

4. The sum of the *local rates on each System shall apply for the through journey in all cases where cheaper than the Intersystem Through Rates, but no total rate is to be made up of the local rate for one System added to another System's porportion of the through Intersystem rates, except as provided in paragraph 5.

* See District Rates, page 163.

INTERSYSTEM GOODS RATES—*continued.*

5. These special Intersystem rates shall not apply to the commodities shown hereunder viz :—

Basketware
Bicycles, cycles, tricycles, velocipedes, etc.
Bicycle frames and wheel-rims
Breakfast foods
Cabinets (radio)
Cages
Cans (empty)
Cartons (not collapsed)
Chimney glasses
Cotton wool and wadding
Concrete mixers (hand operated)
Electric light bulbs, globes, neon lights and fittings
Feathers and down
Hair
Hats
Ice Cream Cones
Jex
Lampware
Piping (down, and stove when not attached to stove)
Punnets, (strawberry etc.)
Safes (wire)
Sanitary pads and towels
Steel Wool
Tins (garbage and sanitary)
Tinware (loose or not nested)
Wheelbarrows (assembled)
Wool, mineral (Insul, Slag, etc.)

which shall be charged at the sum of the local rates based on the classification of the Systems concerned, with the following exceptions :—

(*a*) Consignments conveyed over the Commonwealth Railways shall continue to be charged for that System's proportion of the journey at the Intersystem rate for the applicable classification, and

(*b*) Consignments originating in South Australia, consisting of any of the commodities outlined above, if declared on consignment notes to measure less than 240 cubic feet to the ton and provided waybills are so endorsed, shall be charged for the South Australian proportion of the journey at the Intersystem rate for the applicable classification.

6. Goods forwarded Intersystem between other stations for which Intersystem Through Rates are not provided shall be charged at the sum of the local rates for each system concerned, unless it is cheaper to charge the combination of the Intersystem Through Rates concerned and the local rate for the mileage to or from the city to which such Intersystem Through Rate applies, less the mileage of the maximum distance zone specified in paragraph 2. For traffic charged at the sum of the local rate for each System, see Addenda pages 9 and 10, for terminal charges at Albury and Wodonga and New South Wales Uniform Freight Rates in the Darling Harbour Area.

7. The following charges for sundry services en route shall be imposed in addition to the freight charges for all Intersystem traffic forwarded between Victoria and New South Wales and Queensland :—

Loading at originating station and unloading at destination station (where due).

At Albury as prescribed in subclause (i.) and (iii.) ...	Transhipping Counting
At Tocumwal as prescribed in subclause (i.) and (ii.)...	Transhipping
At Wallangarra or Clapham (Queensland) as prescribed in subclauses (iv.) and (v.)	Transhipping Counting
For Queensland traffic charged at Intersystem Through Goods Rates only as prescribed in subclause (vi.)	Terminal Charges (Queensland)

INTERSYSTEM GOODS RATES—*continued.*

Clause 7—*continued.*

Details of these service charges are shown hereunder :—

(i) **TRANSHIPPING AT ALBURY AND TOCUMWAL.**

All goods transhipped by the Railways Department at Albury and Tocumwal, whether consigned direct or to the care of an agent, shall be charged for as under :—

	Albury (All N.S.W. proportion)	Tocumwal		
		Total	Proportions	
			Vic.	N.S.W.
	s. d.	s. d.	s. d.	s. d.
All commodities (except where otherwise specified) ... ... Per ton	5 0	5 0	2 10	2 2
*Goods—light, bulky—exceeding 200 cubic feet to the ton ... ... ,,	10 0	10 0	5 9	4 3
Vehicles, including motor cars, boats and aeroplanes ... ... ,,	10 0	10 0	5 9	4 3
Sheep skins (loose) ... ... ,,	10 0	10 0	5 9	4 3
Vegetables (loose) ... ... ,,	6 0	6 0	3 5	2 7
Wool ... ... Per Bale	0 9	0 9	0 5	0 4
,, ... Per Bag	0 3	0 3	0 2	0 1
Minimum charge—				
Per consignment of two or more packages ...	2 0	2 0	1 2	0 10
Single packages ... ... each	1 2	1 2	0 8	0 6

*Includes—

Aeroplane parts
Balsa wood
Basketware
Bedding
Bicylces
Bins
Breakfast foods (patent)
Cages
Cans (empty)
Cartons (not collapsed)
Cases and crates (empty)
Casks (empty)
Christmas stockings
Coal dust, in bags
Combine dressings
Corks
Drums (empty)
Electric light bulbs
Feathers and down
Felt pads
Fibre pads
Flock
Flowers (artificial)
Furniture
Hats
Hay
Heaters
Ice cream cones
Insulwool
Kapok
Lampware
Mattresses
Motor cycles and side cars
Musical Instruments
Pans
Perambulators and strollers
Popcorn
Sanitary paper and pads
Slag wool
Sponge rubber
Steel wool
Straw
Tanks
Theatrical scenery
Tins
Toys
Underfelt
Vats
Watering cans
Wheelbarrows (assembled)
Wireless cabinets
Wireless sets
Wood wool
Wool (knitting)

and other similar traffic of a light and bulky nature.

The Commissioners reserve to themselves the right to make a special charge for the transhipment of traffic when the cost of such transhipping exceeds the rates quoted, such charge to be not less than the actual cost of transhipment.

INTERSYSTEM GOODS RATES—*continued.*

Clause 7—*continued.*

(ii) TRANSFER OF INTERSYSTEM TRAFFIC BY THE MECHANICAL TRANSPORTERS AT WODONGA AND TOCUMWAL.

Intersystem traffic transferred by the mechanical transporters at Wodonga and Tocumwal will be charged at the rate of 2s. 6d. per ton and credited as follows :—

via Wodonga	All Victorian proportion.
via Tocumwal	Victoria—1s. 5d. per ton. New South Wales—1s. 1d. per ton.

(iii) COUNTING AT ALBURY.

A charge of 1s. 6d. per ton (minimum 1s. 0d. per consignment) will be made at Albury for counting goods charged at rates less than Class 1 (according to the N.S.W. Classification) in transit between Intersystems stations.

Counting charges shall not apply to a single package.

In instances where Fresh Fruit consigned from Victoria to New South Wales or Queensland is not tallied by the Department at the forwarding station, no tally will be made at Albury and in such instances this counting is not to be imposed.

The charge for counting fruit at Albury shall not be imposed in respect of consignments account McRostie & Growers Pty. Ltd., The Committee of Direction of Fruit Marketing and Fruit and Vegetables Traders Ltd.

(iv) TRANSHIPPING AT WALLANGARRA AND CLAPHAM.

The charges for transhipping all goods at Wallangarra and goods for Queensland stations beyond South Brisbane, at Clapham, whether booked through or requiring to be rebooked, shall be as under :—

	Per ton s. d.	Minimum charge per consignment s. d.
Theatrical scenery	Actual cost	4 1
Motor bodies	Actual cost	4 1
Timber (including counting)	10 5	2 9
Meat	9 1	2 9
All other traffic	8 9	2 9

The Commissioners reserve to themselves the right to make a special charge for the transhipment of traffic when the cost of such transhipping exceeds the rates quoted, such charge to be not less than the actual cost of transhipment.

Transhipping charges shall be divided between New South Wales and Queensland as follows :—

Traffic transhipped at Clapham.—All Queensland proportion.

Traffic transhipped at Wallangarra.—Divided equally between New South Wales and Queensland. When an odd penny is concerned, it shall be apportioned to New South Wales when traffic is forwarded to Queensland, or to Queensland when traffic is forwarded from Queensland.

(v) COUNTING AT WALLANGARRA AND CLAPHAM.

The counting charges at Wallangarra and Clapham shall be as follow :—

	Per ton s. d.	Minimum charge per consignment s. d.
All goods (single packages and timber excepted) carried at a rate less than Class 1	2 9	2 9

This charge shall be entered separately on waybills and apportioned similarly to transhipping charges.

For counting at Wallangarra the charge shall be based on the classification in the originating State for the traffic concerned, and at Clapham on the Queensland classification.

Exceptions.

(*a*) The charge for counting fruit at Wallangarra shall not be imposed in respect of consignments account McRostie & Growers Pty. Ltd., The Committee of Direction of Fruit Marketing and Fruit and Vegetable Traders Ltd.

(*b*) At Clapham the counting charge is not imposed on consignments of fruit or vegetables for The Committee of Direction of Fruit Maketing or D. G. Wills (Bowen Fruit Export Co.)

INTERSYSTEM GOODS RATES—*continued.*

Clause 7—continued

(vi) QUEENSLAND TERMINAL CHARGES.

Terminal charges (all Queensland proportion) as set out hereunder, shall be made on all consignments charged under the Through Intersystem Rates :—

Class C per cwt.	Class 1 per cwt.	Class 2 per cwt.	Minimum charge	Maximum charge per ton
s. d.	s. d.	s. d.	s. d.	s. d.
0 9	0 9	0 9	0 11	2 3

Fractions of 1 cwt. shall be charged as 1 cwt.

INTERSYSTEM THROUGH GOODS RATES PER TON

Between		Miles	Class A.	Class B.	Class C.	Class 1	Class 2	Mini-charge per Con-signment
			s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
MELBOURNE (a) and ADELAIDE	Vic.	287	63 7	82 4	107 5	117 3	149 6	7 9
	S.A.	194	42 11	55 8	72 7	79 3	101 0	5 3
		481	106 6	138 0	180 0	196 6	250 6	13 0
GEELONG and ADELAIDE	Vic.	268	61 9	80 1	104 5	114 0	145 4	7 6
	S.A.	194	44 9	57 11	75 7	82 6	105 2	5 6
		462	106 6	138 0	180 0	196 6	250 6	13 0
MELBOURNE (a) and SYDNEY (b) *via Albury*	Vic.	191	41 0	52 7	69 11	78 11	93 3	3 8
	N.S.W.	403	86 6	110 11	147 7	166 7	196 9	7 10
		594	127 6	163 6	217 6	245 6	290 0	11 6
via Tocumwal	Vic.	157	31 7	40 7	53 11	60 11	71 11	2 10
	N.S.W.	476	95 11	122 11	163 7	184 7	218 1	8 8
		633	127 6	163 6	217 6	245 6	290 0	11 6
MELBOURNE (a) and NEWCASTLE (c) via *Albury*	Vic.	191	40 0	52 4	69 6	78 5	93 0	3 9
	N.S.W.	492	103 0	134 8	179 0	202 1	239 6	9 9
		683	143 0	187 0	248 6	280 6	332 6	13 6
via Tocumwal	Vic.	157	31 1	40 8	54 0	61 0	72 4	2 11
	N.S.W.	565	111 11	146 4	194 6	219 6	260 2	10 7
		722	143 0	187 0	248 6	280 6	332 6	13 6

These rates shall also apply to and from stations or sidings distant by rail not more than —

(a) 20 miles from Melbourne.

(b) 25 miles from Darling Harbour and to or from stations Hornsby to St. Leonards inclusive.

(c) 25 miles from Newcastle.

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INTERSYSTEM THROUGH GOODS RATES PER TON—*continued.*

Between		Miles	Class A.	Class B.	Class C.	Class 1	Class 2	Minimum charge per Consignment
			s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
MELBOURNE (a) and SOUTH BRISBANE *via Albury and Kyogle*	Vic.	191	—	—	69 7	81 0	94 8	4 1
	N.S.W.	932	—	—	339 4	395 3	461 8	19 11
	Q.	69	—	—	25 1	29 3	34 2	1 6
		1,192	—	—	434 0	505 6	590 6	25 6
via Tocumwal and Kyogle	Vic.	157	—	—	55 4	64 6	75 4	3 3
	N.S.W.	1,005	—	—	354 4	412 8	482 1	20 10
	Q.	69	—	—	24 4	28 4	33 1	1 5
		1,231	—	—	434 0	505 6	590 6	25 6
MELBOURNE (a) and BRISBANE (Roma St.) (d) *via Albury and Wallangarra*	Vic.	191	—	—	70 2	81 8	95 5	4 1
	N.S.W.	768	—	—	282 0	328 5	383 8	16 7
	Q.	223	—	—	81 10	95 5	111 5	4 10
		1,182	—	—	434 0	505 6	590 6	25 6
via Tocumwal and Wallangarra	Vic.	157	—	—	55 10	65 0	75 11	3 3
	N.S.W.	841	—	—	298 11	348 2	406 9	17 7
	Q.	223	—	—	79 3	92 4	107 10	4 8
		1,221	—	—	434 0	505 6	590 6	25 6
SYDNEY (b) and ADELAIDE *via Albury*	N.S.W.	403	84 6	105 10	134 9	155 9	182 0	9 1
	Vic.	469	98 4	123 2	156 10	181 3	211 10	10 7
	S.A.	194	40 8	51 0	64 11	75 0	87 8	4 4
		1,066	223 6	280 0	356 6	412 0	481 6	24 0
via Tocumwal	N.S.W.	476	97 10	122 7	156 1	180 5	210 10	10 6
	Vic.	417	85 9	107 5	136 9	158 1	184 9	9 3
	S.A.	194	39 11	50 0	63 8	73 6	85 11	4 3
		1,087	223 6	280 0	356 6	412 0	481 6	24 0
SOUTH BRISBANE and ADELAIDE *via Kyogle and Albury*	Q.	69	—	—	23 5	26 8	31 7	1 6
	N.S.W.	932	—	—	315 7	359 7	426 6	20 2
	Vic.	469	—	—	158 10	180 11	214 8	10 2
	S.A.	194	—	—	65 8	74 10	88 9	4 2
		1,664	—	—	563 6	642 0	761 6	36 0
via Kyogle and Tocumwal	Q.	69	—	—	23 1	26 3	31 2	1 6
	N.S.W.	1,005	—	—	336 1	382 11	454 2	21 5
	Vic.	417	—	—	139 5	158 11	188 6	8 11
	S.A.	194	—	—	64 11	73 11	87 8	4 2
		1,685	—	—	563 6	642 0	761 6	36 0

These rates shall also apply to and from stations or sidings distant by rail not more than—

(a) 20 miles from Melbourne.

(b) 25 miles from Darling Habour and to or from stations Hornsby to St. Leonards, inclusive.

(d) 21 miles from Brisbane (Roma Street).

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INTERSYSTEM THROUGH GOODS RATES PER TON —*continued.*

TRANS-AUSTRALIAN AND WESTERN AUSTRALIAN RATES

Between—	Miles	Class							Minimum charge per Consignment
		A.P.	M	A	B	C	1	2	
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
MELBOURNE (a) and PERTH—									
Victoria	**287**	27 10	31 1	46 1	57 11	69 1	92 10	117 5	4 1
South Australia ...	**329**	31 11	35 8	52 10	66 4	79 2	106 5	134 8	4 8
Commonwealth ...	**1108**	107 5	120 1	178 0	223 7	266 9	358 4	453 5	15 10
West Australia ...	**380**	36 10	41 2	61 1	76 8	91 6	122 11	155 6	5 5
	2104	204 0	228 0	338 0	424 6	506 6	680 6	861 0	30 0
MELBOURNE and KALGOORLIE									
Victoria	**287**	26 11	31 4	46 10	59 4	71 10	93 4	118 10	5 0
South Australia ...	**329**	30 10	35 10	53 9	68 0	82 4	106 11	136 3	5 9
Commonwealth ...	**1108**	103 9	120 10	180 11	229 2	277 4	360 3	458 11	19 3
	1724	161 6	188 0	281 6	356 6	431 6	560 6	714 0	30 0
GEELONG and PERTH—									
Victoria	**268**	26 3	29 4	43 5	54 7	65 1	87 6	110 8	3 10
South Australia ...	**329**	32 2	36 0	53 4	67 0	79 11	107 5	135 10	4 9
Commonwealth ...	**1108**	108 5	121 2	179 8	225 7	269 2	361 7	457 7	15 11
West Australia ...	**380**	37 2	41 6	61 7	77 4	92 4	124 0	156 11	5 6
	2085	204 0	228 0	338 0	424 6	506 6	680 6	861 0	30 0
GEELONG and KALGOORLIE—									
Victoria	**268**	25 5	29 7	44 3	56 0	67 10	88 1	112 3	4 9
South Australia ...	**329**	31 2	36 3	54 4	68 10	83 3	108 2	137 9	5 9
Commonwealth ...	**1108**	104 11	122 2	182 11	231 8	280 5	364 3	464 0	19 6
	1705	161 6	188 0	281 6	356 6	431 6	560 6	714 0	30 0

(a) These Melbourne–Perth rates also shall apply to or from stations or sidings not more than 20 miles by rail from Melbourne.

The rates to Perth and Kalgoorlie shown above include transhipment at stations where break of gauge occurs on the direct route via Port Pirie Junction.

INTERSYSTEM FRESH FRUIT RATES.
VICTORIA AND NEW SOUTH WALES.

(See Notes on page 175)

Between	and	via	Total freight Per ton	Proportions per ton Vic.	Proportions per ton N.S.W.	Subject to minimum tonnage specified in notes (on page 174)
			s. d.	s. d.	s. d.	
1. Melbourne (and stations not more than 20 miles therefrom) and the following Goulburn Valley (Vic.) stations :—Merrigum, Mooroopna, Kyabram, Shepparton, Tatura, Tongala and Toolamba	Sydney (Darling Harbour)	Albury	119 2	36 2	83 0	(a)
			140 2	36 2	104 0	(b)
2. Emu Plains, Granville, Hawksbury River, Kurrajong, Liverpool, St. Leonards (N.S.W.) and and intermediate stations	Melbourne and stations not more than 20 miles therefrom					
3. Murwillumbah (N.S.W.)	Melbourne and stations not more than 20 miles therefrom	Albury	155 2	36 2	119 0	(a)
			185 2	36 2	149 0	(b)
4. Melbourne (and stations not more than 20 miles therefrom) and the following Goulburn Valley (Vic.) stations :—Merrigum, Mooroopna, Kyabram, Shepparton, Tatura, Tongala and Toolamba	Newcastle or Honeysuckle (N.S.W.)	Albury	127 2	36 2	91 0	(a)
			149 2	36 2	113 0	(b)

VICTORIA AND QUEENSLAND

(See Notes on page 175)

Between	and	via	Total freight per ton	Proportions per ton Vic.	Proportions per ton N.S.W.	Proportions per ton Q.	Subject to minimum tonnage specified in notes (on page 174)
			s. d.	s. d.	s. d.	s. d.	
5. Melbourne (and stations not more than 20 miles therefrom) and the following Goulburn Valley (Vic.) stations, Merrigum, Mooroopna, Kyabram, Shepparton, Tatura, Tongala, and Toolamba	South Brisbane	Albury and Kyogle	189 10	36 2	117 0	36 8	(a)
			219 10	36 2	147 0	36 8	(b)

VICTORIA AND SOUTH AUSTRALIA

				Vic.	S.A.		
			s. d.	s. d.	s. d.		
6. Melbourne (and stations not more than 20 miles therefrom) and from Ballarat and Geelong	Adelaide (Mile end)	—	100 0	59 8	40 4	—	(c)

INTERSYSTEM FRESH FRUIT RATES—*continued.*

SOUTH AUSTRALIA AND NEW SOUTH WALES

(See Notes on page 175)

Between	and	via	Total freight per ton	Proportions per ton			Subject to minimum tonnage specified in notes below and on page 175
				S.A.	Vic.	N.S.W.	
			s. d.	s. d.	s. d.	s. d.	
7. Adelaide (Mile End) ...	Sydney ...	Albury	219 2	40 4	95 10	83 0	(ba)
			240 2	40 4	95 10	104 0	(bb)
8. Adelaide (Mile End) ...	Murwillumbah (N.S.W.)	Albury	255 2	40 4	95 10	119 0	(ba)
			285 2	40 4	95 10	149 0	(bb)

SOUTH AUSTRALIA AND QUEENSLAND

Between	and	via	Total freight per ton	Proportions per ton				Subject to minimum tonnage specified in notes below and on page 175
				S.A.	Vic.	N.S.W.	Q.	
			s. d.	s. d.	s. d.	s. d.	s. d.	
9. Adelaide (Mile End)	South Brisbane	Albury and Kyogle	289 10	40 4	95 10	117 0	36 8	(ba)
			319 10	40 4	95 10	147 0	36 8	(bb)

NOTES.—These rates for fresh fruit are subject to such of the following minimum tonnages (*a*), (*b*), (*c*), (*ba*), and (*bb*) shown below as are indicated opposite the rates on pages 173 and 174:—

(*a*) Victoria—6 tons per 4-wheeled truck. Exceptions: 9 tons per 15-ton U truck.
New South Wales—Full weight carrying capacity of truck used except bogie louvred vans of 24 tons, 28 tons or 40 tons capacity, for which the minimum shall be 21 tons per truck.
Queensland—N.S.W. minima applies.

(*b*) Victoria—6 tons per 4-wheeled truck. Exception: 9 tons per 15-ton U truck.
New South Wales—U trucks 9 tons; other trucks 6 tons per 4-wheeled truck.
Queensland—N.S.W. minima applies.

(*c*) Victoria—8 tons per 4-wheeled truck; 16 tons per bogie truck.
South Australia ... 9 tons per Victorian 11-ton 'U' truck.
14 tons per Victorian 15 ton 'U' truck.
10 tons per other 4-wheeled truck.
20 tons per bogie truck.

(*ba*) South Australia ... 9 tons per Victorian 11 ton 'U' truck.
14 tons per Victorian 15 ton 'U' truck.
10 tons per other 4-wheeled truck.
20 tons per bogie truck.
Victoria ... 9 tons per Victorian 15 ton 'U' truck.
8 tons per other 4-wheeled truck.
16 tons per bogie truck.
New South Wales ... Full weight carrying capacity of truck used except bogie louvred vans of 24 tons, 28 tons or 40 tons capacity for which the minimum shall be 21 tons per truck.
Queensland ... N.S.W. minima apply.

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INTERSYSTEM FRESH FRUIT RATES—*continued.*

(*bb*) South Australia ... 9 tons per Victorian 11 ton 'U' truck.
14 tons per Victorian 15 ton 'U' truck.
10 tons per other 4-wheeled truck.
20 tons per bogie truck.

Victoria ... ... 9 tons per Victorian 15 ton 'U' truck.
8 tons per other 4-wheeled truck.
16 tons per bogie truck.

New South Wales ... 'U' trucks 9 tons, other trucks 6 tons per 4-wheeled truck.

Queensland ... N.S.W. minima apply.

(*d*) The service charges for loading, unloading, inspection, transhipping, counting shall be imposed where due, also the Uniform Rating charge of 6d. per ton applicable at Darling Harbour.

(*e*) When fresh fruit is sent to or from stations other than those shown above, mileage rates shall be charged to or from the nearest station shown above plus the Intersystem through rates when cheaper than charging the sum of the local rates.

Fresh fruit forwarded Intersystem to or from stations distant **more than 20 miles from Melbourne** shall be charged at the Intersystem Through rates to or from Melbourne, plus the ordinary mileage rates for the distance in excess of 20 miles from Melbourne, when cheaper than the sum of local rates.

(*f*) Consignments in truck loads from different consignors at the same station to one consignee on the same day and bearing one distinctive overriding brand shall be treated as one consignment.

(*g*) Fresh fruit loaded in REFRIGERATOR TRUCKS shall be charged as follows :—

Victoria—Tariff rates plus 25%, subject to full carrying capacity of the truck being charged for.

New South Wales.—At the special mileage rates for fresh fruit (Table C), subject to the following minima :—15 tons in 16-ton bogie refrigerator cars ; 19 tons in 20-ton bogie refrigerator cars.

Queensland.—Mileage rates (minimum 100 miles), plus 50%, subject to the following minima :—15 tons in 16-ton bogie refrigerator cars ; 19 tons in 20-ton bogie refrigerator cars.

(*h*) A terminal charge of 11d. per ton shall be credited on the waybill to New South Wales from the Victorian rates per ton shown in Schedules 1, 2, 3, 4, 5, 7, 8 and 9.

Charges for Transfer of Goods To and From Narrow-gauge Lines.

—	Colac	Upper Ferntree Gully	Moe (see note (**))
	Per ton s. d.	Per ton s. d.	Per ton s. d.
Briquettes	2 8	3 2	2 6
Firewood	2 8	3 2	2 6
Pulpwood	2 8(†)	3 2	2 6(†)
Sawn and other hardwood timber	2 8	3 2	2 6
Grain and other produce in bags	2 8	3 2	2 6
Iron Ore, Lime, Limestone (loose)	2 8	3 2	2 6
Lime in bags	2 8	—	2 6
Material, account Way and Works and Construction Branches (piles excepted)	2 8	3 2	2 6
Piles (including Scaffolding Poles)	2 8	3 6	2 6
Screenings	2 8	—	—
Goods not otherwise specified	2 8	3 2	2 6
Live Stock, per Narrow Gauge Truck	5 0	6 6	—
,, per Broad Gauge truck	—	—	6 0

Notes.—(*a*) Minimum Transfer Charges :—Small Vehicles, 3s. 4d. each ; other small consignments 7d. each.

(*b*) The Commissioners reserve to themselves the right to make a special charge for the transhipment of traffic when the cost of such transhipping exceeds the rates quoted above, such charge to be not less than the actual cost of transhipment.

(c) Bulky or heavy articles shall not be received for transfer to or from narrow-gauge lines except under special agreement.

(**) Any traffic including wood billets for paper pulp purposes transhipped at the Electric Gantry Crane at Moe shall be charged as follows :—

	Per ton
a/c Australian Paper Manufacturers	1s. 9d.
a/c other rail users	2s. 6d.

(†) Applies also to Wood Billets for paper pulp purposes.

DETENTION TO GOODS TRAINS ON BROAD AND NARROW-GAUGE LINES

When ordinary Goods Trains are stopped *en route* for the purpose of loading or discharging goods between stations on Narrow or Broad Gauge lines, the following charges shall be made for the delay to the train—

On Narrow Gauge lines ... 15s. 6d. for the first 15 minutes detention or part thereof, and 5s. 2d. for each 5 minutes detention or part thereof beyond the first 15 minutes.

On Broad Gauge lines ... 30s. 10d. for the first 15 minutes detention or part thereof, and 9s. 4d. for each 5 minutes detention or part thereof beyond the first 15 minutes.

and subject to freight charges being computed as to or from the adjacent station farthest from the loading or discharging point. These instructions shall also apply when (under special arrangements), ordinary goods trains are detained for the purpose of loading or unloading consignments exceeding 10 cwt. at certain stations which have no siding accommodation. The consignors shall be responsible for any damage to rolling stock resulting from the loading or unloading of heavy consignments due to lack of proper loading or discharging facilities.

Pier Charges.

CHARGES FOR HAULAGE OF GOODS BETWEEN GEELONG PIER AND GEELONG STATION AND BROUGHAM STREET CROSSING (GEELONG).

1. The rate for general cargo hauled between **Geelong Pier and Geelong Station** shall be as under :—

12s. 3d. per ton, including unloading but not loading.

The freight shall be computed on the dead weight of 2,240 lbs. or measurement of 40 cubic feet to the ton at the option of the Commissioners, and shall be subject to the following minimum charges per consignment :—

	s.	d.
Not exceeding 10 cubic feet or 5 cwt.	7	8
Over 10 cubic feet or 5 cwt.	9	0

2. **All goods between other stations and Geelong Pier** shall be charged ¾ of a mile in addition to the mileage to or from Geelong, but the charge shall not be less than the freight between Geelong Pier and Geelong Station shown in Clause 1.

3. (*a*) **GOODS BETWEEN BROUGHAM STREET CROSSING AND STATIONS OTHER THAN GEELONG PIER.**—Mileage shall be computed as to or from Geelong, plus ¾ of a mile. In addition the following shunting charges shall be imposed for the placing at Brougham Street of trucks whether loaded or empty :—

	s.	d.
Per 4-wheeled truck	6	0
Per bogie truck	12	0
Minimum charge	24	0
Maximum charge	48	0

(*b*) **COAL** in truck loads from Geelong Pier to Brougham Street Crossing or to Huddart Parker & Co.'s Coal Gears shall be charged 4s. 7d. per ton.

4. **Local Haulage of Wheat.**

(*a*) Wheat previously railed to stores and stacking sites at Geelong or North Geelong, and subsequently hauled from such locations to piers in the vicinity, shall be charged for such haulage at the rate of 2s. 11d. per ton, subject to the tonnage minimums prescribed for wheat. Smaller quantities to be charged at the rate prescribed for wheat, minimum 3 tons, if cheaper.

(*b*) Wheat rejected at the ship's side and forwarded from the pier to Geelong or North Geelong, or to any sheds or stacking sites thereat, shall be charged 2s. 11d. per ton, subject to the prescribed tonnage minimums. Smaller quantities to be charged at the rate prescribed for wheat, minimum 3 tons, if cheaper.

PORTLAND STATION AND PIER—(Owner's Risk Rates).

The conditions under which goods may be forwarded to or from unattended stations or private sidings at Commissioners' or at Owner's Risk, are set forth in the General Conditions covering the carriage of goods.

1. Trucks used between the Old Pier and Portland Station shall be charged for at the rate of 68s. 2d. per four-wheeled truck per day or part thereof, the time to be reckoned from when the trucks are supplied at Portland Station until their release thereat.

2. Wheat previously railed to stacking sites in the vicinity of Portland, and subsequently hauled from such locations to Portland Pier, shall be charged for such haulage at the rate of 2s. 11d. per ton subject to the tonnage minimums prescribed for wheat.

3. Wheat, other grain and flour for export, consigned from country stations to Portland Pier shall be charged ½ of a mile in addition to the mileage to Portland.

4. Coal from Portland Pier to inland stations shall be charged ½ of a mile in addition to the mileage from Portland.

PIER CHARGES—*continued.*

PORTLAND—*continued.*

5. Cargo, including ship's stores, oil, motor spirit and goods N.O.S., forwarded from or to the ship's side at the Portland New Pier shall be charged the following haulage charges :—

	Per ton s.	d.
(*a*) From ship's side to Portland station for local delivery	9	9
(*b*) From Portland station to ship's side, minimum 3 tons per 4-wheeled truck ...	9	9
(*c*) Between Portland station and ship's side for despatch to or from inland stations (in addition to the tariff rate between Portland station and destination)	4	7

These haulage charges shall be subject to a minimum equivalent to engine detention charge, viz., 30s. 10d. for the first 15 minutes and 9s. 4d. for each subsequent 5 minutes, minimum charge 30s. 10d. for the time the engine is engaged at the Pier, time to be computed from when the engine leaves Portland station until its return thereto.

Portland Wool Appraisement Centre Siding.

1. Freight Charges on all traffic except where otherwise specified shall be computed on the mileage to Portland, viz., 251½ miles from Melbourne.

2. In addition to freight, the following charges shall be imposed for each placing of trucks on the siding :—

	s.	d.
Per 4-wheeled truck	10	3
Per bogie truck	20	6
Minimum charge for each placing	20	6

3. Between the Siding and Portland Station or Portland Pier.

The following charges shall be imposed :—

	s.	d.	
(*a*) Wool ... ... (Double dumped bales double the single bale rate).	3	2	per single bale.

(*b*) Goods other than Wool shall be charged 79s. 2d. per 4-wheeled truck irrespective of weight or class of goods.

(*c*) Cargo shut out of vessels at Pier shall be charged half rate from the ship to the siding and also from the siding to the ship when the cargo is re-shipped.

4. Engine Services—Special Charges for—

When the services of the local engine are required by the siding holder between the hours of 10 p.m. and 7.25 a.m. for the purpose of hauling trucks from the siding to Pier or *vice versa*, the charge for such services shall be at the rate of 61s. 1d. per hour in addition to the charges specified in Clauses 1, 2 and 3.

If a special engine has to be brought from Ararat, Hamilton or Heywood in connexion with traffic to or from the siding, the following charges shall be imposed to cover the cost of bringing the engine from and returning it to the depot from where it was supplied :—

If the Depot from which engine is supplied is—	Special Charge £	s.	d.
Ararat	98	18	8
Hamilton	49	9	4
Heywood	12	7	4

No charge shall be made for the engine while it is at Portland unless it be occupied for more than 12 hours, after which time, the rate for each working hour or part thereof shall be 61s. 1d., in addition to the charges specified in Clauses 1, 2 and 3.

If the special engine be also required for traffic at both Portland Freezing Works Siding and the Portland Appraisement Centre Siding, the special charges shall be borne proportionately by each, in accordance with the time the engine is occupied moving traffic to, from, or on the respective sidings.

PIER CHARGES—*continued.*

Portland Freezing Works Siding (Thos. Borthwick & Sons (Aust.) Pty. Ltd.)

1. Freight Charges on all goods, wool and live stock traffic, except where otherwise specified, shall be computed on the mileage to Portland, viz., 251½ miles from Melbourne.

2. In addition to freight, the following charges shall be imposed for placing all trucks on the siding except where otherwise specified :—

	s.	d.
Per 4-wheeled truck	10	3
Per Bogie truck	20	6
Minimum charge for each placing	20	6

(Live Stock is exempted from these placing charges).

3. Between the Siding and Portland Station or Portland Pier.
The following charges shall be imposed :—

(*a*) Frozen Meat and Produce and Preserved Meats to be consigned at Owner's Risk and loaded and unloaded by owners—

Subject to a minimum of—	Per ton s. d.
8 tons per 12-ton "T" truck	12 3
12 tons per 15-ton "T" truck	

(*b*) Wool ... ... s. d. 3 2 per single bale
(Double dumped bales double the single bale rate)

(*c*) Export Fruit previously carried by rail to Portland Freezing Works for storage when railed from the Works to Portland Pier for shipment shall be charged for the latter journey { 10s. 3d. per ton Minimum— 79s. 2d. per 4-wheeled truck

(*d*) Goods not otherwise specfied shall be charged 79s. 2d. per 4-wheeled truck irrespective of weight or class of goods.

(*e*) Cargo shut out of vessels at pier shall be charged half rates from the ship to the siding and also from the siding to the ship when the cargo is re-shipped.

4. "T" Trucks sent from Melbourne to Portland for the purpose of making special trips with Frozen Produce shall be charged 1s. 5½d. each per mile, one way only, provided that this charge shall not be imposed for three refrigerator trucks loaded with tallow pipes or other goods at the Company's Newport Works for conveyance to Portland.

5. Engine Services.—The Special Charges shown on page 178, clause 4, shall also apply to such engine services for this siding.

WARRNAMBOOL PIER.

1. **Between Warrnambool Pier and Warrnambool Station.**—The charges for the haulage of goods shall be as follows :—

	Per ton s.	d.
Goods N.O.S.	7	4
Coal, loose, owners to load and unload, (subject to the prescribed minimums)	6	11

2. **Warrnambool Pier and Dennington.**—Goods carried between Warrnambool Pier and Dennington shall be charged the mileage rates between Warrnambool Station and Dennington with the following additional charges for carriage between Warrnambool Station and the Pier, viz.:—

	Per ton s.	d.
General cargo	4	7
Coal (subject to the prescribed minimums)	3	6
Sugar	4	7
Milk	4	7

(Owners to load and unload)

3. **Warrnambool Pier and all Stations other than Warrnambool and Dennington.**—Goods carried between Warrnambool Pier and all Stations other than Warrnambool and Dennington shall be charged the mileage rate between Warrnambool Station and such stations, with the following additional charges for carriage between Warrnambool Station and the Pier.

	Per ton s.	d.
Agricultural produce	4	7
General cargo	7	4
Coal and Briquettes (subject to the prescribed minimums)	3	11

(Owners to load and unload)

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CHARGES FOR THE CARRIAGE OF GOODS TO AND FROM THE PORT MELBOURNE AND WILLIAMSTOWN PIERS (Not including loading or unloading at Port Melbourne and Williamstown Piers).

INWARDS CARGOES.

To Montague Shipping Shed from Port Melbourne, Victoria Dock or Williamstown Piers—

	s. d.
General cargo	... 11 11 per ton
Vehicles—two-wheeled	£2 4 2 each
,, four-wheeled	£2 18 7 each

Wheat, Flour, Bran, Pollard and Goods in the Grain or AP class shall be carried at the rates and conditions applicable to those classes, unless otherwise specified.

To Geelong from Port Melbourne or Williamstown Piers—

	£ s. d.
General cargo	1 12 9 per ton

To Graham Goods Station from Port Melbourne Pier—

	s. d.
General cargo	9 5 per ton

To Williamstown Siding or Sheds from Williamstown Pier—

	s. d.
Coal, in full truck loads	... 6 0 per ton
Artificial manures, in full truckloads	... 5 7 ,,

To Other Stations from Port Melbourne or Williamstown Piers—

The Tariff rates and conditions shall be applicable subject to the rates from the Piers to Montague Shipping Shed as a minimum.

OUTWARD CARGOES.

From Montague Shipping Shed to Port Melbourne or Williamstown Piers and from Melbourne Goods Sheds to Williamstown Pier, or Port Melbourne Pier—

	s. d.
General cargo	... 11 11 per ton
Vehicles	... Same as Inwards

Wheat, Flour, Bran, Pollard and Goods in the Grain or AP class from Melbourne to Port Melbourne or Williamstown Piers shall be carried at the rates and conditions applicable to those classes, unless otherwise specified.

Fresh Fruit in truck loads, consigned from Melbourne Goods Depot to the Port Melbourne and Williamstown Piers direct for export, is to be charged under Grain rate, subject to the tonnage minimum prescribed for Fresh Fruit.

Lesser quantities than truck loads are to be charged at the general cargo rate, viz., 11s. 11d. per ton.

From Port Melbourne or Williamstown Sidings and Sheds to the Piers at Port Melbourne and Williamstown respectively—

	s. d.
General cargo	... 9 5 per ton

From Graham Goods Station to Port Melbourne Pier—

	s. d.
Coal, including weighing ; owners to load and unload ...	... 12 3 per ton
Tallow, in truck loads ; owners to load	... 9 9 ,,

From Geelong to Port Melbourne and Williamstown Piers—

	s. d.
General cargo	... 32 9 per ton

CHARGES FOR THE CARRIAGE OF GOODS TO AND FROM THE PORT MELBOURNE AND WILLIAMSTOWN PIERS—*continued.*

From Other Stations to Port Melbourne or Williamstown Piers—

1. The Tariff rates and conditions shall be applicable subject to the rates from Montague Shipping Shed to the Piers as a minimum.

2. Goods in Classes 1 and 2 despatched from stations other than Montague Shipping Shed to Port Melbourne Pier or Williamstown Pier. Subject to Clause 1, when waybilling goods consigned to Port Melbourne Pier or Williamstown Pier, for direct shipment, which are charged for under Class 1 or Class 2, a deduction of 6d. per ton from the tariff rates shall be made and consignments waybilled accordingly.

3. Agricultural Machines, set up or in cases, forwarded from Melbourne or Sunshine to Port Melbourne or Williamstown Piers shall be charged 29s. 11d. for each machine if cheaper than measurement weight at their respective tariff rates.

NOTE.—(i) See also special rates for the carriage of meat (page 156) and wool (page 193).
(ii) The mileage from Spencer Street to Montague Shipping Shed is 3 miles.

GENERAL.

1. The minimum charges which shall be imposed in respect of the traffic to and from the Piers are as under :—

	s.	d.
Not exceeding 10 cubic feet or 5 cwt.	5	9
Over 10 cubic feet or 5 cwt.	6	9

2. Cargo landed in mistake from inwards vessels forwarded to Montague Shipping Shed and subsequently railed back to the Piers for shipment, shall be charged half rate for both journeys.

Cargo previously carried by rail to Piers shut out of outwards vessels, and returned to Montague Shipping Shed to be subsequently railed back to the Piers for shipment, shall be charged half rate for both journeys, provided full rate has been charged on the first journey to the Piers.

3. Any single package which exceeds 2 tons dead weight shall be charged 5s. 2d. per ton in addition to the freight to cover the extra cost of loading or unloading at Montague Shipping Shed.

PORT MELBOURNE PIER.

(*a*) Fruit, fresh or dried, or any other export traffic hauled from a ship on one side of the pier to a ship on the other side shall be charged 6s. 0d. per 4-wheeled truck, subject to a minimum of 12s. 0d. and a maximum of 24s. 0d.

(*b*) Fruit, fresh or dried, or any other export traffic rejected at ship's side and hauled therefrom to the Port Melbourne Yard or Graham Goods Station, shall be charged 12s. 0d. per ton. If the traffic is subsequently returned from the Port Melbourne Yard or Graham Goods Station to the Pier for export, no additional charge shall be imposed for the return journey.

(*c*) Fruit, fresh or dried, or any other export traffic, rejected at ship's side, and left in trucks which have to be specially placed at some point for delivery or repacking shall be charged 12s. 0d. per ton, subject to a minimum charge of 12s. 0d. per consignment.

HAULAGE OF GRAIN, FODDER, ARTIFICIAL MANURE AND TIMBER AT WILLIAMSTOWN PIER.

(*a*) Wheat previously railed to stores and stacking sites at Spotswood and Williamstown Pier, and subsequently hauled from such locations to piers in the vicinity, shall be charged for such haulage at the rate of 2s. 11d. per ton subject to a minimum of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

(*b*) Agricultural produce discharged into the sheds and then manufactured, pressed, or repacked shall be charged 2s. 11d. per ton for haulage from the shed to the pier, subject to a minimum of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

CHARGES FOR THE CARRIAGE OF GOODS TO AND FROM THE PORT MELBOURNE AND WILLIAMSTOWN PIERS—*continued.*

(*c*) Agricultural produce rejected at the ship's side shall be charged 2s. 11d. per ton for haulage from the piers to the shed, subject to a minimum of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

(*d*) Agricultural produce hauled from one shed to another shed shall be charged 2s. 11d. per ton, subject to a minimum of 10 tons 9 cwt. per 11-ton truck and 15 tons 4 cwt. per 16, 22 or 27-ton truck.

(*e*) Artificial manures hauled from the piers to local sheds shall be charged 5s. 5d. per ton.

(*f*) Timber for lining vessels or dunnage purposes forwarded from the local sidings at Williamstown Pier to ship's side shall be charged 4s. 0d. per ton, minimum 8½ tons per 11-ton truck and 14½ tons per 16, 22 or 27-ton truck. Timber (old sleepers) forwarded from one grain site or shed to another grain site or shed at Williamstown Pier shall be charged 2s. 11d. per ton, subject to a minimum of 8½ tons per 11-ton truck and 14½ tons per 16, 22 or 27-ton truck.

The rates shown in clauses (*a*) to (*f*) are the Owner's Risk rates. The conditions under which goods may be forwarded at Commissioners' Risk from or to unattended stations, platforms, or sidings, are set forth in the General Conditions covering the carriage of goods.

ARTIFICIAL MANURES.

Artificial manures forwarded from Williamstown Pier or Port Melbourne Pier shall be charged the tariff rates. When the covering of this traffic is performed by the Department in addition to the usual charge of 10s. 7d. for the use of each tarpaulin, a charge of 2s. 1d. per tarpaulin shall be imposed.

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Delivery of Chaff and other Agriculture Produce at Melbourne.

1. A terminal charge of 3s. 2d. per truck shall be imposed in respect of all chaff, potatoes, onions and all other agricultural produce railed to Melbourne, for delivery at the Cowper Street chaff or potato platforms, in addition to the unloading charge and shall be shown separately on waybills by waybilling stations.

2. Chaff and other agricultural produce consigned to Melbourne (Cowper Street) from any outside place shall be deemed as accepted for delivery at such one of the sidings at Spencer Street as the proper officer may determine on the arrival of the truck or trucks containing the same.

3. Chaff and other agricultural produce consigned to Melbourne shall, except as hereinafter provided, be taken delivery of direct from the truck into road vehicle, whether such truck be placed for delivery at a siding alongside a platform or at a siding unprovided with a platform, but provided with an approach road. In the case of a truck placed for delivery at a siding inside the shed known as the chaff shed at Cowper Street, the chaff or other agricultural produce shall be unloaded and discharged from such truck on to the floor of the said shed, and such unloading and discharging shall be performed by the Commissioners and paid for by the consignee at the prescribed rates, but in any such case the chaff or other agricultural produce so unloaded or discharged must be removed from the railway premises not later than 5 p.m. on the working day next following the day upon which the same was unloaded or discharged, and failing removal of such chaff or other agricultural produce as aforesaid a storage charge of 4s. 0d. per ton or part of a ton per day—or part of a day—shall be imposed and levied in respect of so much of the said chaff as remains on railway premises after the time specified herein for removal.

4. Except as hereinbefore provided, the unloading and discharge of chaff or other agricultural produce from any truck placed in any siding for the purpose of delivery shall be performed by and at the expense and risk of the consignee within the period allowed for unloading after placing specified in General Condition No. 28.

5. The demurrage charges and conditions specified in General Condition No. 28 shall be imposed and levied in respect of any truck containing chaff or other agricultural produce which is detained at Spencer Street, for unloading or discharging purposes beyond the time therein specified. This provision shall not apply in any case where any truck containing chaff or other agricultural produce is placed for delivery at a siding inside the Cowper Street chaff shed and unloaded and discharged on to the floor of such shed.

6. The Commissioners will not, except as prescribed in clause 2 thereof, provide room or space on railway premises for the storage of chaff or other agricultural produce. The consignee shall, except as hereinbefore provided, forthwith upon unloading or discharging chaff from any truck placed in any siding for delivery remove from the railway premises the chaff so unloaded or discharged.

7. The foregoing conditions shall be and be deemed to be incorporated in any contract made or entered into for the carriage by the Commissioners of any chaff or other agricultural produce, and all such consignments shall be received *inter alia* under and subject to such conditions.

8. The words " proper officer " shall mean and include the Metropolitan Goods Superintendent of the Commissioners for the time being, or such other officer or class of officer as the Commissioners may from time to time appoint in that behalf, and any other officer of the Commissioners duly authorised to place trucks for unloading or discharge.

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MELBOURNE.—Haulage Charges at Sidings in the vicinity of and at Victoria Dock.

The charges for the haulage of goods at Owner's Risk between Melbourne and (1) Piggott-street Sidings, (2) Straw Siding or open roads on Cowper-street Siding, (3) Victoria Dock, (4) Harbor Trust Crane Siding, shall be as follows :—

Siding.	Haulage Charge (in addition to the Tariff Rate to or from Melbourne except where marked † which indicates the total rate to be charged).	For Traffic.
	Per ton. s. d.	
(1) **Piggot-street Sidings—**		
Open for inwards or outwards consignments in truck loads, minimum 3 tons, for the following companies only :—		
(a) Haughton and Co. (for traffic to store only)	3 4	Inwards Goods, n.o.s.
Melbourne Harbour Trust ...	0 3½ (per bale)	Wool
Melbourne Hydraulic Power Co.	1 10	Coal from Victorian Coal-fields
H. Deville	4 0	Outwards
Wool Pressing and Contracting Co. Pty Ltd.	... ...	
(b) Victorian Stevedoring and General Contracting Co. Pty. Ltd.	0 3½ (per bale)	Wool, Inwards
Victorian Stevedoring and General Contracting Co. Pty. Ltd. account Crompton and Son (Victoria)	6 0	Goods, N.O.S. Inwards and Outwards
(c) Gas and Fuel Corporation of Victoria	2 1	Inwards and Outwards
Gas and Fuel Corporation of Victoria Siding Extension account	Nil	Coal to stations under 10 miles from Spencer or Flinders Streets
Commonwealth Shipping Board	1 10	Coal to stations over 10 miles from Spencer or Flinders Streets
(d) Victorian Butter Factories Co. ...	6 0	Inwards timber and straw
	†6 3	,, coal from Victoria Dock
	1 10	,, ,, from Victorian coal-fields
	†6 0 minimum, 40 2 per special trip	Outwards new empty boxes in truck loads to the Victoria Dock
	6 0	,, all other consignments

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Melbourne.—Haulage Charges, etc.—continued.

Siding.	Haulage Charge (in addition to the Tariff Rate to or from Melbourne except where marked † which indicates the total rate to be charged).	For Traffic.
	Per ton. s. d.	
(2) **Straw Siding and Open Roads on Cowper-street Siding**	...	Open for the receipt of truck loads of— (*a*) Certain descriptions of goods, the unloading of which, by the consignee may be permitted— Fresh Fruit in unlidded cases, Cement, or Potash Salts, Salt, (unrefined bulk,) which shall be placed without charge (*b*) Casein, skim milk powder, Lactogen, Charcoal, Fruit Pulp, Granose, Fruit—canned, dried, or fresh, n.o.s., Flour, Bran, Pollard, Malt Jam, Tomato Sauce, Molasses, Salt, n.o.s., which shall be charged 6s. per 4-wheeled truck and 12s. per bogie truck for placing (*c*) Other consignments shall not be placed without the authority of the Chief Traffic Manager, or the Chief Commercial Manager
(2A) Melbourne Harbour Trust Siding account Potato Marketing Board (Victoria)	4 9	The siding is situated adjacent to the Government Cool Store and is open for inwards and outwards traffic in truckloads

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Melbourne.—Haulage Charges, etc.—continued.

Siding.	Haulage Charge (in addition to the Tariff Rate to or from Melbourne except where marked † which indicates the total rate to be charged).	For Traffic.
	Per ton. s. d.	
(3) **Victoria Dock—**		**Inwards Traffic—**
Open for Inwards or Outwards consignments in truck loads to or from vessels lying at berths accessible by rail	0 6½	Grain, hay, straw, flour, bran, pollard, compressed fodder and chaff
	2 1	Wool and all other goods unless otherwise specified
	1 10	Coal from Victorian coal-fields
Consignments of less than 3 tons may be accepted for Victoria Dock. The freight on such consignments shall be computed on the actual weight to Melbourne, plus a minimum of 3 tons at the prescribed haulage charge to Victoria Dock	†4 0 minimum, 40 2 if special trip is required	Inwards wool or grain from Grain Shed (not including loading or unloading)
		When way-billing goods in classes 1 and 2 from stations other than Melbourne to the Victoria Dock for direct shipment, a deduction of 6d. per ton from the tariff rates (which will not include unloading) shall be made and so way-billed.
		Special rates are applicable for certain traffic, such as Butter, Poultry, Rabbits, Fruit, and Meat, forwarded to Victoria Dock from certain stations, see pages 188 and 189
		Outwards Traffic—
	†6 0	Coke and grain in truck loads to Spencer-street
	†6 3	Coal in truck loads to Spencer-street
	Nil	Coal to stations under 10 miles from Spencer-street or Flinders-street
	2 1	Coke to stations under 10 miles from Spencer-street or Flinders-street
	1 10	Coal to stations over 10 miles from Spencer-street or Flinders-street
	2 1	Scrap Iron
	1 10	Bitumen, Manures
	3 2	Softwood timber cut to sizes for case-making, in shooks
	4 0	All other goods,
		Traffic hauled from one part to another part of the wharf at Victoria Dock shall be charged 6s. per 4-wheeled truck, subject to a minimum of 12s. and a maximum of 24s.

Melbourne.—Haulage Charges, etc.—continued.

Siding.	Haulage Charge (in addition to the Tariff Rate to or from Melbourne except where marked † which indicates the total rate to be charged).	For Traffic.
	Per ton. *s. d.*	
(4) **Melbourne Harbor Trust Crane Siding,** situated on the Wharf-road–Piggott-street extension, between the Victoria Dock and the River Wharf	...	The placing of loaded trucks and removal of empties from the Crane necessitates the service of a special engine and the following charges shall be imposed in respect of traffic in truck loads to or from the Melbourne Harbor Trust Crane Siding :— Freight charges shall be computed at Classification rates to or from Melbourne plus the following additional charges between Melbourne and the Crane Siding, viz :— Haulage—8s. 9d. per ton Special Train Service—£4 12s. 9d. for each trip either to place or remove trucks Supervision—51s. for each trip when delivery or acceptance of traffic is involved Detention—30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes, minimum charge 30s. 10d. shall be imposed for detention to engine when utilized for such work as the movement of trucks at the Crane Siding in connexion with the discharging of heavy consignments of machinery The haulage charge of 8s. 9d. per ton must be added to the freight charges on the way-bills to and from Melbourne Goods Superintendent, Melbourne, shall raise debit in Miscellaneous Book for the special train service and supervision when a delivery or acceptance of traffic is involved

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HAULAGE CHARGES FOR BACON, BUTTER, CHEESE, EGGS, HONEY, MEAT, FRUIT, POULTRY, RABBITS AND HARES FOR EXPORT.

All at Owner's Risk. Loading or unloading to be done by or at the expense of owners.

Commodity	From—	To—	Charge
(*a*) Rabbits and Hares in truck loads, Bacon, Butter, Cheese, Eggs, Honey, Meat, and Fruit for export	Country stations ...	Vic. Butter Factories Co.'s Sdg. Govt. Cool Stores at Vic. Dock	per ton s. d. 4 10, 4 10 — in addition to tariff rate to Melbourne
(*b*) Rabbits, Hares, Poultry, Eggs and Honey, small lots	,, ,, ...	Govt. Cool Stores at Vic. Dock	per package 3 2 — in addition to tariff rate to Melbourne
(*c*) Bacon and Butter (frozen), Cheese, Eggs, Honey and Fruit	Govt. Cool Stores at Vic. Dock, Vic. Butter Factories, Siding, between Melb. Goods Depot (in insulated trucks) and	Port Melbourne or Williamstown Piers	per ton 11 11 Min. charge, 48s. 2d. per 4-wheeled truck
(*d*) Hares, Poultry and Rabbits (frozen)	Govt. Cool Stores at Vic. Dock, Vic. Butter Factories' Siding, Melbourne Goods Depot, Angliss Siding, Footscray, Newport Freezing Works	Port Melbourne or Williamstown Piers	Per crate not exceeding cubic feet — Hares and Poultry / Rabbits: 3½ — 1s. 5d. / 1s. 0d.; 7 — 2s. 1d. / 1s. 6d.; 9 — 2s. 4d. / 2s. 1d. Min. charge per truck, 48s. 2d. Per "T" Truck Capacity Tons — Maximum Charge £ s. d.: 12 — 6 11 5; 15 — 8 6 3
(*e*) Mixed consignments of Bacon, Butter, Rabbits, Hares, Poultry and Meat (frozen)	Govt. Cool Stores at Vic. Dock, Vic. Butter Factories' Siding, Melbourne Goods Depot	Port Melbourne or Williamstown Piers	Bacon, Butter and Meat at rates and minimum shown in clause (*c*) Hares, Rabbits and Poultry at rates and minimum shown in clause (*d*)
(*f*) Rabbits (frozen) ...	Mitcham	Port Melbourne Pier	£7 3s. 11d. per 12-ton truck
	Borthwick's Siding (South Brooklyn) ...	Williamstown Pier ...	£9 2s. 9d. per 15-ton truck
		Victoria Dock ...	
(*g*) Rabbits (frozen) ...	Vic. Butter Factories' Siding	Victoria Dock ...	11s. 11d. per ton, minimum charge, £2 8s. 2d. per 4-wheeled truck. Per "T" Truck Capacity Tons — Maximum Charges £ s. d.: 12 — 6 11 5; 15 — 8 6 3

HAULAGE CHARGES FOR BACON, BUTTER, CHEESE, EGGS, HONEY, MEAT, FRUIT, POULTRY, RABBITS AND HARES FOR EXPORT—*continued.*

Commodity	From—	To—	Charge
(*h*) Butter and Rabbits (frozen) Cheese and Fresh Fruit	Govt. Cool Stores at Vic. Dock	Victoria Dock ...	10s. 7d. per ton, minimum charge £2 8s. 2d. per 4-wheeled truck

(*i*) Produce shut out of vessels at the Piers—

Hares and rabbits will be carried back to Melbourne at half of the above rates.

Butter, fruit and frozen produce other than hares, rabbits and meat will be carried back to Melbourne at the rate of 12s. 3d. per ton.

In each case the consignments will be forwarded back to the Pier for export free.

(*j*) *The freight must be prepaid in every instance, except on consignments to the Victorian Butter* Factories Company's Siding, which may be waybilled to pay.

(*k*) Consignments of butter, cheese, honey, poultry and game for export will not be accepted *at country stations for carriage direct to any vessels at the Pier except by special arrange*ment with the Chief Traffic Manager.

(*l*) For charges in respect of frozen meat, also special trains, see pages 156 and 157.

MILEAGE RATES PER BALE FOR WOOL.

The rates shown hereunder are for carriage at Owner's Risk and include loading and unloading unless otherwise specified and are subject to a mimimum charge of 5/1d. per consignment.

For carriage at Commissioners Risk add 5 per cent. mimimum additional charge 3/4d. per consignment.

When the loading of wool is performed by consignors at stations in charge of Caretakers or at No-one-in-charge stations, the following allowances for loading by consignors shall be deducted from the freight charges :—

Per bale	6d.
„ fadge	2d.
„ bag	1d.

Miles.	Rate per Bale.	Miles.	Rate per Bale.	Miles.	Rate per Bale.	Miles.	Rate per Bale.
	s. d.		s. d.		s. d.		s. d.
1–10	2 1	61– 62	9 11	112–114	17 1	214–217	23 9
11–12	2 2	63	10 1	115–116	17 2	218–221	24 0
13	2 6	64	10 3	117	17 5	222–223	24 1
14	2 10	65	10 6	118–120	17 6	224–227	24 2
15	2 11	66– 67	10 8	121–122	17 7	228–232	24 5
16	3 0	68– 69	10 11	123–124	17 10	233–235	24 6
17– 18	3 3	70	11 1	125	17 11	236–239	24 8
19	3 4	71	11 3	126–128	18 2	240–244	24 11
20– 21	3 8	72	11 4	129	18 3	245–247	25 1
22	3 9	73	11 7	130	18 7	248–251	25 3
23	4 1	74	11 8	131–133	18 8	252–256	25 5
24	4 3	75	11 9	134	18 10	257–260	25 6
25– 26	4 5	76	12 4	135–136	19 0	261–264	25 8
27	4 6	77– 78	12 5	137–139	19 1	265–269	25 10
28	4 10	79	12 6	140–141	19 3	270–273	25 11
29	5 0	80	12 9	142–144	19 7	274–277	26 2
30	5 1	81	12 10	145	19 8	278–282	26 3
31	5 3	82	13 0	146–148	19 10	283–286	26 7
32	5 5	83	13 2	149	20 1	287–290	26 8
33	5 9	84– 85	13 3	150	20 4	291–294	26 11
34– 35	5 10	86	13 5	151–154	20 5	295–299	27 0
36	5 11	87	13 9	155–158	20 6	300–304	27 1
37– 38	6 3	88	13 10	159–160	20 9	305–309	27 4
39	6 6	89– 90	14 0	161–162	20 10	310–316	27 5
40	6 7	91– 92	14 6	163–166	21 1	317–321	27 7
41– 42	6 8	93	14 7	167–168	21 2	322–328	27 9
43	7 0	94	14 8	169–170	21 3	329–333	27 10
44– 45	7 5	95	14 11	171–174	21 6	334–337	28 2
46	7 7	96	15 0	175–178	21 11	338–341	28 4
47	7 9	97	15 4	179–182	22 0	342–346	28 5
48	7 11	98	15 5	183–184	22 2	347–350	28 7
49– 50	8 0	99	15 8	185–188	22 4	351–357	28 9
51– 52	8 4	100	15 9	189–190	22 6	358–362	28 10
53	8 5	101–102	15 11	191–192	22 7	363–369	29 1
54	8 8	103–104	16 1	193–196	22 9	370–375	29 2
55– 56	9 1	105–106	16 2	197–201	22 11	376–382	29 3
57	9 2	107–108	16 4	202–203	23 0	383–391	29 6
58	9 5	109	16 6	204–207	23 4	392–400	29 10
59	9 6	110	16 8	208–211	23 5	401–408	29 11
60	9 7	111	16 11	212–213	23 8	409–410	30 0

Double dumped bales shall be charged double the single bale rate.

Fadges—Two fadges shall be charged as one bale.

Bags—Not exceeding chaff bag size shall be charged ⅛th of the bale rate.

Bags—Exceeding chaff bag size shall be charged the bale rate.

SPECIAL RATES FOR THE CARRIAGE OF WOOL.

WOOL FORWARDED FROM THE FOLLOWING STATIONS SHALL BE CHARGED AT THE UNDERMENTIONED RATES :—

1. Between the following stations and destinations shown.

Between—	And—	Per Bale. s. d.
Goroke	Melbourne, Kensington, South Kensington, Newmarket, West Footscray, Macaulay ...	18 10
Gymbowen	" " " " ...	18 10
Mortat	" " " " ...	19 3
Carpolac	" " " " ...	19 5
Goroke	Geelong or Portland	17 6
Gymbowen	" "	17 6
Mortat	" "	17 10
Carpolac	" "	18 2
Geelong, North Geelong or South Geelong	Melbourne, Victoria Park, Port Melbourne, Williamstown Pier, Macaulay, Victoria Dock, Kensington, South Kensington, Newmarket, Burnley, Coburg, West Footscray, North Williamstown, Jewell, Spotswood, Footscray, Brunswick :—	
		s. d.
	Per Bale	3 8
	Per Fadge	1 7
	Per Bag	0 10

Double dumped bales are to be charged double the single bale rate.

2. To Victoria Dock from stations other than Melbourne or Geelong—3½d. per bale in addition to the mileage rates to Melbourne.

3. From the Darling River District.—Wool forwarded from the Darling River District via Echuca, Kerang, Swan Hill, Mildura, Merbein, Yelta or Robinvale, shall be charged the following rates from the stations specified.

The brands of the bales and the name of the holding from which they are forwarded shall be entered on the consignment notes, and such other information shall be supplied as may be required by the Commissioners as evidence that such wool has been duly forwarded from the Darling River District :—

Echuca.		Kerang.		Swan Hill.		Mildura, Merbein or Yelta.		Robinvale.	
To Melbourne, Kensington, Sth. Kensington, Newmarket, West Footscray and Macaulay.	To G'long.	To Melbourne, Kensington, Sth. Kensington, Newmarket, West Footscray and Macaulay.	To G'long.	To Melbourne, Kensington, Sth. Kensington, Newmarket, West Footscray and Macaulay.	To G'long.	To Melbourne, Kensington, Sth. Kensington, Newmarket, West Footscray and Macaulay.	To G'long.	To Melbourne, Kensington, Sth. Kensington, Newmarket, West Footscray and Macaulay.	To G'long.
Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.	Per bale s. d.
11 8	13 2	14 0	15 5	16 4	17 7	18 10	18 10	18 2	18 2

SPECIAL WOOL RATES—(*continued*)

4. WOOL FROM CERTAIN NEW SOUTH WALES STATIONS.

Wool forwarded from the undermentioned New South Wales stations to Melbourne, Kensington, South Kensington, Newmarket, Macaulay or West Footscray via Tocumwal shall be charged the New South Wales tariff rate per bale to Tocumwal plus the special Victorian rate per bale from Tocumwal to destination as shown hereunder.

New South Wales Stations	New South Wales		Victoria
	Miles	per bale	per bale
		s. d.	s. d.
Broken Hill	517	49 0	10 8
Menindie	443	47 0	11 9
Gum Lake	395	46 0	13 0
Darnick	365	45 0	13 9
Beilpajah	355	45 0	11 7
Ivanhoe	325	43 0	12 4
Conoble	302	41 0	12 10
Trida	284	41 0	13 3
Wee Elwah	269	39 0	13 10
Roto	257	39 0	14 3
Lowlands	247	39 0	14 7
Hillston	226	37 0	15 5
Dirrung	219	35 0	15 8
Langtree	214	35 0	15 11
Goorawin	208	34 0	16 1
Merriwagga	202	34 0	16 4
Budawong	196	33 0	16 8
Goolgowi	190	32 0	16 11
Corolgi	183	32 0	17 5
Tabbita	178	31 0	17 6
Warburn	171	31 0	17 11
Tharbogang	164	29 0	18 3
Griffith	160	29 0	18 8
Yoogali East	155	29 0	18 10
Widgelli	153	29 0	19 0
Wumbulgal	149	28 0	19 3
Neeambah	146	28 0	19 5
Murrami	141	27 0	19 8
Hay	219	35 0	15 8
Illilliwa	209	34 0	16 1
Nulabor	203	34 0	16 4
Beabula	201	34 0	16 4
Uardry	191	33 0	16 11
Carrathool	184	32 0	17 2
Groongal	176	31 0	17 7
Bringagee	168	30 0	18 2
Benerembah	162	29 0	18 7
Willbriggie	150	28 0	19 1
Whitton	140	26 0	19 8

TRANSHIPPING CHARGES.

	New South Wales	Victoria
	s. d.	s. d.
Per bale ...	0 4	0 5
Per bag ...	0 1	0 2

SPECIAL WOOL RATES—(*continued*)

5. Between Melbourne, Williamstown Pier, Port Melbourne Pier and Victoria Dock, and to Corio Quay.

	Per Bale. s.	d.
(*a*) Melbourne, Macaulay, West Footscray, South Kensington, Kensington and Newmarket to Williamstown Pier or Port Melbourne Pier and vice versa	3	3
(*b*) South Kensington, Macaulay, West Footscray, Kensington and Newmarket to Victoria Dock	3	3
(*c*) Port Melbourne Pier to Williamstown Pier, or vice versa ...	4	8
(*d*) Port Melbourne Pier or Williamstown Pier to Victoria Dock or vice versa	4	8
(*e*) Geelong—Brougham Street Crossing, Dennys Lascelles and Dalgety and Co'.s Sidings (plus shunting charges thereat) to Corio Quay ...	3	3

Double dumped bales are to be charged double the single bale rate.

Wool may be consigned direct from country stations to the Williamstown or Port Melbourne Piers, provided that the name of the vessel it is intended for is shown on the consignment note.

If a Special Train be engaged to carry wool from Melbourne to the Port Melbourne or Williamstown Piers, a minimum charge of £11.1.10 will be imposed in addition to the ordinary freight charge.

6. To Wool-washing Establishments in Victoria.

Wool and sheep skins forwarded from Melbourne, Geelong, or any country station to Country Wool-washing Establishments in Victoria (including Albury) to be scoured or treated, and so declared on the consignment note, shall be charged at the following classifications :—

	Class.
Minimum 6 tons per 4-wheeled truck ...	A less 28½ per cent.
Smaller lots	B less 28½ per cent.

The weight of wool shall be computed on the basis of 7½ bales per ton.

This Concession Rate shall also be charged on Scoured Wool forwarded to Country Woollen Mills, which have furnished the Chief Commercial Manager with a statutory declaration that such scoured wool will be used for the manufacture of woollen or worsted textiles.

7. General

(*a*) The rate per bale, fadge or bag shall be subject to a minimum charge of 5/1d. per consignment.

(*b*) Except where otherwise provided, wool placed in the Commissioners' sheds for storage, will be charged for at the rate of 7d. per bale per day until the wool is removed by the consignee, or sent to store by the Commissioners at the consignee's cost and risk, and if so sent to store by the Commissioners the consignee shall pay such storage and other charges as may be in force.

(*c*) When rates less than the mileage rates are charged between any other station and Melbourne, the application of such rates in respect of suburban stations within 20 miles of Melbourne, shall be as shown in clause 3 of District Rates.

(*d*) Unless otherwise expressly provided, the mileage rates shall apply.

(*e*) For conditions under which goods may be forwarded at Commissioners' Risk from or to unattended stations, platforms or sidings, stations in charge of caretakers or private sidings, see General Conditions Nos. 4 and 5.

(*f*) Double dumped bales shall be charged double the single bale rate.

WOOL RATES.

Rates per bale from principal forwarding stations to Melbourne, Kensington, South Kensington, Newmarket, West Footscray, Macaulay and Geelong.

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Addington	92¼	14 7	73	11 7
Alberton	133½	18 10	178½	22 0
Albury	190½	22 7	233½	24 6
Alexandra	102	15 11	145	19 8
Allendale	90¼	14 6	71	11 3
Amphitheatre	135¼	19 0	120¼	17 7
Antwerp	235¾	24 8	216½	23 9
Ararat	131	18 8	111¾	17 1
Arcadia	98	15 5	141	19 3
Argyle	71¼	11 4	114¼	17 2
Armytage	76¾	12 5	31¾	5 5
Avenel	72	11 4	115	17 2
Avoca	127	18 2	112	17 1
Avonmore	124¾	17 11	162¾	21 1
Axedale	89¼	14 0	132¼	18 8
Bacchus Marsh	31¾	5 5	69¾	11 1
Baddaginnie	113¾	17 1	156¾	20 6
Bairnsdale	171¼	21 6	216¼	23 9
Ballan	49½	8 0	70¼	11 3
Ballarat	73¾	11 8	54½	9 1
Balmattum	98¾	15 8	141¾	19 7
Balmoral	238½	24 8	204¼	23 4
Balranald	272	25 11	315	27 5
Bannockburn	55¾	9 1	13¾	2 10
Banyena	194½	22 9	175¼	21 11
Barfold	72	11 4	110	16 8
Barnawartha	174	21 6	217	23 9
Barnes	152¼	20 5	195¼	22 9
Barrakee	168¼	21 3	172¾	21 6
Barraport	177	21 11	181½	22 0
Barwon	98¾	15 8	53¾	8 8
Bealiba	137¾	19 1	122¾	17 10
Beaufort	102½	16 1	83¼	13 3
Beetoomba	245¼	25 1	288¼	26 8
Bena	66	10 8	111	16 11
Benalla	121¼	17 7	164¼	21 1
Benarca	158¼	20 9	201¼	23 0
Bendigo	100¾	15 11	138¾	19 1
Ben Nevis	144	19 7	124¾	17 11
Berambong	237¾	24 8	280½	26 3
Berriwillock	225¾	24 2	230¼	24 5
Berrybank	94½	14 11	52½	8 5
*Berwick	28	4 10	73	11 7
Bet Bet	121	17 7	106	16 2
Beulah	238¾	24 8	219½	24 0
Beveridge	26	4 5	69	10 11

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Birchip	214¾	23 9	199¼	22 11
Birregurra	83½	13 3	38½	6 6
Bittern	43	7 0	88	13 10
Blowhard	85	13 3	65¾	10 8
Bolinda	35	5 10	73	11 7
Bonnie Doon	118	17 6	161	20 10
Boolarra	102	15 11	147	19 10
Boorcan	131¼	18 8	86¼	13 9
Boort	169	21 3	173¼	21 6
Borung	156	20 6	160½	20 10
Bowman	169	21 3	212	23 8
Branxholme	213¼	23 9	179	22 0
Bridgewater	126½	18 2	140	19 3
Bright	196	22 9	239	24 8
Brim	229	24 5	209¼	23 5
Broadford	46¾	7 9	89¼	14 0
Bruthen	190¾	22 7	235¼	24 8
Buangor	116½	17 5	97¼	15 5
Buckley	64½	10 6	19½	3 8
Buckrabanyule	165	21 1	169½	21 3
Bullioh	217	23 9	260	25 6
Buln Buln	67¾	10 11	112¾	17 1
Bumberrah	183½	22 2	228½	24 5
Bung Bong	119½	17 6	104½	16 2
Bunnaloo	173¼	21 6	216¼	23 9
Burraboi	204½	23 4	247½	25 3
Burrumbeet	86¾	13 9	67½	10 11
Caldermeade	46¼	7 9	91¼	14 6
Caldwell	184¾	22 4	227¾	24 5
Camperdown	123¼	17 10	78¼	12 6
Carapooee	152½	20 5	137½	19 1
Carisbrook	107½	16 4	101½	15 11
Carpolac	254½	19 5	235¼	18 2
Casterton	245¼	25 1	211	23 5
Castlemaine	78	12 5	116	17 2
Cathkin	93¼	14 8	136¼	19 1
Cavendish	213¼	23 9	179	22 0
Charlton	173¾	21 6	178¼	22 0
Cheviot	83¼	13 3	126¼	18 2
Clunes	96¼	15 4	77½	12 5
Clyde	31¾	5 5	76¼	12 5
Cobram	155¼	20 6	198¼	22 11
Cohuna	185½	22 4	223½	24 2
Colac	95¼	15 0	50¼	8 4
Colbinabbin	117½	17 6	160½	20 10
Coleraine	221¼	24 1	187	22 4
Colquhoun	199¼	22 11	244¼	25 1
Condah	221¼	24 1	187	22 4
Cope Cope	175	21 11	160	20 9
Coromby	190¾	22 7	171½	21 6
Cosgrove	126¾	18 2	169¾	21 3

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WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Cowwarr	114¾	17 2	159¾	20 9
Craigieburn	16¼	3 3	59¼	9 7
Cranbourne	28¼	5 0	73¼	11 8
Cressy	85½	13 5	43½	7 5
Creswick	85½	13 5	66¼	10 8
Cudgewa	255	25 5	298	27 0
... ...				
Culgoa	217	23 9	221½	24 1
Curyo	227½	24 5	212½	23 8
Dandenong	19½	3 8	64½	10 6
Darbyshire	224¾	24 2	267¾	25 10
Darnum	66½	10 8	111½	17 1
Dartmoor	261¼	25 8	227	24 2
Daylesford	75½	12 4	91¼	14 6
Dean Marsh	90¾	14 6	45¾	7 7
Deniliquin	190¼	22 7	233¼	24 6
Derrinal	78¼	12 6	121¼	17 7
Derrinallum	109½	16 8	67½	10 11
Detpa	255½	25 5	236¼	24 8
Devenish	137¾	19 1	180¾	22 0
Dhuragoon	221	24 0	264	25 8
Diapur	259¼	25 6	240	24 11
Diggers Rest	20¼	3 8	58¼	9 6
Dimboola	224¾	24 2	205½	23 4
Dingee	131¼	18 8	169¼	21 3
Dobie	125½	18 2	106¼	16 4
Donald	182½	22 2	167½	21 2
Donnybrook	20¾	3 8	63¾	10 3
Dookie	130	18 7	173	21 6
Doroq	66¾	10 8	24¾	4 5
Drouin	57½	9 5	102½	16 1
Drysdale	57½	9 5	12½	2 6
Dumosa	200¼	22 11	204¾	23 4
Dunkeld	178½	22 0	144¼	19 8
Dunolly	125¾	18 2	110¾	16 11
Ebden	195¾	22 9	238¾	24 8
Echuca	145	19 8	188	22 4
Echuca Wharf	146	19 10	189	22 6
Elaine	76¼	12 5	34½	5 10
Elmhurst...	144¾	19 8	129¾	18 7
Elmore	128½	18 3	166½	21 2
Elphinstone	70¼	11 3	108¼	16 6
Emu	145¼	19 10	130¼	18 8
Englefield	233	24 6	198¾	22 11
Epping	14	2 10	57½	9 5
Euroa	93¾	14 8	136¾	19 1
Everton	161	20 10	204	23 4
Fairley	184¼	22 4	222¼	24 1
Fernbank	153½	20 5	198½	22 11
Fish Creek	100½	15 11	145½	19 10

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Flynn	107	16 4	152	20 5
Foster	108¼	16 6	153¼	20 5
Garvoc	144½	19 8	99½	15 9
Gatum	223¾	24 2	189½	22 6
Geelong	45	See page 191		
Gelliondale	130¼	18 8	175¼	21 11
Gerang Gerung	235¼	24 8	216	23 9
Gheringhap	51¾	8 4	9¾	2 1
Gisborne	40	6 7	78	12 5
Glengarry	104½	16 2	149½	20 4
Glenloth	183	22 2	187½	22 4
Glenorchy	162½	21 1	143¼	19 7
Glenrowan	136	19 0	179	22 0
Glen Thompson	167¼	21 2	133	18 8
Goorambat	131¾	18 8	174¾	21 11
Goornong	118	17 6	156	20 6
Gordon	56¾	9 2	63½	10 3
Goroke	245½	18 10	226¼	17 6
Grassdale	222¾	24 1	188½	22 6
Great Western	141¾	19 7	122½	17 10
Gredgwin	180¾	22 0	185¼	22 4
Greenwald	254	25 5	219¾	24 0
Gulpa	177¼	21 11	220¼	24 0
Gunbower	172	21 6	210	23 5
Gymbowen	238¾	18 10	219½	17 6
Haddon	84¼	13 3	65	10 6
Hamilton	197½	22 11	163¼	21 1
Hawkesdale	189½	22 6	144½	19 8
Healesville	40	6 7	85	13 3
Heathcote	73½	11 8	116½	17 5
Henty	236¼	24 8	202	23 0
Heyfield	121½	17 7	166½	21 2
Heywood	235¼	24 8	201	22 11
High Camp	51½	8 4	94½	14 11
Hillside	164	21 1	209	23 5
Homewood	74¼	11 9	117¼	17 6
Hopetoun	254½	25 5	235¼	24 8
Horsham	203¼	23 4	184	22 2
Hunter	135½	19 0	173½	21 6
Huon	201	22 11	244	24 11
Impimi	259	25 6	302	27 1
Inglewood	131	18 8	135½	19 0
Ingliston	45	7 5	75¼	12 4
Inverleigh	62¼	10 1	20½	3 8
Irrewarra	91¾	14 6	46¾	7 9
Irymple	347¼	28 7	332¼	27 10
Jallumba	230½	24 5	211¼	23 8
Jeffries	240½	24 11	217¼	24 0
Jeparit	247¾	25 3	228½	24 5
Jimaringle	212	23 8	255	25 5
Joyce's Creek	95¼	15 0	113¾	17 1

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WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Jung	192	22 7	172¼	21 6
Kanagulk	245¼	25 1	212¼	23 8
Kanawalla	203	23 0	168¾	21 3
Kaniva	272½	25 11	253¼	25 5
Kanumbra	101½	15 11	144½	19 8
Katamatite	147	19 10	190	22 6
Katunga	140¼	19 3	183¼	22 2
Kerang	179½	22 0	217½	24 0
Kerrisdale	68¼	10 11	111¼	17 1
Kiata	240	24 11	220¾	24 0
Kilmore	42½	7 0	85½	13 5
Kilmany	120½	17 7	165½	21 1
Kingston	92¼	14 7	73	11 7
Kinnabulla	224½	24 2	209½	23 5
Knowsley	83	13 2	126	18 2
Koetong	229	24 5	272	25 11
Korlella	97½	15 5	140½	19 3
Koroit	175¼	21 11	130½	18 8
Korong Vale	151	20 5	155½	20 6
Korumburra	69¾	11 1	114¾	17 2
Koyuga	135	19 0	178	21 11
Kyabram	124	17 10	167	21 2
Kyneton	57	9 2	95	14 11
Kyup	209	23 5	174¾	21 11
Lake Boga	205	23 4	243	24 11
Lake Charm	190¼	22 7	228¼	24 5
Lalbert	205¾	23 4	210¼	23 5
Lal Lal	78½	12 6	41¾	6 8
Lancefield	46	7 7	84	13 3
Lang Lang	48¼	8 0	93¼	14 8
Langi Logan	136¾	19 1	117½	17 6
Langwarrin	30¾	5 3	75¾	12 4
Lara	35¾	5 11	9¼	2 1
Larpent	100¼	15 11	55¼	9 1
Leitchville	177	21 11	215	23 9
Leongatha	79	12 6	124	17 10
Lethbridge	61¾	9 11	19¾	3 8
Lillimur	278¾	26 3	259½	25 6
Lindenow	160¼	20 10	205¼	23 4
Linton	98¾	15 8	74	11 8
Lismore	103¼	16 1	61¼	9 11
Litchfield	192	22 7	177	21 11
Little River	29½	5 1	15½	3 0
Loch	60	9 7	105	16 2
Lockington	147¾	19 10	185¾	22 4
Locksley	79¼	12 9	122¼	17 10
Longwood	84¾	13 3	127¾	18 2
Lorquon	261½	25 8	242¼	24 11
Lubeck	175	21 11	155¾	20 6
Lyons	248	25 3	213¾	23 9
Macorna	165¼	21 1	203¼	23 4

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Maffra	132¼	18 8	177¼	21 11
Maindample	122½	17 10	165½	21 1
Maldon	89	14 0	127	18 2
Malmsbury	63½	10 3	101½	15 11
Mangalore	68	10 11	111	16 11
Mansfield	131¼	18 8	174¼	21 11
Marnoo	200	22 11	180¾	22 0
Maroona	144¼	19 8	110	16 8
Maryborough	112	17 1	97	15 4
Massey	197¼	22 11	182¼	22 2
Mathoura	169¼	21 3	212¼	23 8
Meatian	212¾	23 8	217¼	24 0
Meeniyan	89	14 0	134	18 10
Melton	23¼	4 3	61½	9 11
Meredith	71	11 3	29	5 0
Merino	231¼	24 5	197	22 11
Merrigum	117¾	17 6	160¾	20 10
Merton	108¾	16 6	151¾	20 5
Middle Creek	112½	17 1	93¼	14 8
Mildura	351¼	28 9	336¼	28 2
Mincha	159¼	20 9	197¼	22 11
Minhamite	196¼	22 11	151¼	20 5
Mininera	138½	19 1	96½	15 4
Minyip	198¼	22 11	179	22 0
Miram	266½	25 10	247¼	25 3
Mirboo North	110	16 8	155	20 6
Mitiamo	142¼	19 7	180¼	22 0
Mitre	227¾	24 5	208½	23 5
Moama	146½	19 10	189½	22 6
Moira	160¼	20 10	203¼	23 4
Molesworth	90½	14 6	133½	18 10
Mologa	147	19 10	185	22 4
Monomeith	44	7 5	89	14 0
Moolort	100¾	15 11	108¼	16 6
Moorooduc	35¾	5 11	80¾	12 10
Mooroopna	110	16 8	153	20 5
Moranding	49¾	8 0	92¾	14 7
Moriac	60	9 7	15	2 11
Mornington	40¼	6 8	85¼	13 5
Mortat	251¼	19 3	232	17 10
Mortlake	150¼	20 5	105¼	16 2
Morton Plains	207½	23 5	192½	22 9
Morwell	90	14 0	135	19 0
Moulamein	231½	24 5	274½	26 2
Moutajup	184¾	22 4	150½	20 5
Mulwala	164	21 1	207	23 4
Munro	145¼	19 10	190¼	22 7
Murchison	93¼	14 8	136¼	19 1
Murchison East	91½	14 6	134½	19 0
Murrabit	194½	22 9	232¼	24 6

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Murroon	95½	15 0	50½	8 4
Murtoa	185¼	22 4	166	21 1
Myall	190¼	22 7	228¼	24 5
Myamyn	224½	24 2	190¼	22 7
Myrtleford	177½	21 11	220½	24 0
Mysia	160¼	20 10	164¾	21 1
Mystic Park	196¾	22 11	234¾	24 6
Nagambie	78¼	12 6	121¼	17 7
Nathalia	147¾	19 10	190¾	22 7
Natimuk	218¾	24 0	199½	22 11
Nerrin Nerrin	126¾	18 2	84¾	13 3
Netherby	267½	25 10	248¼	25 3
Newlyn	90½	14 6	76¼	12 5
Newstead	92¼	14 7	116¾	17 5
Newtown	92¾	14 7	68	10 11
Nhill	248¼	25 3	229	24 5
Niemur	216¼	23 9	259¼	25 6
Noradjuha	223½	24 2	204¼	23 4
Nowa Nowa	208¼	23 5	253¼	25 5
Nullawil	207	23 4	211½	23 8
Numurkah	133¾	18 10	176¾	21 11
Nyora	56½	9 2	101½	15 11
Oaklands	199¾	22 11	242¾	24 11
Orbost	231½	24 5	276½	26 2
Ovens	180¾	22 0	223¾	24 2
Pakenham	36½	6 3	81½	13 0
Parwan	29¼	5 1	67¼	10 11
Penshurst	208½	23 5	163½	21 1
Perekerten	247½	25 3	290½	26 11
Picola	154½	20 6	197½	22 11
Pimpinio	213½	23 9	194¼	22 9
Pine Lodge	121	17 7	164	21 1
Pittong	104¼	16 2	79½	12 9
Pomborneit	113¾	17 1	68¾	10 11
Port Fairy	186½	22 4	141½	19 7
Prairie	136¼	19 1	174¼	21 11
Pura Pura	121½	17 7	79½	12 9
Purdeet	201½	23 0	156½	20 6
Pyalong	56½	9 2	99½	15 9
Pyramid	155	20 6	193	22 9
Quambatook	191	22 7	195½	22 9
Rainbow	266¼	25 10	247	25 1
Ravenswood	90¼	14 6	128¼	18 3
Raywood	120	17 6	158	20 6
Redesdale	76¼	12 5	114¼	17 2
Rennie	181	22 0	224	24 2
Riddell	35¼	5 11	73¼	11 8
Rochester	138¾	19 1	176¾	21 11
Rockbank	18½	3 4	56½	9 2
Romsey	40½	6 8	78½	12 6

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Rosebery	246	25 1	226¾	24 2
Rosedale	112¼	17 1	157¼	20 6
Roslynmead	157¼	20 6	195¼	22 9
Rowsley	34¼	5 10	72¼	11 7
Rupanyup	184¾	22 4	165½	21 1
Rushworth	104¾	16 2	147¾	19 10
Rutherglen	169¼	21 3	212¼	23 8
Sale	128¾	18 3	173¾	21 6
Sandford	242½	24 11	208¼	23 5
Sanger	188½	22 6	231½	24 5
Sea Lake	238	24 8	242½	24 11
Serviceton	287	26 8	267¾	25 10
Seymour	61¼	9 11	104¼	16 2
Sheephills	206¼	23 4	187¼	22 4
Shelbourne	99¼	15 9	137¼	19 1
Shelley	235½	24 8	278½	26 3
Shepparton	113	17 1	156	20 6
Skipton	111½	17 1	86¾	13 9
Sloane	169	21 3	212	23 8
Springhurst	160	20 9	203	23 0
St. Arnaud	158½	20 9	143½	19 7
Stavely	162	20 10	127¾	18 2
Stawell	150	20 4	130¾	18 8
St. James	141¾	19 7	184¾	22 4
Stony Creek	91	14 6	136	19 0
Stony Point	46½	7 9	91½	14 6
Stratford	138½	19 1	183½	22 2
Strathallan	146¼	19 10	184¼	22 4
Strathkellar	192	22 7	157¾	20 6
Strathmerton	146	19 10	189	22 6
Sunbury	23¾	4 3	61¾	9 11
Sutherland	165½	21 1	150½	20 5
Swan Hill	214½	23 9	252½	25 5
Tabilk	74	11 8	117	17 5
Tabor	210	23 5	170	21 3
Talbot	107½	16 4	88¼	14 0
Tallangatta	212½	23 8	255½	25 5
Tallarook	56	9 1	99	15 8
Tallygaroopna	123¾	17 10	166¾	21 2
Tandarra...	127	18 2	165	21 1
Tantonan	179	22 0	222	24 1
Tarnagulla	136¼	19 1	121¼	17 7
Tarranyurk	242	24 11	222¼	24 1
Tarrawingee	157¾	20 6	200¾	22 11
Tatura	110	16 8	153	20 5
Tatyoon	144¼	19 8	102¼	16 1
Telford	154¾	20 6	197¾	22 11
Terang	137¼	19 1	92¼	14 7
Thorpdale	92½	14 7	137½	19 1
Thyra	168¼	21 3	211¼	23 8
Tinamba	127½	18 2	172½	21 6

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Tocumwal	156¼	20 6	199¼	22 11
Tongala	130¼	18 8	173¼	21 6
Tooborac	62¾	10 1	105¾	16 2
Toolamba	103¼	16 1	146¼	19 10
Toolondo	234¼	24 6	215½	23 9
Toongabbie	109¾	16 8	154¾	20 6
Toora	114½	17 2	159½	20 9
Tourello	93	14 7	73¾	11 8
Tragowel	170¾	21 6	208¾	23 5
Traralgon	98½	15 8	143½	19 7
Trawalla	97½	15 5	78¼	12 6
Trawool	63	10 1	106	16 2
Tungamah	149¼	20 4	192¼	22 9
Ultima	221¼	24 1	225¾	24 2
Urangara	218	24 0	183¾	22 2
Vasey	228	24 5	193¾	22 9
Violet Town	105¼	16 2	148¼	20 1
Waala	140½	19 3	183½	22 2
Wahgunyah	174	21 6	217	23 9
Wahring	84½	13 3	127½	18 2
Wail	219¼	24 0	200	22 11
Waitchie...	233½	24 6	238	24 8
Wakool	197½	22 11	240½	24 11
Wallan	29½	5 1	72¼	11 7
Wal Wal	169	21 3	149¾	20 4
Wanalta	112½	17 1	155½	20 6
Wandong	34¼	5 10	77¼	12 5
Wangamong	194½	22 9	237½	24 8
Wangaratta	145½	19 10	188½	22 6
Wannon	210½	23 5	176¼	21 11
Waranga	99¼	15 9	142¼	19 7
Warncoort	87	13 9	42	6 8
Warne	213	23 8	217½	24 0
Warracknabeal	216½	23 9	197¼	22 11
Warragoon	175½	21 11	218½	24 0
Warragul	62¼	10 1	107¼	16 4
Warrenheip	69¾	11 1	50¾	8 4
Warrnambool	166	21 1	121	17 7
Warrong	179	22 0	134	18 10
Watchem	202½	23 0	187½	22 4
Watchupga	234¾	24 6	219¾	24 0
Waubra	95¼	15 0	76	12 4
Wedderburn	151	20 5	155¼	20 6
Wedderburn Junction ...	146	19 10	150¼	20 5
Weerite	117¾	17 6	72¾	11 7
Werribee	19¾	3 8	25¼	4 5
Westmere	132½	18 8	90½	14 6

WOOL RATES—*continued.*

Station.	Miles.	To Melbourne, Kensington, South Kensington, Newmarket, West Footscray and Macaulay. Rate per Bale.	Miles.	To Geelong. Rate per Bale.
		s. d.		s. d.
Whittlesea	26¾	4 6	70¼	11 3
Willaura	154¼	20 6	120¼	17 7
Winchelsea	70¾	11 3	25¾	4 5
Wingeel	73½	11 8	31½	5 5
Winnap	258	25 6	223¾	24 2
Wodonga	187	22 4	230	24 5
Womboota	163¼	21 1	206¼	23 4
Wonthaggi	87¼	13 10	132¼	18 8
Woodend	48½	8 0	86½	13 9
Woodfield	114½	17 2	157½	20 6
Woomelang	241¼	24 11	226¼	24 2
Wunghnu	129½	18 7	172½	21 6
Wycheproof	190¼	22 7	194¾	22 9
Wychitella	159¾	20 9	164¼	21 1
Yaapeet	276¾	26 2	257½	25 6
Yabba North	138¼	19 1	181¼	22 0
Yackandandah	184	22 2	227	24 2
Yallakool	191	22 7	234	24 6
Yanac	279¾	26 3	260½	25 8
Yangalake	266¼	25 10	309¼	27 5
Yan Yean	23	4 1	66½	10 8
Yarck	97	15 4	140	19 3
Yarra Glen	31¾	5 5	76¾	12 5
Yarrawonga	161½	20 10	204½	23 4
Yatchaw	204¾	23 4	170¼	21 6
Yea	79¾	12 9	122¾	17 10
Yendon	75	11 9	45¾	7 7
Yinnar	97¼	15 5	142¼	19 7

Rates and Conditions (Supplementary to the General Conditions) for the Carriage of Live Stock.

1. The Commissioners will not be liable for the loss of or injury to any horses, cattle, or other animals, or birds in excess of the amounts specified hereunder, unless at the time of delivery thereof at the station or siding for the purpose of being carried, the value of such horses, cattle, or other animals, or birds, shall have been declared by the consignor thereof, and the increased charges hereunder set forth (which are in addition to the freight charges), shall have been paid to and accepted by the employee duly authorised to receive the same.

Power is reserved to the Commissioners to regulate the number per truck or container of horses, cattle or other animals or birds insured at the risk of the Commissioners, and the Commissioners will not be responsible for any loss or injury which may occur to any live stock during the loading or unloading, or during transit, when such loss or injury arises from fear or restiveness, or through such live stock being suffocated or trampled upon, and without negligence on the Commissioners' part.

The Commissioners' liability is limited as follows, viz.:—For any horse, £50. For any neat cattle, per head, £20. For any sheep, pig, dog, or other small animal, or bird, per head, £1.

The increased charges are as under, and must be prepaid upon consignment of the stock :—

Distance in Miles	Per cent. on the Declared Value in excess of the Commissioners' Liability	Minimum Additional Charges	
		Horses and Cattle, including Bulls	Pigs, Sheep (including Rams), Dogs, or other small Animals or Birds
	£ s. d.	£ s. d.	£ s. d.
From 1 to 150 miles	4 0 0	1 0 0	0 2 6
,, 151 to 200 ,,	5 0 0	1 10 0	0 5 0
,, 201 to 300 ,,	6 0 0	2 0 0	0 10 0
,, 301 to 400 ,,	7 0 0	2 0 0	0 10 0
,, 401 to 500 ,,	8 0 0	2 0 0	0 10 0
,, 501 to 600 ,,	9 0 0	2 0 0	0 10 0

Provided that in all cases any animal or bird exceeding in value £100 shall be insured only under special agreement.

2. The Commissioners may decline to receive live stock for carriage on Sunday, Good Friday, Christmas Day, or on any proclaimed holiday, or on any day immediately preceding such Sunday or holiday, when through transit cannot be provided.

3. The Commissioners will not be bound to receive any live stock for carriage to or from any railway station which is not equipped with platforms, yards, and other suitable facilities for loading and unloading live stock, provided that, at the option of the Commissioners, live stock may be received for carriage to or from such places if the owners accept all risk and undertake the loading and unloading, and sign the prescribed special requisition form.

4. Except where otherwise provided, live stock shall be loaded and unloaded by the consignors and consignees respectively, by whom also the trucks shall be hand shunted to and from the loading race, and the truck doors and fastenings secured.

5. The number of animals must be so limited that the gross weight in any one truck shall not exceed its weight-carrying capacity.

6. No live stock shall be brought to any railway station except on the condition that the owner accepts all risk of injury to such live stock on the premises not arising from negligence on the part of the Commissioners, and the Commissioners shall be held free from all risk and responsibility, in respect of loss of or damage to such live stock arising in connection with loading or unloading, or from suffocation, or being trampled on, bruised, or otherwise injured in transit, or from fire, or where the loss or injury arises from fear or restiveness, or from any other cause whatsoever, not arising from negligence as aforesaid ; and all fastenings in either horse boxes or trucks must be made secure by the consignor, upon whom shall rest the sole responsibility therefor.

The Commissioners will not be responsible for any particular number of live stock specified as being loaded into any truck for carriage by rail.

LIVE STOCK—*continued.*

7. The Commissioners do not guarantee, under any circumstances, the arrival or delivery of live stock at any particular time, by any particular train, or for any particular market, neither do they undertake to advise consignees of the arrival of live stock.

8. All live stock must be removed from the railway premises immediately after it is unloaded ; or if left will remain at the owner's risk and expense, and may be sent to agistment or livery. The cost of such sending and of such agistment or livery shall be paid by the owner, and such cost must be paid on demand as part of the authorized charges ; and such stock, if not removed within seven days, may be sold by auction by order of the Commissioners, and the proceeds applied in payment of all expenses incurred, and the balance thereof handed over to the owner on demand.

9. Rugs, bridles, saddles, or other trappings forwarded with horses, or other animals, are carried entirely at the risk of the owner, and the Commissioners accept no responsibility for damage done to these articles, unless forwarded and charged for as goods.

Yokes, harness and other goods shall be separately consigned and charged for if loaded in same truck as live stock ; the total charge shall not exceed the rate for a full truck of Live Stock.

10. When a truck is only partly occupied by a consignment, the Commissioners reserve the right to fill it up to its full carrying capacity with other stock, if the rate for a full truck load has not been paid.

11. Requisitions for trucks for the carriage of live stock by rail shall be made on the prescribed form, unless it be necessary to requisition therefor by telegram or letter, or through an employee of the Commissioners at any station or siding, when such requisition shall be subject to the same conditions as if made on the prescribed form.

The acceptance of any requisition or cash deposit by any employee of the Commissioners at any station or siding shall not be taken to imply that the required trucks will be supplied.

12. Requisitions for the carriage of live stock must be lodged with the Commissioners not less than three clear days (Sundays excluded) before the date on which the live stock is to be loaded, and such requisitions must be accompanied by a cash deposit of 63s. 7d. for each live stock truck, and 31s. 10d. for each 4-wheeled goods truck so requisitioned, or by the actual amount of the freight charge if it be less than the deposit specified, provided that no requisition for the carriage of live stock will be received on any date in excess of 21 days prior to the date on which the live stock is to be loaded.

In respect of recognized agents or consignors of stock, a fixed deposit, or a guarantee in the form approved by the Commissioners, may be accepted in lieu of the cash deposit for each truck prescribed in the foregoing.

13. Any requisition for the carriage of live stock which has been lodged with and accepted by the Commissioners cannot be withdrawn, neither can the required number of trucks be reduced, nor the specified loading station altered, except on payment of the penalty charges shown hereafter.

(*a*) Orders for trucks to load to Newmarket sales :—

(1) 63s. 7d. if truck be supplied before notice of cancellation or alteration be received, provided that if a truck be not used on account of rain, or industrial dispute, the penalty charge shall be 15s. 10d. per truck ;

(2) 15s. 10d. if notice of canellation or alteration be received before truck is supplied.

(*b*) All orders (except as per (*a*)) :—

(1) No charge if notice of canellation be received before truck is supplied or if truck be supplied and is not loaded but is held for a later order at the same station or in respect of stand-by trucks which are supplied to meet anticapated orders ex country sales.

(2) 31s. 10d. if an ordered truck is supplied empty but is not loaded and the truck is moved empty in any direction to another station.

In respect of requisitions for 14 trucks and upwards, one truck in every 13 shall be exempt from the foregoing provisions.

The Commissioners, however, reserve to themselves the right to charge for all trucks ordered and not used on the date and at the station specified in the order as if used.

14. If a special train be provided at the request of the owner for live stock which otherwise would be carried by ordinary train, a charge at the rate of 90s. 2d. per train hour shall be imposed in addition to the freight charge, provided the train mileage for the return trip does not exceed 50 miles ; the time to be computed from when the train is scheduled to leave and scheduled to return to the originating station. Any special train for live stock is subject to arrangements being made with the Live Stock Agent.

15. When an alteration is made in the destination while live stock trucks are in transit, a charge of 7s. 9d. per truck shall be made for each alteration.

16. Live stock must be yarded in sufficient time to be loaded for and despatched by the trains specified by the proper employee of the Commissioners. If such live stock be not loaded in accordance with the arrangement, the Commissioners will retain the cash deposit lodged in respect of any truck not so loaded, and, in addition, will charge the person who lodged the

LIVE STOCK—*continued.*

requisition the difference (if any) between the amount of the deposit on the trucks not loaded and the freight charges thereon to the destination station specified on the requisition ; and the Commissioners also reserve the right to remove or otherwise use the trucks supplied for the purpose.

17. Mixed stock will, at the request of the consignor, be carried together in the same truck, provided the whole consignment belongs to one person, and the Commissioners are specially relieved of all responsibility occasioned thereby.

18. The Commissioners may refuse to carry any horse, cattle, sheep, swine, bird, or other live stock, which they reasonably believe to be diseased. No person shall wilfully or negligently drive or bring, or cause or permit to be driven or brought upon or into any station yard, shed, or premises of the Commissioners, any horse, cattle, sheep, swine, bird, or other live stock, having or suffering from any disease whatosever. Any person guilty of a breach of this by-law shall be liable to a penalty not exceeding £20.

19. Consignors will not, owing to the danger of fire arising through sparks from the engine, be permitted to place straw in sheep trucks.

20. Horse boxes are subject to the same regulations in respect of deposits and penalties as are prescribed in Conditions Nos. 12 and 13 for live stock trucks.

21. **Tarpaulins.**—Tarpaulins used at the request of the owner of the stock, to cover live stock trucks (including trucks conveying stock free or at reduced rates) shall be charged for at the rate of 10s. 7d. for each tarpaulin.

If covering of the trucks is performed by the Commissioners an additional charge of 2s. 1d. for each tarpaulin shall be imposed.

22. **Refund Fee on Live Stock Rebates.**—From any gross rebate which may be granted in respect of—

(*a*) Store stock on Up or Down journey or combined Up and Down journey ; or

(*b*) Stock from Agricultural Shows or returning from stud, horses returning from race meetings or staving stock, for which the necessary certificates were not presented at the time of consigning,

the Commissioners will deduct the sum of 2s. 6d. per truck and the surplus remaining over after such deduction has been made shall be the net rebate payable to the owner or to his order.

23. **Loading and Unloading Live Stock at more than one Station.**—(*a*) Any truck of live stock listed to set down animals *en route* shall be charged the rate applicable to the number of animals originally loaded from the starting station to the final destination station, with an additional charge of 12s. 9d. for each occasion on which the truck is detached from the train for the purpose of setting down animals *en route*.

(*b*) Any truck of live stock listed to pick up animals *en route* shall be charged at the rate for a full truck from the starting station to the final destination station, with an additional charge of 12s. 9d. for each pick-up listed, but the charges shall be adjusted at the final destination station in accordance with the number of animals actually carried plus a charge of 12s. 9d. for each occasion on which the truck was detached from the train for the purpose of picking up animals.

(*c*) See Addenda page 22.

24. **Stock Spelled en route.**—Live stock which can be given through transit but is spelled *en route* at the request of owner shall be charged on basis of through mileage rate plus an extra charge of 12s. 9d. per truck, provided break of journey does not exceed four days at spelling station ; if break of journey exceeds four days, the rates for two separate journeys shall be charged.

No extra charge shall be imposed in respect of consignments booked by the Department for spelling during interval between trains.

If any portion of consignment be offered for sale or sold at the spelling station, the mileage rates for two separate journeys shall be charged.

25. Live stock despatched from one district to another which is placed at Newmarket at the request of the owner for any purpose other than spelling shall be charged the freight as for two separate journeys, i.e. :—

(*a*) from the forwarding station to Newmarket, and

(*b*) from Newmarket to destination station ;

provided that stock which is not sold or offered for sale and which would be entitled to store rate as a through consignment, shall be charged store rate as for two separate journeys as per (*a*) and (*b*).

26. **Feeding Charges.**—Wherever it is necessary that live stock be fed *en route*, or at the destination on account of delay by the consignee in taking delivery, the cost of feeding if defrayed by the Department shall be debited to the consignee.

27. **Live Stock Agent** shall mean the Live Stock Agent of the Commissioners, or such officer as may be acting in that capacity.

LIVE STOCK—*continued.*

THE RATES FOR THE CARRIAGE OF LIVE STOCK PER 4-WHEELED TRUCK SHALL, UNLESS OTHERWISE PROVIDED, BE COMPUTED ON THE FOLLOWING BASIS (See Notes on page 209).

Number of Head	Sheep, Calves, and Mixed Lots of Sheep and Calves (See Notes 1, 2, 3)	Mixed Lots of Sheep, Calves and Pigs ; Calves and Pigs ; Pigs and Sheep (See Notes 1, 2, 3)		Pigs. (See Note 1)	
		1/249 Miles	250 Miles and over	1/249 Miles	250 Miles and over
1	10d. per mile, minimum 30s. 8d.; half full rate to be maximum charge	... ...	... ...	8 $\frac{9}{10}$d. per mile, minimum 27s. 7d.; half full rate, less 10 per cent. to be maximum charge	8 $\frac{9}{10}$d. per mile ; half full rate, less 10 per cent., to be maximum charge
2–15	Half full rate ...	Half full rate ...	Half full rate ...	Half full rate, less 10 per cent	Half full rate, less 10 per cent
16–25	Two-thirds full rate ; unless half full rate, plus $\frac{9}{10}$d. *per mile for each* animal over 15 is cheaper ; one tier rate to be maximum charge	Two-thirds full rate ; unless half full rate, plus $\frac{9}{10}$d. *per mile for each* animal over 15 is cheaper	Two-thirds full rate ; unless half full rate, plus $\frac{9}{10}$d. *per mile for each* animal over 15 is cheaper	Two-thirds full rate, less 10 per cent. ; unless half full rate, less 10 per cent., plus $\frac{9}{10}$d. per mile for each animal over 15 is cheaper	Two-thirds full rate, less 10 per cent. ; unless half full rate, less 10 per cent., plus $\frac{9}{10}$d. per mile for each animal over 15 is cheaper
26 and over	One tier rate ; unless two-thirds full rate, plus $\frac{9}{10}$d. per mile for each animal over 25 is cheaper	Full rate, less 10 per cent.; unless two thirds full rate, plus $\frac{9}{10}$d. per mile for each animal over 25 is cheaper ; one tier rate for 250 miles to be maximum charge	One tier rate ; unless two-thirds full rate, plus $\frac{9}{10}$d. per mile for each animal over 25 is cheaper	Full rate, less 10 per cent. ; unless two-thirds full rate, less 10 per cent, plus $\frac{9}{10}$d. per mile for each animal over 25 is cheaper ; one tier rate for 250 miles to be maximum charge	One tier rate ; unless two-thirds full rate, less 10 per cent., plus $\frac{9}{10}$d. per mile for each animal over 25 is cheaper
Any number loaded in two tiers of sheep truck	Full rate	Full rate, plus 20 per cent. ; full rate for 250 miles to be maximum charge	Full rate	Full rate, plus 20 per cent. ; full rate for 250 miles to be maximum charge	Full rate

LIVE STOCK—*continued.*

Number of Head	CATTLE (Including Bulls)	HORSES (Including Stallions) (See Note 4)
1	10d. per mile, minimum 30s. 8d. ; half full rate to be maximum charge ; one calf not over three months old at foot of cow, free	Half full rate, one foal not over six months old at mare's foot, free
2	Half full rate ; one calf not over three months old at foot of each cow, free	Two-thirds full rate ; one foal not over six months old at each mare's foot, free
3	Two-thirds full rate ; one calf not over three months old at foot of each cow, free	Full rate
4	Full rate	

MIXED CONSIGNMENTS, not otherwise specified, shall be charged as follows :—

1 *calf, sheep or pig	...	with 1 cattle	... Half full rate
,, ,, ,,	...	,, 2 ,,	... Two-thirds full rate
,, ,, ,,	...	,, 3 ,,	... Full rate
,, ,, ,,	...	,, 1 horse	... Two-thirds full rate
,, ,, ,,	...	,, 2 horses	... Full rate
2 to 15 *calves, sheep or pigs		,, 1 cattle or 1 horse	Two-thirds full rate
,, ,, ,,		,, 2 cattle or 2 horses	Full rate
16 or more *calves, sheep or pigs		,, 1 cattle or 1 horse	Full rate, unless two-thirds full rate, plus $\frac{8}{10}$d. per mile for each calf, sheep or pig over 15 is cheaper
1 cattle	... ...	,, 1 horse	... Two-thirds full rate
2 cattle	... ...	,, 1 horse	... Two-thirds full rate
1 cattle	... ...	,, 2 horses	... Full rate

Live Stock conveyed on Narrow-gauge Lines.—The charges for live stock conveyed on narrow-gauge lines and between narrow and broad gauge lines shall be computed as shown hereunder :—

Class of Live Stock	Loaded in—	To be charged as if loaded in Broad-gauge Four-wheeled Truck as under:
Cattle or horses ...	1 narrow-gauge truck	One cattle truck
Sheep	1 ,, ,, ...	One tier of sheep truck
	2 ,, ,, ...	Two tiers of sheep truck
Pigs, *calves, mixed consignments of sheep, pigs and *calves	1 ,, ,, ...	One tier of sheep truck
	2 ,, ,, ...	Two tiers of sheep truck

* Calves not exceeding 36 inches in height at shoulder : if exceeding this height calves shall be charged the same rates as cattle.

NOTES.—

1. The charges specified for sheep and pigs up to and inclusive of group "26 head and over," are applicable only if loaded in a cattle truck or goods truck or in one tier of a sheep truck; in the latter case the consignment note and waybill must be plainly endorsed "one tier used." If such consignment be spread over both tiers of a sheep truck the rate for a two-tier lot shall be charged.

2. The charges specified for calves and for calves loaded with sheep or pigs up to and inclusive of group "26 and over," are applicable only if the consignments be loaded in one tier of a sheep truck; if such consignments be spread over both tiers of a sheep truck the rate for a two-tier lot shall be charged. Calves exceeding the following heights at the shoulder shall not be loaded in sheep trucks:—Top tier 36 inches, lower tier 33 inches.

3. Calves of any height loaded in a cattle truck or goods truck shall be charged the same rates as cattle.

4. Shetland ponies not over 40 inches high at the shoulder and so shown on consignment note shall be charged the same rates as cattle.

5. The Commissioners may at their option supply goods or cattle trucks for the carriage of sheep, calves not exceeding 36 inches in height at shoulder, or pigs. One such truck shall be charged as if the consignment was loaded in one tier of a sheep truck, and two such trucks as if loaded in two tiers of a sheep truck.

6. Goats shall be charged the same rate and conditions as sheep.

7. Camels, Elephants or Donkeys shall be charged the same rates and conditions as horses.

8. **Divided Consignments.**—Consignments of live stock forwarded by one or more consignors to one or more consignees shall be charged as one consignment, provided that (*a*) in the case of a "paid" consignment, the combined lot be consigned by one consignor who shall unless otherwise authorized pay the whole of the freight charges; or (*b*) in the case of a "to pay" consignment, the combined lot be consigned to one consignee who shall unless otherwise authorized pay the whole of the freight charges.

9. **Bogie Live Stock Trucks.**—Each "MM" bogie cattle truck or "LL" bogie sheep truck shall be counted as two trucks, provided that if a bogie truck be supplied for Departmental convenience to fulfil an order for one four-wheeled truck the rate for the latter type of truck shall be charged and the waybill so endorsed.

10. **Bogie Live Stock Trucks from South Australia.**—(*a*) A minimum charge of full rate for the Victorian proportion as for one four-wheeled truck shall be charged for any consignment of one, two or three head of cattle, or one or two head of horses forwarded from South Australian stations to Victorian stations in a South Australian bogie cattle truck.

(*b*) Any consignment of sheep or pigs which can be accommodated in a four-wheeled truck must not be accepted for despatch in a South Australian bogie sheep truck on a journey from South Australian stations to Victorian stations. A four-wheeled South Australian or Victorian sheep truck must be used for such consignments and the Victorian proportion of the charges shall be computed at the prescribed rates.

MILEAGE RATES PER 4-WHEELED TRUCK FOR THE CARRIAGE OF LIVE STOCK BY GOODS OR MIXED TRAINS.

The mileage to be charged for Live Stock forwarded from or to cattle sidings situated outside station limits shall, unless otherwise shown in the siding instructions, be that prescribed for general goods traffic at the station concerned.

Miles	Full Rate	Store Rate	One Tier Rate	Miles	Full Rate	Store Rate	One Tier Rate
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
1	3 10 1	2 13 11	1 17 2	51	6 13 8	5 1 5	4 9 8
2	3 10 1	2 13 11	1 17 2	52	6 17 1	5 4 0	4 11 1
3	3 10 1	2 13 11	1 17 2	53	6 19 5	5 5 8	4 13 2
4	3 10 1	2 13 11	1 17 2	54	7 0 4	5 6 6	4 15 6
5	3 10 1	2 13 11	1 17 2	55	7 5 11	5 10 7	4 17 7
6	3 10 1	2 13 11	1 17 2	56	7 6 8	5 11 5	5 0 0
7	3 10 1	2 13 11	1 17 2	57	7 9 1	5 13 4	5 0 11
8	3 10 1	2 13 11	1 17 2	58	7 13 7	5 16 4	5 3 0
9	3 10 1	2 13 11	1 17 2	59	7 15 8	5 18 0	5 4 4
10	3 10 1	2 13 11	1 17 2	60	7 17 7	5 19 8	5 4 4
11	3 10 1	2 13 11	1 17 2	61	7 19 0	6 0 4	5 6 6
12	3 10 1	2 13 11	1 17 2	62	8 1 1	6 1 11	5 7 5
13	3 10 1	2 13 11	1 17 2	63	8 3 6	6 3 7	5 10 8
14	3 10 1	2 13 11	1 17 2	64	8 6 7	6 6 4	5 11 11
15	3 10 1	2 13 11	1 17 2	65	8 8 10	6 7 8	5 13 4
16	3 10 1	2 13 11	1 17 2	66	8 13 4	6 11 0	5 14 0
17	3 10 1	2 13 11	1 17 2	67	8 15 5	6 12 8	5 15 2
18	3 10 1	2 13 11	1 17 2	68	8 16 8	6 13 7	5 16 4
19	3 10 1	2 13 11	1 18 7	69	8 19 10	6 15 11	6 0 5
20	3 10 1	2 13 11	1 18 7	70	9 1 11	6 17 7	6 2 11
21	3 10 1	2 13 11	2 0 7	71	9 5 2	7 0 0	6 3 10
22	3 10 1	2 13 11	2 2 10	72	9 7 8	7 1 8	6 6 4
23	3 10 1	2 13 11	2 4 0	73	9 8 5	7 2 6	6 7 1
24	3 10 1	2 13 11	2 5 1	74	9 11 11	7 5 0	6 8 5
25	3 10 1	2 13 11	2 7 4	75	9 12 11	7 5 11	6 10 6
26	3 14 6	2 17 0	2 9 4	76	9 17 5	7 9 1	6 11 6
27	3 16 8	2 18 8	2 11 10	77	9 18 5	7 9 11	6 13 8
28	3 18 0	2 19 10	2 12 6	78	10 0 10	7 11 10	6 15 0
29	4 1 2	3 1 11	2 13 11	79	10 3 0	7 13 0	6 15 11
30	4 3 2	3 3 7	2 14 11	80	10 6 1	7 15 8	6 15 11
31	4 5 8	3 5 2	2 18 2	81	10 8 2	7 17 5	6 18 2
32	4 6 6	3 6 2	2 19 1	82	10 11 6	7 19 8	7 1 6
33	4 7 10	3 7 1	3 0 4	83	10 16 0	8 3 2	7 3 10
34	4 12 1	3 10 1	3 1 6	84	10 18 4	8 4 11	7 4 7
35	4 14 2	3 11 10	3 3 7	85	11 0 5	8 6 2	7 5 11
36	4 17 7	3 14 4	3 5 11	86	11 1 4	8 7 2	7 6 8
37	5 0 0	3 16 0	3 7 11	87	11 2 7	8 8 1	7 9 1
38	5 3 0	3 18 5	3 9 4	88	11 6 11	8 11 6	7 12 6
39	5 5 1	4 0 1	3 10 1	89	11 7 11	8 12 2	7 13 7
40	5 7 5	4 1 6	3 10 1	90	11 10 4	8 13 8	7 15 8
41	5 9 10	4 3 2	3 14 6	91	11 12 4	8 15 5	7 16 11
42	5 13 4	4 6 1	3 15 11	92	11 14 5	8 17 1	7 17 7
43	5 15 2	4 7 6	3 16 8	93	11 18 10	9 0 2	7 19 0
44	5 16 4	4 8 1	3 19 0	94	12 2 2	9 3 0	8 1 1
45	5 18 5	4 9 8	4 1 2	95	12 4 7	9 4 8	8 3 6
46	6 2 11	4 13 2	4 2 6	96	12 6 8	9 6 2	8 4 5
47	6 5 0	4 14 10	4 3 2	97	12 9 11	9 8 5	8 6 7
48	6 8 5	4 15 10	4 4 7	98	12 11 2	9 9 7	8 7 8
49	6 9 5	4 18 1	4 5 8	99	12 13 5	9 11 2	8 8 10
50	6 12 8	5 0 6	4 9 0	100	12 15 6	9 12 10	8 10 11

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Live Stock Mileage Rates Per 4-Wheeled Truck—*continued.*

Miles	Full Rate	Store Rate	One Tier Rate	Miles	Full Rate	Store Rate	One Tier Rate
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
101	12 17 7	9 14 6	8 13 4	**151**	14 19 2	11 5 5	10 16 0
102	12 18 7	9 15 0	8 14 4	**152**	15 1 7	11 7 2	10 17 0
103	12 18 7	9 15 0	8 14 4	**153**	15 3 8	11 8 11	10 18 4
104	13 0 10	9 16 8	8 15 5	**154**	15 4 10	11 9 7	10 18 4
105	13 0 10	9 16 8	8 15 5	**155**	15 6 8	11 11 5	10 19 1
106	13 1 11	9 18 0	8 17 6	**156**	15 10 1	11 13 10	11 0 5
107	13 1 11	9 18 0	8 17 6	**157**	15 11 5	11 14 5	11 1 4
108	13 4 4	9 19 2	8 18 10	**158**	15 13 6	11 16 6	11 4 8
109	13 4 4	9 19 2	8 18 10	**159**	15 14 8	11 17 2	11 4 8
110	13 5 2	9 19 11	9 1 11	**160**	15 16 10	11 18 8	11 5 10
111	13 5 2	9 19 11	9 1 11	**161**	15 17 10	11 19 4	11 6 11
112	13 6 5	10 1 0	9 3 1	**162**	16 0 0	12 1 2	11 7 11
113	13 6 5	10 1 0	9 3 1	**163**	16 1 4	12 2 1	11 7 11
114	13 11 7	10 5 0	9 5 2	**164**	16 3 5	12 3 8	11 10 4
115	13 11 7	10 5 0	9 5 2	**165**	16 4 6	12 4 7	11 11 5
116	13 12 11	10 5 11	9 6 4	**166**	16 6 7	12 6 1	11 12 4
117	13 12 11	10 5 11	9 6 4	**167**	16 12 2	12 10 6	11 15 7
118	13 12 11	10 5 11	9 6 4	**168**	16 13 1	12 11 2	11 15 7
119	13 14 1	10 6 6	9 7 8	**169**	16 14 4	12 11 11	12 0 2
120	13 14 1	10 6 6	9 7 8	**170**	16 16 7	12 13 6	12 1 2
121	13 15 5	10 7 7	9 9 8	**171**	16 17 6	12 14 2	12 2 2
122	13 15 5	10 7 7	9 9 8	**172**	17 0 11	12 16 11	12 4 7
123	13 18 4	10 9 11	9 10 8	**173**	17 2 0	12 17 7	12 4 7
124	13 18 4	10 9 11	9 10 8	**174**	17 3 1	12 18 5	12 6 8
125	13 19 7	10 11 0	9 14 2	**175**	17 4 0	12 19 1	12 7 10
126	13 19 7	10 11 0	9 14 2	**176**	17 7 6	13 1 10	12 8 11
127	14 0 7	10 11 6	9 15 0	**177**	17 8 7	13 2 7	12 9 11
128	14 0 7	10 11 6	9 15 0	**178**	17 10 8	13 4 4	12 9 11
129	14 1 10	10 12 5	9 16 5	**179**	17 12 10	13 5 11	12 11 2
130	14 1 10	10 12 5	9 16 5	**180**	17 15 2	13 7 6	12 13 5
131	14 3 11	10 14 1	9 17 5	**181**	17 17 4	13 9 2	12 15 6
132	14 3 11	10 14 1	9 17 5	**182**	17 18 6	13 9 11	12 16 6
133	14 5 0	10 14 11	9 18 5	**183**	17 19 2	13 10 11	12 16 6
134	14 5 0	10 14 11	9 18 5	**184**	18 3 0	13 13 1	12 18 7
135	14 7 4	10 16 7	10 0 10	**185**	18 3 11	13 14 1	12 19 10
136	14 7 4	10 16 7	10 0 10	**186**	18 6 0	13 15 10	13 1 11
137	14 7 4	10 16 7	10 1 7	**187**	18 9 6	13 18 0	13 1 11
138	14 9 5	10 18 4	10 1 7	**188**	18 11 7	13 19 8	13 3 1
139	14 9 5	10 18 4	10 1 7	**189**	18 12 8	14 0 7	13 4 4
140	14 10 7	10 19 0	10 5 0	**190**	18 13 10	14 1 5	13 8 6
141	14 10 7	10 19 0	10 5 0	**191**	18 17 0	14 3 11	13 10 11
142	14 11 6	10 19 8	10 6 1	**192**	18 18 4	14 4 7	13 10 11
143	14 11 6	10 19 8	10 6 1	**193**	18 19 4	14 5 6	13 11 7
144	14 13 8	11 1 4	10 9 5	**194**	19 2 10	14 8 0	13 12 11
145	14 13 8	11 1 4	10 9 5	**195**	19 6 0	14 10 7	13 16 1
146	14 15 0	11 2 4	10 10 7	**196**	19 6 10	14 11 2	13 17 6
147	14 15 0	11 2 4	10 10 7	**197**	19 7 11	14 12 0	13 17 6
148	14 17 1	11 4 1	10 13 11	**198**	19 9 4	14 12 11	13 18 4
149	14 17 1	11 4 1	10 13 11	**199**	19 10 0	14 13 8	13 19 7
150	14 19 2	11 5 5	10 16 0	**200**	19 13 6	14 15 4	14 0 7

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Live Stock Mileage Rates Per 4-Wheeled Truck—*continued.*

Miles	Full Rate	Store Rate	One Tier Rate	Miles	Full Rate	Store Rate	One Tier Rate
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
201	19 15 2	14 16 6	14 1 10	251	23 17 5	17 18 1	16 18 10
202	19 17 0	14 17 10	14 1 10	252	23 18 10	17 19 1	16 19 11
203	19 18 4	14 18 10	14 2 10	253	24 0 4	18 0 2	17 0 11
204	20 0 0	15 0 0	14 3 11	254	24 1 8	18 1 6	17 2 0
205	20 1 11	15 1 7	14 5 2	255	24 3 4	18 2 4	17 2 11
206	20 3 7	15 2 8	14 6 4	256	24 4 8	18 3 7	17 4 0
207	20 4 11	15 3 8	14 7 5	257	24 6 1	18 4 8	17 5 2
208	20 6 8	15 5 1	14 8 6	258	24 7 7	18 5 10	17 6 1
209	20 8 5	15 6 6	14 9 10	259	24 9 0	18 6 10	17 7 1
210	20 10 1	15 7 7	14 11 2	260	24 10 7	18 8 1	17 8 4
211	20 11 7	15 8 10	14 12 4	261	24 11 10	18 9 0	17 9 2
212	20 13 4	15 10 0	14 13 7	262	24 13 5	18 10 2	17 10 5
213	20 15 1	15 11 5	14 14 8	263	24 14 10	18 11 1	17 11 4
214	20 16 10	15 12 4	14 15 10	264	24 16 4	18 12 5	17 12 5
215	20 18 4	15 13 7	14 16 11	265	24 17 10	18 13 2	17 13 6
216	21 0 0	15 15 0	14 18 2	266	24 19 5	18 14 6	17 14 2
217	21 1 8	15 16 1	14 19 2	267	25 0 7	18 15 6	17 15 7
218	21 3 5	15 17 8	15 0 5	268	25 2 0	18 16 8	17 16 8
219	21 4 11	15 18 6	15 1 7	269	25 3 7	18 17 8	17 17 5
220	21 6 7	15 19 11	15 2 8	270	25 5 0	18 18 10	17 18 6
221	21 8 4	16 1 4	15 3 10	271	25 6 6	19 0 0	17 19 10
222	21 9 11	16 2 5	15 5 1	272	25 8 1	19 1 2	18 0 7
223	21 11 4	16 3 5	15 6 6	273	25 9 7	19 2 1	18 1 10
224	21 13 0	16 4 11	15 7 7	274	25 11 0	19 3 4	18 2 6
225	21 14 8	16 6 2	15 8 10	275	25 12 6	19 4 5	18 3 11
226	21 16 5	16 7 6	15 10 0	276	25 13 11	19 5 5	18 4 10
227	21 17 11	16 8 5	15 11 1	277	25 15 2	19 6 5	18 5 10
228	21 19 8	16 9 8	15 12 2	278	25 16 8	19 7 10	18 6 10
229	22 1 5	16 11 0	15 13 6	279	25 18 4	19 8 6	18 8 1
230	22 3 1	16 12 2	15 14 8	280	25 19 10	19 9 10	18 9 0
231	22 4 6	16 13 6	15 15 7	281	26 1 1	19 11 0	18 9 11
232	22 6 2	16 14 7	15 16 10	282	26 2 7	19 12 4	18 10 11
233	22 7 11	16 15 11	15 18 2	283	26 4 0	19 13 0	18 12 1
234	22 9 6	16 17 4	15 19 4	284	26 5 7	19 14 5	18 13 1
235	22 11 1	16 18 2	16 0 2	285	26 7 0	19 15 2	18 14 5
236	22 12 10	16 19 5	16 1 7	286	26 8 6	19 16 4	18 15 4
237	22 14 5	17 0 11	16 2 10	287	26 9 11	19 17 6	18 16 2
238	22 16 1	17 2 2	16 4 0	288	26 11 4	19 18 8	18 17 7
239	22 17 6	17 3 1	16 4 11	289	26 12 10	19 19 8	18 18 5
240	22 19 2	17 4 6	16 6 2	290	26 14 2	20 0 10	18 19 5
241	23 0 11	17 5 10	16 7 6	291	26 15 10	20 1 11	19 0 5
242	23 2 7	17 7 1	16 8 5	292	26 17 2	20 3 1	19 1 7
243	23 4 1	17 8 4	16 9 7	293	26 18 8	20 4 0	19 2 10
244	23 5 11	17 9 5	16 10 10	294	27 0 4	20 5 4	19 3 6
245	23 7 7	17 10 8	16 11 11	295	27 1 10	20 6 4	19 4 7
246	23 9 1	17 12 1	16 13 1	296	27 3 2	20 7 6	19 6 0
247	23 10 10	17 13 2	16 14 2	297	27 4 6	20 8 5	19 6 8
248	23 12 6	17 14 2	16 15 5	298	27 6 0	20 9 7	19 7 10
249	23 14 2	17 15 7	16 16 7	299	27 7 4	20 10 7	19 8 6
250	23 15 11	17 17 1	16 17 8	300	27 8 10	20 11 8	19 9 10

Live Stock Mileage Rates Per 4-Wheeled Truck—*continued.*

Miles	Full Rate	Store Rate	One Tier Rate	Miles	Full Rate	Store Rate	One Tier Rate
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
301	27 10 2	20 12 7	19 10 8	351	30 14 2	23 0 8	21 16 2
302	27 11 7	20 13 10	19 11 8	352	30 15 0	23 1 5	21 16 10
303	27 12 11	20 14 8	19 12 8	353	30 16 5	23 2 4	21 17 7
304	27 14 0	20 15 6	19 13 6	354	30 17 4	23 3 0	21 18 6
305	27 15 5	20 16 7	19 14 5	355	30 18 6	23 4 0	21 18 11
306	27 16 10	20 17 7	19 15 2	356	30 19 7	23 4 8	21 19 11
307	27 17 11	20 18 6	19 16 2	357	31 0 7	23 5 7	22 0 11
308	27 19 0	20 19 4	19 17 4	358	31 1 8	23 6 4	22 1 7
309	28 0 5	21 0 4	19 17 11	359	31 3 1	23 7 6	22 2 1
310	28 1 10	21 1 6	19 19 0	360	31 3 10	23 8 0	22 3 1
311	28 3 1	21 2 2	19 19 10	361	31 5 1	23 9 0	22 3 10
312	28 4 2	21 3 5	20 0 8	362	31 6 2	23 9 5	22 4 6
313	28 5 6	21 4 1	20 1 6	363	31 7 2	23 10 8	22 5 4
314	28 6 11	21 5 2	20 2 6	364	31 8 4	23 11 1	22 6 2
315	28 8 1	21 6 4	20 3 7	365	31 9 7	23 12 5	22 6 11
316	28 9 5	21 7 0	20 4 4	366	31 10 4	23 12 10	22 7 8
317	28 10 7	21 8 0	20 5 4	367	31 11 8	23 14 0	22 8 7
318	28 11 11	21 9 1	20 6 4	368	31 12 10	23 14 6	22 9 5
319	28 13 2	21 10 1	20 7 1	369	31 13 10	23 15 5	22 10 0
320	28 14 5	21 10 10	20 8 0	370	31 14 11	23 16 0	22 10 10
321	28 15 10	21 11 10	20 8 11	371	31 16 2	23 17 1	22 11 7
322	28 17 5	21 13 0	20 9 10	372	31 17 0	23 17 8	22 12 4
323	28 18 4	21 13 8	20 10 11	373	31 18 5	23 18 10	22 13 2
324	28 19 6	21 14 8	20 11 7	374	31 19 2	23 19 5	22 14 0
325	29 0 11	21 15 6	20 12 7	375	32 0 6	24 0 6	22 14 7
326	29 2 5	21 16 10	20 13 7	376	32 1 6	24 1 0	22 15 7
327	29 3 5	21 17 7	20 14 5	377	32 2 7	24 2 0	22 16 4
328	29 4 7	21 18 7	20 15 2	378	32 3 7	24 2 7	22 16 10
329	29 6 0	21 19 8	20 15 11	379	32 4 10	24 3 8	22 17 10
330	29 7 7	22 0 5	20 17 2	380	32 5 10	24 4 5	22 18 6
331	29 8 8	22 1 7	20 17 10	381	32 7 1	24 5 5	22 19 2
332	29 9 8	22 2 6	20 18 11	382	32 8 1	24 6 1	23 0 0
333	29 11 1	22 3 2	20 19 5	383	32 9 4	24 7 1	23 0 8
334	29 12 7	22 4 6	21 0 7	384	32 10 4	24 7 10	23 1 5
335	29 13 11	22 5 4	21 1 8	385	32 11 5	24 8 6	23 2 5
336	29 14 8	22 6 2	21 2 5	386	32 12 5	24 9 4	23 3 1
337	29 16 1	22 7 0	21 3 5	387	32 13 10	24 10 1	23 4 0
338	29 17 8	22 8 4	21 4 1	388	32 14 6	24 11 0	23 4 8
339	29 18 11	22 9 1	21 5 2	389	32 15 10	24 11 10	23 5 7
340	30 0 2	22 10 0	21 5 11	390	32 17 0	24 12 8	23 6 1
341	30 1 2	22 11 1	21 6 11	391	32 17 11	24 13 7	23 7 2
342	30 2 8	22 12 1	21 8 0	392	32 19 0	24 14 6	23 7 8
343	30 4 0	22 12 11	21 8 8	393	33 0 5	24 15 2	23 8 7
344	30 5 2	22 14 0	21 9 8	394	33 1 1	24 15 8	23 9 4
345	30 6 6	22 14 11	21 10 6	395	33 2 4	24 16 11	23 10 4
346	30 7 11	22 15 10	21 11 7	396	33 3 6	24 17 5	23 11 0
347	30 9 1	22 16 10	21 12 6	397	33 4 7	24 18 6	23 11 10
348	30 10 4	22 17 10	21 13 4	398	33 5 7	24 19 5	23 12 6
349	30 11 6	22 18 6	21 14 1	399	33 6 10	25 0 2	23 13 7
350	30 13 1	22 19 10	21 15 4	400	33 7 8	25 1 1	23 14 0

Live Stock Mileage Rates Per 4-Wheeled Truck—*continued.*

Miles	Full Rate	Store Rate	One Tier Rate	Miles	Full Rate	Store Rate	One Tier Rate
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
401	33 8 10	25 1 7	23 14 6	451	35 14 4	26 15 10	25 7 1
402	33 9 10	25 2 4	23 15 5	452	35 15 5	26 16 4	25 7 11
403	33 10 6	25 3 0	23 15 11	453	35 16 2	26 17 2	25 8 4
404	33 11 5	25 3 7	23 16 8	454	35 17 2	26 17 11	25 9 4
405	33 12 5	25 4 7	23 17 5	455	35 17 11	26 18 7	25 9 8
406	33 13 5	25 5 0	23 17 10	456	35 18 11	26 19 1	25 10 4
407	33 14 1	25 5 5	23 18 8	457	35 19 8	27 0 1	25 11 4
408	33 15 1	25 6 5	23 19 2	458	36 0 11	27 0 6	25 11 8
409	33 15 11	25 6 11	24 0 1	459	36 1 7	27 1 0	25 12 6
410	33 17 0	25 7 11	24 0 6	460	36 2 6	27 1 11	25 13 1
411	33 17 7	25 8 2	24 1 0	461	36 3 6	27 2 6	25 13 11
412	33 18 8	25 9 2	24 1 8	462	36 4 5	27 3 6	25 14 5
413	33 19 8	25 9 8	24 2 5	463	36 5 2	27 3 11	25 14 11
414	34 0 8	25 10 4	24 3 4	464	36 6 2	27 4 11	25 15 7
415	34 1 6	25 11 4	24 3 8	465	36 7 2	27 5 5	25 16 4
416	34 2 5	25 11 8	24 4 5	466	36 8 4	27 6 1	25 16 11
417	34 3 4	25 12 8	24 5 1	467	36 8 10	27 6 10	25 17 7
418	34 4 4	25 13 2	24 5 11	468	36 10 0	27 7 4	25 18 4
419	34 5 0	25 13 11	24 6 5	469	36 10 10	27 8 2	25 19 0
420	34 6 1	25 14 7	24 7 1	470	36 11 10	27 8 10	25 19 10
421	34 7 0	25 15 2	24 7 10	471	36 12 6	27 9 5	26 0 1
422	34 8 0	25 16 0	24 8 4	472	36 13 6	27 10 2	26 0 11
423	34 8 8	25 16 6	24 8 8	473	36 14 5	27 10 10	26 1 7
424	34 9 7	25 17 4	24 9 7	474	36 15 5	27 11 7	26 2 2
425	34 10 6	25 18 0	24 10 1	475	36 16 1	27 12 2	26 2 11
426	34 11 7	25 18 10	24 11 0	476	36 17 1	27 12 11	26 3 4
427	34 12 4	25 19 4	24 11 5	477	36 18 2	27 13 8	26 4 0
428	34 13 4	26 0 0	24 12 2	478	36 19 1	27 14 1	26 4 11
429	34 14 5	26 0 8	24 12 11	479	37 0 0	27 15 0	26 5 4
430	34 15 1	26 1 6	24 13 7	480	37 0 8	27 15 8	26 6 1
431	34 16 1	26 2 2	24 14 1	481	37 1 11	27 16 6	26 6 8
432	34 16 11	26 2 11	24 14 10	482	37 2 10	27 17 2	26 7 6
433	34 17 11	26 3 7	24 15 4	483	37 3 6	27 17 10	26 7 11
434	34 19 0	26 4 5	24 16 2	484	37 4 8	27 18 6	26 8 7
435	34 19 7	26 4 11	24 16 5	485	37 5 5	27 19 0	26 9 6
436	35 0 8	26 5 7	24 17 4	486	37 6 6	28 0 0	26 9 11
437	35 1 6	26 6 4	24 18 4	487	37 7 0	28 0 5	26 10 4
438	35 2 8	26 7 0	24 19 0	488	37 8 2	28 1 1	26 11 2
439	35 3 2	26 7 6	24 19 6	489	37 8 11	28 1 10	26 11 11
440	35 4 4	26 8 2	25 0 2	490	37 10 1	28 2 6	26 12 7
441	35 5 2	26 9 1	25 0 7	491	37 10 6	28 3 1	26 13 0
442	35 6 4	26 9 7	25 1 6	492	37 11 10	28 3 10	26 13 10
443	35 6 11	26 10 2	25 1 11	493	37 12 10	28 4 7	26 14 6
444	35 8 0	26 10 11	25 2 11	494	37 13 7	28 5 2	26 15 5
445	35 9 0	26 11 7	25 3 4	495	37 14 6	28 6 0	26 15 10
446	35 9 10	26 12 5	25 4 1	496	37 15 5	28 6 8	26 16 4
447	35 10 10	26 13 0	25 4 8	497	37 16 5	28 7 7	26 17 0
448	35 11 6	26 13 10	25 5 4	498	37 17 5	28 8 1	26 17 7
449	35 12 7	26 14 6	25 6 2	499	37 18 1	28 8 6	26 18 5
450	35 13 7	26 15 5	25 6 8	500	37 19 1	28 9 5	26 18 11

Live Stock Mileage Rates Per 4-Wheeled Truck—*continued.*

Miles	Full Rate	Store Rate	One Tier Rate	Miles	Full Rate	Store Rate	One Tier Rate
	£ s. d.	£ s. d.	£ s. d.		£ s. d.	£ s. d.	£ s. d.
501	38 0 1	28 10 1	26 19 7	551	40 5 7	30 4 1	28 11 10
502	38 1 0	28 10 11	27 0 5	552	40 6 6	30 4 11	28 12 7
503	38 1 8	28 11 4	27 0 10	553	40 7 5	30 5 7	28 13 2
504	38 2 8	28 11 11	27 1 10	554	40 8 6	30 6 6	28 14 1
505	38 3 8	28 12 8	27 2 1	555	40 9 1	30 6 11	28 14 5
506	38 4 8	28 13 5	27 2 7	556	40 10 2	30 7 7	28 15 4
507	38 5 5	28 14 1	27 3 6	557	40 11 4	30 8 4	28 16 0
508	38 6 5	28 14 8	27 3 11	558	40 12 0	30 9 1	28 16 6
509	38 7 4	28 15 5	27 4 11	559	40 13 0	30 9 10	28 17 5
510	38 8 4	28 16 1	27 5 5	560	40 13 8	30 10 5	28 17 10
511	38 9 0	28 16 11	27 6 0	561	40 14 10	30 11 5	28 18 4
512	38 10 0	28 17 7	27 6 10	562	40 15 11	30 11 11	28 19 2
513	38 11 0	28 18 4	27 7 4	563	40 16 6	30 12 6	28 19 6
514	38 12 1	28 19 2	27 8 2	564	40 17 7	30 13 2	29 0 4
515	38 12 8	28 19 6	27 8 8	565	40 18 4	30 13 11	29 1 1
516	38 13 11	29 0 4	27 9 1	566	40 19 7	30 14 8	29 2 1
517	38 14 7	29 1 0	27 9 11	567	41 0 1	30 15 0	29 2 6
518	38 15 8	29 1 8	27 10 7	568	41 1 2	30 15 11	29 3 2
519	38 16 4	29 2 5	27 11 0	569	41 2 1	30 16 7	29 3 10
520	38 17 5	29 3 0	27 11 11	570	41 3 2	30 17 4	29 4 6
521	38 18 1	29 3 10	27 12 5	571	41 3 10	30 18 0	29 4 11
522	38 19 2	29 4 6	27 13 4	572	41 4 11	30 18 6	29 5 10
523	38 19 10	29 4 11	27 13 10	573	41 5 11	30 19 2	29 6 4
524	39 0 11	29 5 10	27 14 6	574	41 6 8	31 0 1	29 7 2
525	39 2 0	29 6 4	27 15 5	575	41 7 6	31 0 7	29 7 8
526	39 2 8	29 7 2	27 15 8	576	41 8 5	31 1 7	29 8 4
527	39 3 8	29 7 11	27 16 6	577	41 9 6	31 2 2	29 9 4
528	39 4 5	29 8 8	27 17 1	578	41 10 6	31 3 1	29 9 8
529	39 5 8	29 9 5	27 17 10	579	41 11 2	31 3 5	29 10 5
530	39 6 8	29 10 0	27 18 6	580	41 12 2	31 4 0	29 10 11
531	39 7 5	29 10 8	27 18 11	581	41 13 1	31 5 0	29 11 5
532	39 8 4	29 11 2	27 19 7	582	41 14 1	31 5 6	29 12 4
533	39 9 4	29 12 2	28 0 5	583	41 14 10	31 6 2	29 12 10
534	39 10 4	29 12 10	28 1 1	584	41 15 10	31 6 10	29 13 6
535	39 11 0	29 13 0	28 1 8	585	41 16 7	31 7 10	29 14 4
536	39 12 0	29 14 0	28 2 2	586	41 17 10	31 8 4	29 14 8
537	39 12 10	29 14 7	28 3 1	587	41 18 4	31 8 7	29 15 7
538	39 13 11	29 15 7	28 3 6	588	41 19 5	31 9 7	29 16 1
539	39 14 6	29 16 0	28 4 2	589	42 0 5	31 10 1	29 17 0
540	39 15 7	29 16 10	28 4 10	590	42 1 4	31 11 1	29 17 5
541	39 16 7	29 17 5	28 5 6	591	42 2 1	31 11 8	29 17 11
542	39 17 7	29 17 11	28 6 4	592	42 2 11	31 12 6	29 18 7
543	39 18 4	29 18 11	28 6 8	593	42 4 1	31 13 2	29 19 4
544	39 19 4	29 19 6	28 7 7	594	42 5 2	31 13 10	30 0 2
545	40 0 2	30 0 4	28 8 1	595	42 5 8	31 14 6	30 0 7
546	40 1 2	30 1 0	28 8 10	596	42 6 11	31 15 0	30 1 2
547	40 1 11	30 1 7	28 9 5	597	42 7 7	31 15 11	30 2 0
548	40 2 11	30 2 4	28 10 1	598	42 8 8	31 16 7	30 2 8
549	40 3 11	30 3 0	28 10 11	599	42 9 5	31 17 0	30 3 4
550	40 4 11	30 3 8	28 11 4	600	42 10 5	31 17 10	30 4 0

LIVE STOCK—*continued.*

SPECIAL RATES

1. **For Distances up to 23 Miles.**—The following special charges shall be operative in respect of live stock conveyed for distances up to 23 miles, and which are chargeable at half full rate, half full rate less 10 per cent., two-thirds full rate or two-thirds full rate less 10 per cent., as indicated on pages 207 and 208.

Distance	For Consignments Chargeable at—				
	Half Full Rate less 10 per cent.	Half Full Rate	Two-thirds Full Rate less 10 per cent.	Two-thirds Full Rate	
—	Pigs	Other Stock	Pigs	Sheep, Calves, Mixed Sheep and Calves	Other Stock
Miles	s. d.	s. d.	s. d.	s. d.	s. d.
1–18	27 7	30 8	35 7	37 2	39 6
19	28 10	32 1	35 7	38 7	39 6
20	29 8	32 10	36 8	38 7	40 7
21	30 8	34 0	38 7	40 7	42 10
22	31 11	35 4	40 7	42 10	45 1
23	31 11	35 4	41 7	44 0	45 11

24 miles and over on the basis indicated on pages 207 and 208.

If more than 15 and less than 25 sheep, calves and /or pigs, the special charge shall be as for 15 head plus $\frac{9}{10}$d. per mile (minimum 18 miles) for each animal in excess of 15 if this be cheaper than the special charge for 25 head.

If more than 25 pigs or more than 25 head in a mixed lot of pigs and calves ; or pigs and sheep ; or pigs, calves and sheep ; the special charge shall be as for 25 head plus $\frac{9}{10}$d. per mile (minimum 18 miles) for each animal in excess of 25 if this be cheaper than the ordinary rate for for 26 and over.

2. **Live Stock** in crates carried by Goods or Mixed Trains shall be charged goods mileage rate as follows :—

Crates of—	Classification to be charged Subject to the "Smalls" Minimum Tariff
Cattle, horses, sheep, bears, dogs, goats, hares, kangaroos, monkeys, wallabies, and wombats (any distance)	Class "2" less 10 per cent.
Pigs (1 to 249 miles)	Class "2" less 20 per cent. (not to exceed 250 miles rate)
Pigs (250 miles and over)	Class "2" less $33\frac{1}{3}$ per cent.

Crates of live stock loaded in the same truck with "loose" stock charged less than the rate for a full truck, shall be separately charged for at the above-mentioned rates or, if cheaper, the charge shall be computed at the rate for the total number of animals both loose and in the crates, the rate for a full truck not to be exceeded.

3. **Stud Sheep Purchased at Agricultural or Pastoral Shows** by exhibitors thereat, and forwarded from the Show uncrated in the same truck as the exhibitor's returned unsold exhibits of sheep to the original forwarding station of the latter shall be charged at Class "2" mileage rate less 10 per cent. on the basis of 3 cwt. per animal and subject to the "Smalls" Minimum Tariff where cheaper than the ordinary rates.

4. **Animals in Horse Boxes.**—Animals carried by Goods trains in Horse Boxes will be charged the same rates as by Passenger trains.

LIVE STOCK—SPECIAL RATES—*continued.*

5. **Horses for certain events.**—Hunters' horses taking part in hunting meetings during the season from May to the end of September ; polo ponies taking part in polo matches ; horses forwarded to parades, gymkhanas or race meetings ; Police horses to and from Public Demonstrations, shall be charged at the full ordinary rates on the forward journey, but, on production of Form P.L. 11 suitably amended and signed by the Master of the Hunt, the Secretary of the Polo Club, the Organizer of the Parade or Gymkhana, Secretary of the Race Club or Police Department respectively, shall be charged half ordinary rate for the homeward journey when returning to the station from which they were originally forwarded.

6. **Horses forwarded from Newmarket** to the Victoria Dock, Port Melbourne, and Williamstown Piers, in consignments of six or not exceeding ten trucks shall be charged £13 18s. 10d for the lot, and each additional truck shall be charged £1 8s. 7d. If forwarded in consignments of less than six trucks, the charge shall be £2 18s. 5d. per truck, subject to maximum charge of £13 18s. 10d. Charges shall be prepaid.

7. **Horses in truck loads from Port Melbourne** to ship's side at the Pier shall be charged £1 8s. 7d. per truck. Charges shall be prepaid.

8. **Horses and other Live Stock** forwarded from country stations to the Victoria Dock, Port Melbourne Pier, or Williamstown Pier, for shipment, and discharged from the trucks at the ship's side, shall be charged the rates to Melbourne, Port Melbourne Pier, or Williamstown Pier respectively. Charges shall be prepaid.

9. **Sheep, Lambs, Cattle and Pigs for Export.**—The freight charges on—

(*a*) Sheep and lambs in truck loads carried on all days except Monday.

(*b*) Cattle in truck loads carried on all days except Tuesday for treatment and export as chilled beef in quarters ;

(*c*) Pigs which have been charged at full rate plus 20 per cent, or at full rate, consigned to recognized exporters and subsequently exported, may be rebated to the extent of 10 per cent. on application being submitted to the Live Stock Agent within six (6) months from the date the consignments are forwarded. Statutory declaration that the identical animals shown in claim for rebates were exported and satisfactory proof of export must be furnished with the rebate claim.

10. **Brood Mares and Stallions,** also Grooms', Ponies accompanying Stallions travelling for breeding purposes during the months of July, August, September, October, November, December, January and February, shall be charged full ordinary rates for the outward journey, whether sent in horse-boxes or live stock trucks but shall be charged half rate for the homeward journey when returning to the station from which they were originally forwarded on condition that before commencing the outward journey, and previous to commencing the homeward journey, a certificate (Form G.F. 7) shall be furnished and that more vehicles are not used on the return journey than on the forward journey. Entires and Grooms' Ponies may be allowed to break their journey. The homeward journey must be completed before the end of February.

11. **Boars, Sows, Rams, Ewes** (in trucks or in crates), **Bulls or Cows** travelling for breeding purposes at any time, shall be charged full ordinary rate for the outward journey, but shall be charged half rate for the homeward journey when returning to the station from which they were originally forwarded on condition that before commencing the outward journey, and previous to commencing the homeward journey, a certificate (Form G.F. 7) shall be furnished, and that more vehicles are not used on the return journey than on the forward journey. The homeward journey must be completed within three months of the date of the outward journey.

12. **Live Stock for Young Farmers' or Calf Clubs.**—Live stock in crates or trucks forwarded to bona fide members of Young Farmers' or Calf Clubs, or forwarded to Agricultrual and Pastoral Shows for exhibition by members of Young Farmers' or Calf Clubs shall be charged at half the ordinary rate, provided that a certificate on Form G.F. 7a, properly filled in and signed by the Secretary of the Young Farmers' or Calf Clubs concerned, is lodged when the Live Stock is consigned. Certificate must be attached to the waybill as the authority for charging half rate.

LIVE STOCK—SPECIAL RATES—*continued.*

13. **Sheep and Cattle from Northern Districts to Bendigo Sales, Reconsigned to Newmarket.**—Sheep and cattle forwarded to the Bendigo market and not sold, if reconsigned from Bendigo to Newmarket on the day of the sale shall be charged at through mileage rates from the original despatching station to Newmarket plus 37s. 11d. per truck. The consignment note must be endorsed by the Bendigo Selling Agent, certifying that the stock were unsold.

14. **Wodonga-Albury.**—Live stock forwarded between Victoria and New South Wales via Albury shall be charged as follows :—

Victoria.—At the Victorian rates for the mileage to or from Wodonga.

New South Wales.—At the New South Wales rates for the mileage to or from Wodonga, provided that in respect of live stock forwarded from Victorian stations to Albury for local requirements and which is placed at the Albury Dock for unloading, the charges shall be computed at the Victorian rates for the mileage to Albury.

15. **Hire of Stock Yards.**—(*a*) Sales of Live Stock may be conducted in Railway Stockyards at towns where there are no private or municipal saleyards, provided that trucking arrangements or railway traffic are not interfered with, on payment of the charges shown in clause (*c*). The yards must not be occupied for a longer period than 24 hours.

(*b*) Travelling mobs of cattle, horses, and sheep may be camped or drafted in Railway yards entirely at the risk of the owner, provided trucking arrangements or other railway traffic is not interfered with, on payment of the charges shown in clause (*c*).

(*c*) The following charges shall be imposed.

For sale of Live Stock ...	£2 18 0	Per day of 24 hours or part thereof.
For camping or drafting travelling stock ...	1 9 0	

16. **Hurdles for Separating Live Stock.**—No charge shall be made for carriage of hurdles used by consignors in cattle trucks to separate animals, but for the return of such hurdles to the original forwarding station the charges shall be—

	Per Hurdle s.	d.
Not over 50 miles	2	4
51 to 101 miles	4	0
102 to 150 miles	5	8
Over 150 miles	7	8

17. **Stock to Agricultural Shows.** (See Agricultural Shows), page 25.

18. **Concessions to Owners or Their Drovers.** (See Passenger Fares Book.)

19. **Live Stock Rates per Passenger Trains.** (See Passenger Fares Book.)

20. **Circus Wild Animals,** such as Elephants, Lions and Tigers. (See Circus Material), page 41.

Store Stock.

1. In the construction of this Regulation, the following expressions shall have the meanings hereby respectively assigned to them, namely :—

"**Store Stock**" shall mean cattle (including calves) or sheep consigned with the Commissioners for carriage which require to be grazed for a period of at least eight weeks from the date of arrival at the destination station, and which are not in a fit condition to permit of their being sold or offered for sale as fat stock.

Provided that—

(*a*) any truck load of cattle containing one or more bulls, dairy cows in milk, or dairy springers which calve within eight weeks from date of arrival at the destination station ;

(*b*) any truck load of sheep containing more than ten rams;

shall not be deemed to come within the meaning of store stock.

"**Mileage Store Rate**" shall mean the rate prescribed as "Store Rate" on pages 210 to 215.

"**Owner**" shall mean the actual owner at the time of the delivery of the stock at the destination station.

"**Up**" **Journey** shall mean a journey in the direction of Melbourne, and "**Down**" **Journey** shall mean a journey in a direction away from Melbourne, as indicated in each case in the Commissioners' current working time-table, provided that, notwithstanding the foregoing definitions and anything contained in the said time-table,

"**Down**" **Journey** shall include journeys as indicated hereunder :—

Between Hamilton and East Natimuk,
" Hamilton and Koroit,
" Ararat and Maryborough,
" Inglewood and Dunolly.

2. **Reduced Rates Applicable to Store Stock.**—Subject to the provisions and conditions of any By-law of the Commissioners for the time being in force, store stock may, in the absolute discretion of the Commissioners, be carried at the following reduced rates :—

Number of Trucks in Consignment. (See Note (a).)	*To be Charged per Four-wheeled Truck.*
1 to 14, any distance	... Mileage Store Rate.
15 and over, under 100 miles ...	... Mileage Store Rate. (100 Mile Rate to be maximum.)
15 to 34, minimum distance 100 miles	... 1s. 3d. per mile (See Note (*b*)). (Mileage Store Rate to be maximum.)
35 to 49, minimum distance 100 miles	... 1s. 1½d. per mile (See Note (*b*)). (Mileage Store Rate to be maximum.)
50 and over, minimum distance 100 miles	... 10¾d. per mile. (See Note (*b*)). (Mileage Store Rate to be maximum.)

NOTES.

(*a*) If cheaper, the minimum of next lower scale is to be charged, i.e. :—

1 to 14 trucks at the charge for 15 trucks.
15 to 34 trucks at the charge for 35 trucks.
35 to 49 trucks at the charge for 50 trucks.

(*b*) In ascertaining the rate per truck, fractions of one penny not exceeding one halfpenny shall not be charged for, but one halfpenny and fractions of one penny exceeding one halfpenny shall be charged as one penny.

Subject to the prior authority of the Live Stock Agent being obtained, a consignment totalling fifteen trucks and over between two or more stations in one district and one or more stations in another district, may be charged the appropriate store rate.

Provided that the abovementioned rates shall not be applicable to—

(i) Any consignment of store stock of less than a full truck lot.

(ii) Any live stock consigned for carriage to Newmarket.

(iii) Any store stock carried on an "up" journey or on an "up" and "down" journey as set out in clause 5 hereof, which are sold or offered for sale within eight weeks from the date of arrival at the destination station.

(iv) Any truck load of sheep or cattle containing one or more animals which are intended to be, or are, slaughtered within eight weeks from the date of arrival at the destination station.

(v) Consignments of pigs in store condition (special provision being made therefor in clause 7, page 221.)

Provided also that, in any case where an employee receiving or delivering any live stock consigned as store stock, is of opinion that such live stock is not store stock, the ordinary live stock rate shall be charged ; and the employee shall submit a report to the Live Stock Agent who may, in his discretion, on production by the owner of satisfactory proof, subsequently authorize the carriage of the consignment at store rate, and make a rebate accordingly.

3. **Store Stock Conveyed on "Down" Journey.**—The reduced rates stipulated in clause 2 shall apply at the time of carriage to all store stock consigned on a "Down" journey, provided that the following conditions are complied with :—

(*a*) The Consignor shall deliver to an authorised employee of the Commissioners at the time of making such consignment, a declaration in the prescribed form (G.F. 144, page 229) declaring *inter alia* that the stock consigned are store stock.

(*b*) The owner shall deliver to an authorized employee of the Commissioners at the time of delivery of such consignment, a declaration in the prescribed form (G.F. 145, page 229), declaring *inter alia* that such stock are store stock, and that the said stock will be grazed for a period of at least eight weeks from the date of arrival at the destination station, and that during the said period of eight weeks none of such stock will be slaughtered.

(*c*) Where the owner is both consignor and consignee of the store stock, he shall only be required to deliver to an authorized employee of the Commissioners a declaration (Form G.F. 145, page 229), as prescribed in sub-clause (*b*).

(*d*) Store stock consigned on a "Down" journey at the reduced rates stipulated in clause 2 may be sold within a period of eight weeks from the date of arrival at the destination station, provided that the said stock are sold as store stock, and are to be grazed by the purchaser for the said period of eight weeks after such date of arrival.

(*e*) Any owner consigning at the one time both store stock and live stock other than store stock on a "Down" journey, may consign the said store stock at the reduced rates applicable thereto, provided that such store stock are consigned separately and comprise at least one full truck load.

(*f*) "Down" journey consignments for an intersystem destination (including Tocumwal) shall be charged store rate at the time of carriage only if the owner's declaration (G.F. 145) be supplied when the stock is consigned at the forwarding station.

4. **Store Stock Conveyed from One Line to Another, comprising both an "Up" and a "Down" Journey.**—(*a*) Any consignment of store stock consigned to a station on another line necessitating both an "Up" and "Down" journey, shall be regarded as a "Down" journey consignment, and shall be subject to the reduced rates and conditions applicable thereto, as set out in clauses 2 and 3, provided that the mileage store rate for the through journey is not less than the ordinary live stock rate for the "Up" portion of such journey.

(*b*) Any consignment of store stock consigned to a station on another line necessitating both an "Up" and "Down" journey, for which the mileage store rate for the through journey is less than the ordinary rate for the "Up" portion of the journey, shall be regarded as a "Down" journey consignment, and shall be subject to the conditions applicable thereto as set out in clauses 2 and 3, with the exception that the reduced charge at time of carriage shall be the ordinary rate for the "Up" portion of the journey ; in such cases, a rebate of the difference between through store rate and the ordinary rate for the "Up" portion of the journey may be authorized, provided the "Up" journey conditions as per clause 5 be subsequently fulfilled.

STORE STOCK—*continued.*

5. **Store Stock Conveyed on " Up " Journey and Certain Consignments necessitating both an " Up " and " Down " Journey.**—(*a*) All store stock consigned on an " Up " journey, or (except as provided in clause 4) on an " Up " and " Down " journey, shall at the time of carriage be charged the ordinary live stock rate, but the Live Stock Agent may, in his discretion, subsequently authorize carriage of any such consignment at store rate, and make the necessary rebate in that connexion, provided that, within six months from the date of the delivery of such consignment to the consignee or to his agent, the owner of such stock shall forward to the Live Stock Agent an application for rebate and a statutory declaration in the prescribed combined form (G.F. 146, page 230), declaring *inter alia*—

(i) That the live stock in respect of which such application is made was store stock at the time of consigning with the Commissioners.

(ii) That at the time of the arrival of such store stock at the destination station, and for a period of at least eight weeks thereafter, he was the owner of the said store stock.

(iii) That for a period of at least eight weeks from the arrival of such store stock at the destination station, the said store stock was grazed by him, and that during such period none of the stock was sold or offered for sale or slaughtered.

(*b*) Where a consignment comprising both store stock and live stock other than store stock has been carried on an " Up " journey, or (except as provided in clause 4 hereof) on an " Up " and " Down " journey, the owner may subsequently, at the discretion of the Live Stock Agent, be granted a rebate in respect of the store stock in such consignment, provided that the said store stock comprise at least one full truck load and that the owner makes an application for rebate in the manner prescribed in sub-clause (*a*).

6. **Conditions under which Horses may be Regarded as Store Stock.**—Horses (stallions excepted) which are not broken in and which are consigned with and carried by the Commissioners in order that they may be transferred to new pastures or used for stocking country, and horses which are in such poor condition that they require to be depastured in order that they may regain condition, and which are consigned with and carried by the Commissioners for that purpose, shall be deemed to be store stock and may, subject to the prior consent of the Live Stock Agent, be carried at the reduced rates specified in clause 2, provided that none of such horses is allowed to be worked for a period of at least eight weeks after arrival at the destination station, and that all the appropriate conditions in the foregoing clauses, with any necessary adaptations thereto, are complied with.

7. **Consignment of Pigs in Store Condition** which are consigned with the Commissioners for carriage to enable the said stock to be fattened for a period of at least eight weeks after arrival at the destination station may, subject to the prior consent of the Live Stock Agent, be carried at a special store rate of 25 per cent. less than the ordinary live stock rate prescribed for the carriage of such stock, provided that each consignment comprises at least one tier lot and that all the appropriate conditions in the foregoing clauses, with any necessary adaptations thereto, are complied with.

Starving Stock and Fodder for Starving Stock

In any instance where reduced rates below the ordinary rates are granted for the carriage of starving stock or for fodder for starving stock such reduced rates shall apply only where any person firm or body corporate or unincorporate who obtained or seeks to obtain the benefit of any such rates has used the railways and no other means whatsoever for the transportation of goods and live stock in cases where railway facilities were available for such transportation. If at any time the Chief Commercial Manager or such officer acting as aforesaid is satisfied upon reasonable inquiry that any goods or live stock have been transported by road or by any method of transportation other than by rail by for or on behalf of any such person firm or body corporate or unincorporate when railway facilities were available for such transportation the Chief Commercial Manager or such officer acting as aforesaid may direct that from and after the date of such direction the rates to be charged and paid for the carriage of any such starving stock or fodder consigned to or on behalf of or by or on behalf of any such person, firm or body corporate or unincorporate shall be the ordinary rates for live stock and Class A rate for fodder.

"A"—LIVE STOCK.

1. **Forward Journey.**—Where the natural feed or water has failed through drought, fire, flood or ravages of grasshoppers on any property, starving cattle, horses or sheep in truck load consignments forwarded from such property for agistment shall, at the time of carriage, be charged at the concession rates shown in clause 2, subject to the conditions and the production of the Statutory Declaration prescribed in clause 3.

2. The concession rates for the carriage of such starving stock shall be :—

1 to 77 miles	Store rate.
78 to 153 miles	£7 11s. 10d. per 4-wheeled truck.
Over 153 miles	Half ordinary rate.

Large lots forwarded a minimum distance of 100 miles shall be charged the special Store Rate shown below if cheaper than the above rates :—

Minimum Number of Trucks	To be Charged per Truck per Mile
	s. d.
15	1 3
35	1 1½
50	0 10¾

3. A Statutory Declaration shall be signed by and submitted by the owner stating :—

(*a*) That the stock is the property of the owner at the date of trucking and has been his property for the preceding three months during which period, except when travelling for agistment, the stock has been upon the property owned and /or occupied by the owner.

(*b*) That the moving of the stock is necessary owing to the failure of the natural feed or water by drought, fire, flood or ravages of grasshoppers on the owner's property.

(*c*) That the stock is being despatched for agistment and will not be sold or offered for sale within a period of eight weeks after arrival at the destination station.

(*d*) That, in respect of starving stock moved from any property in New South Wales, he undertakes for a period of twelve (12) months from the date of trucking, to send and receive by rail all his goods (including wool) and live stock (droving of live stock excepted) when such commodities are forwarded to or received from Victoria and railway facilities are available for such transportation, and further agrees that if during the currency of this undertaking any goods (including wool) or live stock (droving of live stock excepted) are transported other than by rail by for or on his behalf when railway facilities are available for such transportation, he shall refund to the Commissioners the difference between the amount which would have been payable for the carriage of the said starving stock at the Commissioners' ordinary live stock rates and the amount which was paid at the said concession rate for such carriage.

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STARVING STOCK, Etc.—*continued*

(e) The undertaking prescribed in clause (*d*) is not required in respect of starving stock *moved from properties in Victoria. The reduced rates in such instances are allowed* under the conditions provided above.

(*f*) The Statutory Declaration must be submitted by the applicant to the District Lands Department Inspector so far as consignments to or from Victorian stations are concerned and Live Stock Inspector of the District in respect of consignments forwarded to or from New South Wales, from where the stock is forwarded, to certify in the space provided thereon that the particulars shown on the Statutory Declaration are to the best of his knowledge and belief correct.

(*g*) "Forward journey" Form G.F. 141 shall be used for consignments from New South Wales properties and "forward journey" Form G.F. 140 for consignments from Victorian properties.

4. The Commissioners may at their absolute discretion declare that a condition of drought exists generally in a particular district, and on such decision being arrived at, the concession rates may be applied when the stock is trucked, on production of the prescribed Statutory Declaration which need not be certified to by the District Lands Department Inspector so far as consignments to or from Victorian stations are concerned or Live Stock Inspector of the District in respect of consignments forwarded to or from New South Wales, as required in sub-clause (*f*), of clause 3.

5. In the event of any of the stock carried at the concession rate being sold within the specified period of eight (8) weeks after arrival at the destination station, the owner shall notify the Commissioners and pay to them in respect of the stock so sold, the difference between the amount of the freight actually paid and the amount which would have been payable had the freight on such stock been charged at ordinary live stock rate.

6. **Return journey.**—Cattle, horses, or sheep removed by rail on the forward journey under the conditions specified in clauses 1 and 3 and charged at concession rates specified in clause 2, shall, if such stock have not been sold, be carried on the return journey to the owner's property from which they were sent on the forward journey, at the concession rates specified in clause 2 provided that the owner furnishes a Statutory Declaration ("return journey" Form G.F. 142, if declared in Victoria and "return journey" Form G.F. 143, if declared in New South Wales) that the animals being returned are the same animals (and the progeny* thereof born on relief country) as were carried on the forward journey.

7. Stock must be returned to the original pastures not more than twelve (12) months after they have been despatched therefrom, otherwise the concession rate on the return journey may be disallowed.

8. Stock which was originally sent on the forward journey by rail and which is returned by rail to the owner's property over a route or for a mileage other than that used on the forward journey, may, subject to the prescribed conditions being fulfilled, and with the approval of the Live Stock Agent, be allowed the concession rate prescribed herein on the return journey for an equivalent mileage to that for which the stock was forwarded on the forward journey.

9. Stock purchased at any time within three (3) months prior to the day of trucking on the forward journey or sold or offered for sale at any time within eight (8) weeks after arrival at destination station, shall not receive the benefit of the concession rate.

10. If, as the result of rainfall, the natural feed on the original pastures of the stock on the owner's property be restored within eight (8) weeks after the stock is removed for agistment such stock may be returned to the original pastures at any time within the specified eight (8) weeks and be eligible for the concession rate on the forward and return journeys.

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* The number of animals born on relief country must be separately shown on the Consignment Note.

STARVING STOCK, Etc.—*continued.*

"B"—FODDER.

1. In bona fide cases in which any description of fodder (including carrots, turnips, potatoes, mangels, molasses, linseed meal and pumpkins) are being forwarded by rail solely for the purpose of keeping alive starving cattle, horses and sheep, a concession rate of 20 per cent. less than the ordinary tariff rate for consignments of one (1) ton and upwards shall be allowed on such fodder subject to the following conditions :—

2. The owner or manager of the property affected shall lodge with the consignment note or at time of delivery of the fodder at the destination station, a Statutory Declaration stating :—

(*a*) That the natural feed has failed through drought, fire, flood or ravages of grasshoppers on his property for which the fodder is consigned.

(*b*) That such fodder carried by rail will be used solely for feeding his starving cattle, horses, or sheep that would otherwise perish through starvation.

(*c*) That in respect of fodder required to feed stock on any property in New South Wales he undertakes for a period of twelve (12) months from the date the fodder is carried to send and receive by rail all his goods (including wool) and live stock (droving of live stock excepted) whenever such commodities are forwarded to or received from Victoria and railway facilities are available for such transportation, and further agrees that if during the currency of this undertaking any goods (including wool) or live stock (droving of live stock excepted) are transported other than by rail by for or on his behalf when railway facilities are available for such transportation, he shall refund to the Commissioners the difference between the amount which would have been payable for the carriage of the said fodder at the Commissioners' ordinary prescribed rates and the amount which was paid at the said concession rate for such carriage.

(*d*) The undertaking prescribed in clause (*c*) is not required in respect of carriage of fodder to feed stock on properties in Victoria.

(*e*) Form G.F. 139 shall be used for consignments of fodder to feed stock on New South Wales properties and Form G.F. 138 for consignments of fodder for Victorian properties.

(*f*) The Statutory Declaration must be submitted by the applicant to the District Lands Department Inspector so far as consignments to or from Victorian stations are concerned and Live Stock Inspector of the District in respect of consignments forwarded to or from New South Wales, to which the fodder is forwarded to certify in the space provided thereon that drought conditions prevail on the owner's property at the date of the consignment of the fodder.

3. Fodder consigned to stations situated within 30 miles of Melbourne shall be charged for at the ordinary classification rates. Any application for reduced rates on such consignments shall be submitted with Form G.F. 138 completed, to the Chief Commercial Manager within six (6) months from the date the consignments were forwarded.

4. The Commissioners may at their absolute discretion declare that a condition of drought exists generally in a particular district, and on such decision being arrived at the concession rate may be applied to fodder on production of the prescribed Statutory Declaration form, which need not be certified to by the District Lands Department Inspector so far as consignments to or from Victorian stations are concerned or Live Stock Inspector of the District in respect of consignments forwarded to or from New South Wales as required in sub-clause (f), clause (2).

Copies of Forms Used in Connection with the Carriage of Goods and Livestock.

G.F.1

VICTORIAN RAILWAYS.

CONSIGNMENT NOTE.

THE VICTORIAN RAILWAYS COMMISSIONERS hereby give notice that, with certain exceptions set out in the Goods Rates Book, a consignor may consign goods either—

(1) at COMMISSIONERS' RISK, whereby the Commissioners are subject to the ordinary liability of a common carrier, or

(2) at OWNER'S RISK, whereby in consideration of a reduced rate, or the fact that the goods are not properly packed, the consignor agrees to relieve the Commissioners from all liability for loss injury damage deviation misdelivery delay or detention except upon proof that the same arose from the wilful misconduct of the Commissioners' servants.

PROVIDED THAT notwithstanding a request to the contrary expressed by the consignor—

(A) OWNER'S RISK conditions shall apply to goods which are not properly packed, and

(B) OWNER'S RISK conditions shall apply to goods consigned from or to an unattended station platform or siding, station in charge of a caretaker, or private siding, and the Commissioners' liability after carriage shall absolutely cease when the goods are placed or left thereat.

To the Victorian Railways Commissioners

.....Station.....19.....

Please forward the undermentioned goods, subject to the provisions of the Railways Acts and the By-laws Regulations and Conditions of the Commissioners for the carriage of goods to ..... at ..... Railway Station at "OWNER'S RISK *"
(BLOCK LETTERS) (BLOCK LETTERS)

If the words "AT OWNER'S RISK" (where indicated by *) are struck out the consignor requires the goods to be carried at Commissioners' Risk, provided the conditions applicable to such risk are complied with, otherwise they are to be carried at OWNER'S RISK.

..... Signature of Consignor or Agent.
..... Address of Consignor or Agent.

Truck No.	No. and type of Packages ie cases, crates, cartons, bundles, bags etc.	Description of Goods	Brands and Numbers	Weight				Freight payable by
				Tons	Cwt.	Qrs.	Lbs.	

Loaded by { Consignor. / Commissioners. } How weight ascertained..... .....
Signature of Railway Employe receiving Goods

Time..... Date.....

FORMS—*continued.*

G.F. 104

VICTORIAN RAILWAYS.

Declaration for Old Machinery

I.....

*Insert Business

of .....

in the State of Victoria (*).....do solemnly and sincerely declare—

†Insert whether old Mining Machinery, Old Machinery, not Mining, or old Boiler as the case may be.

That the undermentioned (†).....has been in use for a period of not less than three years, viz. :—

(‡) .....

‡Detail the Machinery, particulars of which must correspond with the Consignment Note in every respect.

.....

.....

.....

.....

.....

.....

.....

.....

.....

And I make this solemn declaration conscientiously believing the same to be true and by virtue of the provisions of an Act of Parliament of Victoria rendering persons making a false declaration punishable for wilful and corrupt perjury.

Declared at

in the State of Victoria this
day of One thousand nine
hundred and
Before me—
Justice of the Peace.

T.N.

G.F. 6

VICTORIAN RAILWAYS.

GUARANTEE TO DISCHARGE HEAVY GOODS AT STATION WHERE THERE IS NO CRANE.

The following Goods having this day been consigned by_____

to_____ (Consignee) at_____

station, at which there are no facilities for discharging heavy articles—

I hereby guarantee that in the event of these goods being forwarded by rail they will be
We

promptly discharged from truck by consignee on arrival at above-named destination without damage to the property of the Commissioners.

_____Signature
_____Station
_____Date.

FORMS—*continued.*

VICTORIAN RAILWAYS.

DECLARATION IN CONNECTION WITH THE CONVEYANCE OF MOTOR VEHICLES OVER THE VICTORIAN RAILWAYS.

I,
WE, , of in the State
of Victoria (†) declare in respect of the motor
*vehicle
vehicles hereby described

† Insert business

* Strike out the singular or plural, as the case may be.

(*a*) That no petrol or volatile oils or other volatile inflammable substances accompany or are carried on the said motor *vehicle ;
vehicles ;

(*b*) That all petrol, volatile oils, and other volatile inflammable substances, have been removed from the tanks, piping, carburettors, or other receptacles on the said motor *vehicle ;
vehicles ;

(*c*) That the connecting cables and wires of all batteries on the said motor *vehicle have been disconnected from the battery terminals ; and that vehicles
the said terminals have themselves been so protected that they cannot be connected inadvertently whilst the vehicle is in the care of the Commissioners.

..... Signature of Consignor.

G.F. 11C

VICTORIAN RAILWAYS.

LIVE STOCK CONSIGNMENT NOTE.

Date ..... Train .....
From ..... To .....
Consignor ..... Consignee .....
Truck Nos. { .....
.....

No. of Trucks	Number of							Rate	Weight		Paid on	Paid	To Pay	Initials of Delivery Officer	Date of Delivery	Received in Good Order
	Calves	Cattle	Horses	Pigs	Sheep Rams	Sheep Others	All other Stock		Cwt.	Qrs.						

The animals consigned by me, as herein specified, are required to be carried strictly subject to the provisions of the Railways Acts and to the By-laws, Regulations and Conditions published by the Victorian Railways Commissioners.

Signed and accepted by me as Owner or on the Owner's behalf—

.....

Witness .....

..... { †S.M. †Guard

†One to be struck out.

FORMS—*continued.*

T.N.

H. 100*

LIVE STOCK REQUISITION

The Victorian Railways Commissioners will please provide the undersigned with.....

trucks for the carriage of.....from.....Station

to.....Station on the.....and the

undersigned hereby agree to pay full rates for every truck not loaded up at the place and on the day mentioned, and in time to be despatched by the trains to be specified by the Chief Traffic Manager or the Live Stock Agent ; also to accept all risk of injury to animals while on Railway premises, or while being loaded or unloaded, or while in transit, or from any cause whatsoever not arising from negligence on the part of the Railways Commissioners or their employees.

.....19.....

The Commissioners do not guarantee under any circumstances, the arrival or delivery of Live Stock at any particular time by any particular train or for any particular market, neither do they undertake to advise consignees of the arrival of Live Stock.

T.N.

H. 99*

SPECIAL

LIVE STOCK REQUISITION.

The undersigned being desirious of having conveyed to (or from)..... Station, where no platform, yards, or other suitable place is provided for unloading, loading or receiving Live Stock, and to (or from) which the Victorian Railways Commissioners do not undertake to carry Live Stock request that the Victorian Railways Commissioners will provide.....

trucks for the carriage of .....from.....Station to.....

Station aforesaid on the.....day of..... 19....., the undersigned undertaking to unload (or load) the trucks himself or themselves, and to accept all responsibility and risk in respect of any loss or damage incidental to the unloading or loading, or arising in consequence of the absence of suitable accommodation for unloading, loading, or receiving Live Stock, and hereby agree to pay full rates for every truck not loaded up at the place and on the day mentioned.

.....19.....

The Commissioners do not guarantee under any circumstances the arrival or delivery of Live Stock at any particular time by any particular train or for any particular market, neither do they undertake to advise Consignees of the arrival of Live Stock.

FORMS—*continued.*

VICTORIAN RAILWAYS. G.F. 144.

STORE STOCK CONSIGNMENTS.

(" **Down** " **Journey**).

Consignor's Declaration.

I of
declare that the truck loads of
(Insert number of trucks) (Insert whether cattle or sheep)
consigned by me on behalf of the owner Mr
from station to Mr.
at station on / / * are store stock
within the meaning of the definition of store stock set out hereunder :—

.....Signature of Consignor.

.....Date.

Definition of Store Stock.

Store stock means cattle (including calves) or sheep consigned with the Commissioners for carriage which require to be grazed for a period of at least eight weeks from the date of arrival at the destination station, and which are not in a fit condition to permit of their being sold or offered for sale as fat stock, provided that—

(*a*) any truck load of cattle containing one or more bulls, dairy cows in milk, or dairy springers which calve within eight weeks from the date of arrival at the destination station ;

(*b*) any truck load of sheep containing more than ten rams ;

shall not be deemed to come within the meaning of store stock.

VICTORIAN RAILWAYS. G.F. 145

STORE STOCK CONSIGNMENTS

(Down Journey)

OWNER'S APPLICATION FOR STORE RATE.

To the Victorian Railways Commissioners.

I HEREBY APPLY for store rate to be charged for the carriage of the undermentioned store stock :—

Date of Consignment	Consignor	Consignee	Journey		No. of Trucks	No. of Head	
			Station from	Station to		Sheep	Cattle

OWNER'S DECLARATION.

AND I HEREBY DECLARE—

* See Note (1) below.

(1) That I am the owner * of the abovementioned stock.

(2) That the said stock are store stock inasmuch as they require to be grazed for a period of at least eight weeks from the date of arrival at the destination station and are not in a fit condition to permit of their being sold or offered for sale as fat stock during that period.

(3) That the said stock will be grazed for a period of at least eight weeks from the said date of arrival and that none of such stock will be slaughtered in such period.

(4) That the said stock do not include any bull, any dairy cow in milk, or any dairy springer, which will calve within eight weeks from the said date of arrival.

(5) That the said stock do not include more than 10 rams in any one truck.

.....Signature of Owner.

.....Postal Address.

.....Date.

Witness.....

Note.—

(1) The applicant must be the owner of the stock at the date of arrival of the stock at the destination station.

(2) This application should be completed and lodged by the Owner or his Manager at the date of delivery of the stock at the destination station or be forwarded to reach the Live Stock Agent within six months of delivery, otherwise the consession rate may not be granted.

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FORMS—*continued.*

VICTORIAN RAILWAYS.

G.F. 146.

STORE STOCK CONSIGNMENTS.
(Up Journey)

OWNER'S APPLICATION FOR REBATE.

To the Victorian Railways Commissioners.

I HEREBY APPLY for a rebate of portion of the amount paid by me or on my behalf as freight charges for the carriage of the undermentioned store stock :—

Date of Consignment	Consignor	Consignee	Journey		No. of Trucks	No. of Head	
			Station from	Station To		Sheep	Cattle

OWNER'S STATUTORY DECLARATION.

AND IN SUPPORT OF THIS APPLICATION—

* State postal address.

I, of*

in the State of Victoria, do solemnly and sincerely declare—

(1) That I was the owner of the abovementioned stock at the date of arrival of such stock at the destination station and for a period of at least eight weeks thereafter.

(2) That the said stock at the said date of arrival required to be grazed for a period of at least eight weeks.

(3) That during the said period of eight weeks the said stock were grazed by me and that none of such stock was sold or offered for sale or slaughtered in such period.

(4) That the said stock did not include any bull, any dairy cow in milk, or any dairy springer which calved within eight weeks from the said date of arrival.

(5) That the said stock did not include more than ten rams in any one truck.

AND I make this solemn declaration conscientiously believing the same to be true and by virtue of the provisions of an Act of the Parliament of Victoria rendering persons making false declaration punishable for wilful and corrupt perjury.

DECLARED AT
in the State of Victoria this
day of
19 .
Before me

.....Signature of Owner.

.....

A Justice of the Peace in and for
the Bailiwick
of the State of Victoria.

This application for rebate should be forwarded to reach the Live Stock Agent within six months of the date of delivery of the stock at the destination station, otherwise rebate may not be granted.

LIST OF STATIONS OPEN FOR THE RECEIPT AND DELIVERY OF GOODS.

For conditions of carriage of goods, see General Conditions Nos. 1 to 37.

The loading and unloading of all goods at unattended stations, platforms or siding, stations in charge of caretakers (which are shown in *italics*), must be performed by or at the cost of the consignor or consignee, as the case may be, and the charges on goods consigned to such places must, unless otherwise provided, be prepaid. (See General Conditions Nos. 4 and 5.)

Goods not fully addressed must be marked with the initials of the consignee, together with the distinctive letters shown opposite the name of each station.

References—

(*a*) **Stations marked thus (*a*) have no shed accommodation and are open for goods in truck-loads only (minimum 3 tons).**

Exceptions—

	Minimum Weight
Furniture and household effects—	
Per Four-wheeled goods or 11-ton louvre truck...	**2 tons**
Per 15-ton louvre truck	**3 ,,**
Empties such as cases, crates, drums or tins	**1½ ,,**

The minimum tonnage may be accepted for despatch outwards by one consignor when comprised of consignments for different consignees at the same or different destination stations.

Suburban stations within 20 miles of Melbourne are not open for the despatch of consignments in less than full truckloads unless otherwise authorised by the Chief Traffic Manager.

Where the quantity loaded in a truck is less than the minimum tonnage shown above, freight charges shall be computed at tariff rates for the actual weight of the consignments concerned, plus the difference between the actual weight loaded and the minimum tonnage, at the lowest classification rate of any goods in the truck, as from the country station to the first station at which any part of the goods is or could be transferred.

Where consignments weighing less than the minimum tonnage shown are received inwards, the freight charges shall be computed on the actual weight to the nearest station to which general goods could be forwarded, plus freight as for the minimum tonnage above specified for the distance from such station to the destination station, at the lowest classification of any goods in the truck.

(*b*) **Stations marked thus (*b*) have no siding accommodation. No consignment weighing more than 10 cwt. is to be accepted for these stations.**

G. Stations marked thus (G) are open for goods not requiring crane power. No package sent to such stations must exceed 5 cwt. in weight unless the approved guarantee (see General Condition No. 24) re discharging is lodged with the consignment note.

(*c*) **Stations marked thus (*c*) are open for goods of all descriptions (see General Condition No. 24).**

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Station	Reference	Brand	Mileage from Melbourne
Addington ...	G.	A D	92¼
Agnes ...	G.	A G	117½
Albacutya ...	G.	A H	272¼
Alberton ...	G.	A T	133½
*Albury ...	(c)	A Y	190½
Alexandra ...	(c)	A X R	102
Allansford ...	G.	A F	159
Allendale ...	G.	A E	90¼
Almurta ...	G.	A R	68
Alphington ...	(a) In G Out	A P	6¾
Altona ...	(a)	A L T	10¾
Alvie ...	G.	A L V	105
Amphitheatre ...	G.	A M	135¾
Anderson ...	G.	A R N	77
Annuello ...	(c)	A N O	274
Antwerp ...	G.	A N	235¾
Arapiles ...	G.	A S	223¾
Ararat ...	(c)	A	131
Arcadia ...	G.	A C	98
Arden Street...	(a)	A R D	1
Argyle ...	G.	A R G	71¼
Arkona ...	G.	A K	231¼
Armstrong ...	G.	A O	136½
Armytage ...	G.	A R M	76¾
Arnold ...	G.	A B	139¼
Aspendale ...	G.	A S P	19
Avenel ...	G.	A L	72
Avoca ...	(c)	A V	127
Avonmore ...	G.	A M E	124¾
Axedale ...	G.	A X	89½
Baarmutha ...	(b)	B A A	168¼
Bacchus Marsh	(c)	B M	31¾
Baddaginnie ...	G.	B A	113¾
Bagshot ...	G.	B A G	112
Bairnsdale ...	(c)	B D	171¼
Bald Hills ...	(b)	B L D	82½
Ballan ...	(c)	B L	49½
*Ballarat ...	(c)	B	73¾
Ballarat Cattle Siding ...	...	...	79¾
*Ballarat East ...	(c)	B E	73¼
Balmattum ...	G.	B A L	98¾
Balmoral ...	(c)	B R L	238½
Balranald ...	(c)	B R D	272
**Bambill* ...	G.	B A M	383½
Bannerton ...	G.	B N E	285¼
Bannockburn	G.	B K N	55¾
Banool (N) ...	G.	B N L	116¼
Banyena ...	G.	B A N	194½
Barfold ...	G.	B A R	72
Barnawartha ...	(c)	B R N	174
Barnes ...	G.	B A R N	152¼
Barongarook (N)	G.	B N K	102¼
Barrakee ...	G.	B K	168¼
Barraport ...	G.	B P T	177
Barwo ...	(b)	B W O	150¾
Barwon ...	(c)	B D N	98¾
Batchica ...	(a)	B A T	221¼
Baxter ...	G.	B X R	32½
Bayles ...	G.	B A S	46¾
Bayswater ...	G.	B A Y	19¼
Beaconsfield ...	G.	B F	29½
Bealiba ...	(c)	B E A	137¾
Beaufort ...	(c)	B Q	102½
Beech Forest (N)	(c)	B F T	124¾
Beechworth ...	(c)	B H	171¼
Beetoomba ...	G.	B M B	245¼
Belgrave (N) ...	G.	B E G	26¾
Bell	(c)	B E L	7½
Bena ...	G.	B N	66
Benalla ...	(c)	B J	121¼
Benarca ...	G.	B C C	158¼
*Bendigo ...	(c)	B O	100¾
**Benetook* ...	G.	B T K	355
Ben Nevis ...	G.	B N Y	144
Bennison ...	G.	B N S	111¼
Berambong ...	G.	B B G	237¾
Berriwillock ...	(c)	B E R	225¾
Berrybank ...	G.	B B	94½
Berwick ...	G.	B W	28
Bet Bet ...	G.	B E T	121
Beulah ...	(c)	B U	238¾
Beveridge ...	G.	B E V	26
Birchip ...	(c)	B P	214¾
Birregurra ...	G.	B G E	83½
Bittern ...	G.	B I T	43
Blackburn ...	G.	B L K	11¾
Blowhard ...	G.	B L W	85
Bochara ...	(a)	B O C	205
Boigbeat ...	G.	B G T	232¼
Boinka ...	G.	B O K	332
Bolangum ...	G.	B G M	206½
Bolga ...	G.	B O L	207¾
Bolinda ...	G.	B I N	35
Bolton ...	G.	B T N	265½
Bonegilla ...	(b)	B G L	193¾
Bonnie Doon...	G.	B I	118
Boolarra ...	G.	B O O	102
**Boonoonar* ...	G.	B N N	327
Boorcan ...	G.	B O R	131¼
Boorhaman ...	G.	B H H	156½
Boort ...	(c)	B Z	169
Boronia ...	(b)	B R A	21
Borung ...	G.	B N G	156
Bowman ...	G.	B O W	169
Bowser ...	G.	B W R	149
Box Hill ...	(c)	B O X	10¼
Bradshaw ...	(b)	B C K	52
Branxholme ...	G.	B X	213¼
Bridgewater ...	(c)	B D G	126½
Bright ...	(c)	B T	196
Brim ...	(c)	B R M	229
Broadford ...	(c)	B V	46¾
Broadmeadows	G.	B R Q	10½
Bronzewing ...	G.	B R Z	279¾
Brookfield ...	G.	B R F	164½
Brunswick ...	(a) In, G Out	B S	4½
Bruthen ...	(c)	B H N	190¾
Buangor ...	G.	B G R	116½
Buckley ...	G.	B U K	64½

* *See District Rates, pages* 161 *to* 163. (N) *Narrow gauge line station.*

Station	Reference	Brand	Mileage from Melbourne
Buckrabanyule	G.	B K L	165
Buffalo ...	G.	B U F	95¾
Bullarto ...	G.	B T O	69¾
Bullioh ...	G.	B L H	217
Buln Buln ...	G.	B L N	67¾
Bumberrah ...	G.	B B H	183½
Bungaree ...	G.	B U N	64
Bung Bong ...	G.	B O N	119½
Bunnaloo ...	G.	B L O	173¼
Bunyip ...	G.	B Y	49½
Burnley ...	(*a*)	B L Y	3½
Burraboi ...	G.	B B I	204¼
Burrum ...	G.	B R U	190½
Burrumbeet ...	G.	B U R	86¾
Bylands ...	G.	B Y L	38½
Byrneside ...	G.	B Y N	113
Caldermeade ...	G.	C A L	46¼
Caldwell ...	G.	C W L	184¾
California Gully	G.	C G	104½
Calvert ...	(*a*)	C L V	150¼
Camberwell ...	(*c*)	C L	6½
Campbell ...	G.	C A M	80¾
Camperdown	(*c*)	C D	123¼
Camperdown Cattle Siding...	...	...	123¾
Cannie ...	G.	C I E	198¾
Canterbury ...	(*b*)	C B Y	7¾
Carapooee ...	G.	C P E	152½
Carina ...	G.	C N A	361¾
Carisbrook ...	(*c*)	C B	107½
Carlsruhe ...	G.	C R U	53
Carpolac ...	G.	C P O	254½
Carrum ...	G.	C A R	22¾
*Carwarp ...	G.	C W P	332
Casterton ...	(*c*)	C N	245¼
Castlemaine ...	(*c*)	C	78
Cathkin ...	G.	C T H	93¼
Caulfield ...	G.	C T D	7½
Cavendish ...	G.	C A V	213¼
Charlton ...	(*c*)	C A	173¾
Chelsea ...	G.	C S A	21
Cheltenham ...	G.	C J	14¼
Cheviot ...	G.	C F	83¼
Chewton ...	G.	C I	74¾
Chillingollah ...	(*c*)	C H L	241¼
Chiltern ...	G.	C H	168½
Chinkapook ...	G.	C N K	249½
Clarkefield ...	G.	C F D	31¼
Clayton ...	G.	C L A	13
Clematis (N) ...	G.	C L M	32
Clunes ...	(*c*)	C U	96¾
Clyde ...	G.	C L Y	31¾
Coalville ...	G.	C V	86½
Cobden ...	G.	C O	135¾
*Cobram ...	(*c*)	C M	155¼
Coburg ...	(*c*)	C O B	6¼
Cocamba ...	G.	C O C	255¼
Cockatoo (N)	G.	C R K	37¾
Cohuna ...	(*c*)	C O H	185½
*Colac ...	(*c*)	C K	95¼
Colbinabbin ...	(*c*)	C L B	117½

Station	Reference	Brand	Mileage from Melbourne
Coldstream ...	G.	C S	27
Coleraine ...	(*c*)	C E	221¼
Collins Siding (N)	G.	C O S	98¼
Colquhoun ...	G.	C X N	199¼
Condah ...	G.	C O N	221¼
Congupna Road	G.	C R D	119¼
Cope Cope ...	(*c*)	C P	175
Coragulac ...	G.	C G C	102¾
Corio ...	(*b*)	C O R	39¾
Cororooke ...	G.	C K E	101
Coromby ...	G.	C Y	190¾
Cosgrove ...	G.	C T	126¾
Cowangie ...	G.	C O W	345¼
Cowwarr ...	G.	C W	114¾
Craigieburn ...	G.	C R A	16¼
Cranbourne ...	G.	C Q	28¼
Creighton ...	G.	C R E	89
Cressy ...	(*c*)	C S O	85½
Creswick ...	(*c*)	C R	85½
Crib Point ...	(*c*)	C Z	45½
Crossley ...	G.	C R O	177¾
Crossover ...	G.	C X R	73¼
Crowes (N) ...	G.	C R S	139
Croydon ...	G.	C D N	19½
Cudgee ...	G.	C X	155
Cudgewa ...	(*c*)	C U D	255
Culgoa ...	(*c*)	C U L	217
Curlewis ...	G.	C R L	55
Curdie ...	G.	C C	146¾
Curyo ...	G.	C U R	227½
Dahlen ...	(*a*)	D B G	209
Dalmore ...	G.	D M E	38¼
Dalyston ...	G.	D A Y	83¼
Dandenong ...	(*c*)	D G	19½
Danyo ...	G.	D N Y	350¾
Darbyshire ...	G.	D Y S	224¾
Darlimurla ...	G.	D L A	106½
Darling ...	(*a*)	D L G	8
Darnum ...	G.	D U	66½
Dartmoor ...	G.	D T M	261¼
Dattuck ...	G.	D A T	265¼
David ...	(*b*)	D V D	85
Dawson ...	G.	D S	118
Daylesford ...	(*c*)	D	75½
Dean Marsh ...	(*c*)	D M	90¾
Deep Lead ...	G.	D P L	154¾
Deer Park ...	G.	D P	11
*Deniliquin ...	(*c*)	D N	190¼
Dennington ...	G.	D E N	169
Derby ...	G.	D R	121½
Derrinal ...	G.	D L	78¾
Derrinallum ...	G.	D E R	109½
Detpa ...	G.	D E P	255½
Devenish ...	G.	D V	137¾
Dhuragoon ...	G.	D H U	221
Diamond Creek	G.	D C K	20½
Diapur ...	(*c*)	D W	259¼
Diggers Rest	G.	D T	20¼
Dimboola ...	(*c*)	D A	224¾

* *See District Rates, pages* 161 *to* 163. (N) *Narrow gauge line station.*

Station	Reference	Brand	Mileage from Melbourne
Dingee ...	G.	D E	131¼
Dinmont (N) ...	G.	D M T	121¾
Ditchley (N) ...	(*b*)	D C Y	124¼
Dobie ...	G.	D B	125½
Donald ...	(*c*)	D O	182½
Donnybrook ...	G.	D F	20¾
Dooen ...	G.	D H	198¼
Dookie ...	(*c*)	D I	130
Doroq ...	G.	D Z	66¾
Drouin ...	(*c*)	D J	57½
Drysdale ...	G.	D Q	57½
Duffholme ...	(*a*)	D U F	232¾
Dumosa ...	G.	D O A	200¼
Dunkeld ...	(*c*)	D K	178½
Dunneworthy ...	G.	D U N	141¼
Dunnstown ...	G.	D C	67½
Dunolly ...	(*c*)	D Y	125¾
Duverney ...	G.	D V R	89¾
Dynon (Melbourne) ...	‡	...	...
Dysart ...	(*a*)	D S A	58¾
Eaglehawk ...	(*c*)	E A	105¾
East Metcalfe	G.	E M	66¾
East Natimuk...	(*a*)	E N T	217
Ebden ...	(*c*)	E B	195¾
Echuca ...	(*c*)	E	145
Edgecombe ...	(*c*)	E C	62¾
Elaine ...	G.	E L	76½
Elingamite ...	G.	E X	140½
Ellam ...	G.	E L M	255
Elmhurst ...	G.	E H	144¾
Elmore ...	(*c*)	E O	128½
Elphinstone ...	(*c*)	E P	70¼
Elsternwick ...	G.	E S	7
Eltham ...	G.	E L T	17¼
Emberton ...	G.	E N	69½
Emerald (N) ...	G.	E M D	33
Emu ...	G.	E U	145¼
Englefield ...	G.	E N G	233
Epping ...	G.	E Q	14
Epsom ...	G.	E T	105½
Erica (N) ...	G.	E R A	99½
Erwen ...	G.	E W	109
Essendon ...	G.	E S S	5
Eureka ...	(*a*) In, G Out	E K	74½
Euroa ...	(*c*)	E R	93¾
Eurobin ...	G.	E I	187¾
Eversley ...	G.	E V	147½
Everton ...	G.	E F	161
Fairfield ...	G.	F P	6
Fairley ...	G.	F A	184½
Fairview ...	G.	F V	186½
Ferguson (N) ...	G.	F R	127½
Fernbank ...	G.	F K	153½
Fern Hill ...	(*c*)	F H	59¾
Fish Creek ...	G.	F C	100½
Fitzroy ...	(*a*)	F Y	6
Flynn ...	G.	F L	107
Footscray ...	†	F C Y	4⅝
Forrest ...	(*c*)	F O	103¼
Foster ...	G.	F S	108¼
Frankston ...	(*c*)	F N	27½
Fulham ...	G.	F M	124½
Galah ...	G.	G A H	298
Galaquil ...	G.	G A L	234¾
Gama ...	G.	G A M	253¾
Gannawarra ...	G.	G A W	191
Gapstead ...	G.	G A P	173¼
Garfield ...	G.	G A R	46¾
Garvoc ...	G.	G A V	144½
Gatum ...	G.	G A T	223¾
*Geelong ...	(*c*)	G	45
Gellibrand (N)	G.	G B D	112½
Gelliondale ...	G.	G E L	130¼
Gembrook (N)	G.	G M	41¾
Gerang Gerung	(*c*)	G N G	235¼
Gheringhap ...	G.	G P	51¾
Girgarre ...	G.	G G A	118¾
Gisborne ...	(*c*)	G I	40
Glenalbyn ...	G.	G B	139¼
Glen Forbes ...	G.	G N F	69¾
Glenfyne ...	G.	G F	142¼
Glengarry ...	(*c*)	G Y	104½
Glenhuntly ...	G.	G L N	8½
Glenloth ...	G.	G Z	183
Glenorchy ...	(*c*)	G C	162½
Glenrowan ...	G.	G L	136
Glenroy ...	G.	G N Y	9
Glen Thompson	G.	G T	167¼
Gnarkeet ...	G.	G N K	99¼
Golden Square	(*c*)	G S	99¼
Goldsborough	G.	G H	129¾
Goorambat ...	G.	G A	131¾
Goornong ...	(*c*)	G O	118
Gorae ...	G.	G E	246½
Gordon ...	(*c*)	G D	56¾
Goroke ...	(*c*)	G K	245½
Gould (N) ...	G.	G V	91¾
Gowanford ...	G.	G O W	226¾
Goyura ...	G.	G J	249
Graham ...	(*a*)	G G	3¼
Grassdale ...	G.	G R	222¾
Great Western	G.	G W	141¾
Gredgwin ...	G.	G X	180¾
Green Hill ...	G.	G H L	64½
Greensborough	G.	G R B	14½

* *See District Rates, pages 161 to 163.* (N) *Narrow gauge line station.*

† *Footscray is open for:—*

Inwards—Agricultural machinery or parts for repairs, sugar or manure for reconditioning, and consignments in truck loads.

Outwards—Agricultural machinery, sugar in 10 cwt. lots, and consignments in truck loads.

§ *For junction mileage see Addenda page 53*

‡ *The same rates as to or from Melbourne shall be charged.*

Station	Reference	Brand	Mileage from Melbourne
Greenwald ...	G.	G R N	254
Guildford ...	G.	G U	84½
**Gulpa* ...	G.	G L P	177¼
Gunbower ...	(c)	G W N	172
Gymbowen ...	G.	G N	238¾
Gypsum ...	G.	G Y P	274
Haddon ...	G.	H N	84¼
Hallam ...	G.	H R	23½
Hamilton ...	(c)	H	197½
Hammond ...	G.	H A M	96¼
Hampton ...	(b)	H M T	11¼
Harcourt ...	(c)	H T	82¾
Hastings ...	G.	H S	40½
**Hattah* ...	G.	H A T	310¼
Hawkesdale ...	G.	H W	189½
Hawthorn ...	G.	H A	4½
Healesville ...	(c)	H E	40
Heathcote ...	(c)	H C	73½
Heathmere ...	G.	H M R	241½
Hedley ...	G.	H F	124½
Heidelberg ...	G.	H B	9
Henty ...	G.	H J	236¼
Heyfield ...	(c)	H D	121½
Heywood ...	(c)	H Q	235¼
High Camp ...	G.	H C P	51½
Hillside ...	G.	H X	164
Hinksons ...	G.	H I N	187¾
Hoddle ...	G.	H G	103¾
Homebush ...	G.	H U	123¾
Homewood ...	G.	H O	74¼
Hopetoun ...	(c)	H P	254½
Horsham ...	(c)	H M	203¼
Hunter ...	G.	H T R	135½
Huntly ...	G.	H Y	108
Huon ...	G.	H L	201
Hurstbridge ...	G.	H H	23¾
Illowa ...	G.	I A	172
Impimi ...	G.	I M P	259
Inglewood ...	(c)	I	131
Ingliston ...	G.	I N	45
Inverleigh ...	G.	I H	62½
Irrewarra ...	G.	I R	91¾
**Irymple* ...	G.	I R Y	347¼
Jackson ...	G.	J A C	180½
Jallumba ...	G.	J L	230½
Jeetho ...	G.	J E	62¾
Jeffries ...	G.	J R	240½
Jeparit ...	(c)	J P	247¾
Jewell ...	G.	J W L	4
Jimaringle ...	G.	J I M	212
Joyce's Creek ...	G.	J C	95¼
Jung... ...	G.	J G	192
Kanagulk ...	G.	K G K	245¼
Kanawalla ...	(a)	K L A	203
Kangaroo Flat	G.	K A N	97¾
Kaniva ...	(c)	K X	272½
Kanumbra ...	G.	K B	101¼
Kanyapella ...	(a)	K A	139½
**Karawinna* ...	G.	K W A	372¼
Kardella ...	G.	K C	72½
Karook ...	(b)	K R K	110¼
**Karween* ...	G.	K A E	397½
Katamatite ...	G.	K T	147
Katunga ...	G.	K F	140¼
Kawarren (N)	G.	K A W	108¼
Keely ...	G.	K E E	181¾
Kensington ...	†	K E N	2¼
Kerang ...	(c)	K G	179½
Kernot ...	G.	K H	66¼
Kerrisdale ...	G.	K D	68¼
Kew ...	(a) In, G Out	K W	5¾
**Kiamal* ...	G.	K A B	294
Kiata ...	(c)	K I	240
Kilcunda ...	(b)	K L D	79¼
Kilcunda Siding	(a)	K N S	80¼
Killara ...	G.	K I L	35¾
Kilmany ...	G.	K L M	120½
Kilmore ...	(c)	K R	42½
Kilmore East	G.	K E	39½
Kincaid Siding (N) ...	(b)	K K	131
Kingston ...	(c)	K J	92¼
Kinnabulla ...	G.	K B A	224½
Kirkstall ...	G.	K L	179
Knowsley ...	G.	K N	83
Koetong ...	G.	K T G	229
Koimbo ...	G.	K B O	269½
Kooloonong ...	(c)	K N G	257¾
Koondrook ...	(c)	K D K	193½
Koonwarra ...	G.	K U	84
Koorkab ...	G.	K O O	261½
Koo-wee-rup	(c)	K P	42
Koriella ...	G.	K O R	97½
Koroit ...	G.	K S	175½
Korong Vale ...	(c)	K V	151
Korumburra ...	(c)	K M	69¾
Kotta ...	G.	K T A	152¼
Koyuga ...	G.	K Q	135
Kulwin ...	G.	K U L	275¼
Kurting ...	G.	K Z	135
Kyabram ...	(c)	K Y	124
Kyneton ...	(c)	K	57
Kyup ...	G.	K Y V	209
La La ...	(a)	...	49
Lah	G.	L A H	225¼
Lake Boga ...	(c)	L B	205
Lake Charm ...	G.	L C	190¼
Lalbert ...	(c)	L B T	205¾

* See District Rates, pages 161 to 163. (N) *Narrow gauge line station.*

† *Kensington is open only for consignments for Messrs. W. S. Kimpton & Son, Barastoc Products and Younghusband Ltd. The same rates as to Melbourne shall be charged for Inwards traffic to Kensington and actual mileage rates for Outwards traffic unless otherwise specified.*

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Station	Reference	Brand	Mileage from Melbourne
Lal Lal ...	G.	L A L	78½
Lancefield ...	(c)	L D	46
Langi Logan ...	G.	L L	136¾
Lang Lang ...	G.	L N G	48¼
Langwarrin ...	G.	L G N	30¾
Lara ...	G.	L V	35¾
Larpent ...	G.	L P	100¼
Lascelles ...	G.	L A S	248¼
Launching Place	(c)	L A P	42
Laurie ...	G.	L A U	132¾
Laver's Hill (N)	G.	L V H	137¼
Laverton ...	G.	L A V	13
Learmonth ...	G.	L M	88¾
Leichardt ...	G.	L K	117¼
Leitchville ...	G.	L L E	177
Leitpar ...	G.	L P R	270½
Leongatha ...	(c)	L G	79
Leopold ...	(a)	L E O	52
Lethbridge ...	G.	L E G	61¾
Lillimur ...	(c)	L J	278¾
Lilliput ...	G.	L T	165
Lilydale ...	(c)	L	24½
Lindenow ...	(c)	L W	160¼
Linga ...	G.	L G A	326¼
Linton ...	G.	L N	98¾
Lismore ...	G.	L S	103½
Litchfield ...	G.	L F	192
Little Brooklyn...	†	L E B	9
Little River ...	G.	L R	29½
Llanelly ...	G.	L N Y	138½
Loch ...	G.	L A	60
Lockington ...	G.	L O C	147¾
Locksley ...	G.	L O X	79¼
Londrigan ...	(c)	L O N	152½
Longlea ...	G.	L E A	93
Longwarry ...	G.	L Y	52
Longwood ...	G.	L O	84¾
Lorquon ...	G.	L O R	261½
Lovat (N) ...	G.	L V T	111
Lower Ferntree Gully ...	G.	L X	22½
Loy Yang ...	G.	L Z	102½
Lubeck ...	G.	L U	175
Lyndhurst ...	G.	L Y N	24
Lyons ...	G.	L N S	248
Lyonville ...	G.	L Q	67½
Macedon ...	(c)	M D N	43¼
Macorna ...	(c)	M A	165¼
Maffescioni's Siding ...	(a) Out	...	141¼
Maffra ...	(c)	M F	132½
Maindample ...	G.	M P L	122½
Maldon ...	(c)	M N	89
Malmsbury ...	(c)	M Y	63¼
Malvern ...	(a)	M A L	6¼
Manangatang ...	(c)	M N G	259¾
Mangalore ...	G.	M G R	68
Mannerim ...	G.	M A N	61
Manor ...	G.	M N X	24¾
Mansfield ...	(c)	M D	131¼
Marnoo ...	(c)	M R N	200
Marong ...	G.	M G	112½
Maroona ...	G.	M A R	144¼
Marp ...	G.	M R P	267
Marshall ...	G.	M S L	49¼
Maryborough	(c)	M	112
Massey ...	G.	M S Y	197¼
*Mathoura ...	(c)	M T H	169¼
McColl ...	G.	M C C	144½
McDevitt (N) ...	(b)	M C V	120
McDougall ...	G.	M C D	48
Meatian ...	G.	M E T	212¾
Meeniyan ...	G.	M E Y	89
Melton ...	(c)	M E	23¼
Mentone ...	G.	M E N	15¼
Menzies' Creek (N) ...	G.	M Z K	30¾
*Merbein ...	(c)	M R E	358¼
Meredith ...	(c)	M H	71
*Meringur ...	G.	M E G	393¼
Merino ...	G.	M N O	231¼
Mernda ...	G.	M N A	21
Merrigum ...	G.	M E R	117¾
*Merrinee ...	G.	M N E	366
Merton ...	G.	M T N	108¾
Middle Brighton	(a) In, G. Out	M B M	9½
Middle Creek	G.	M C K	112½
*Mildura ...	(c)	M D A	351¼
Millbrook ...	G.	M I L	58½
Millgrove ...	G.	M G V	46½
Milltown ...	G.	M L M	227¾
Mincha ...	G.	M C	159¼
Minhamite ...	G.	M I H	196¼
Mininera ...	G.	M I N	138½
Minyip ...	(c)	M I	198¼
Miralie ...	G.	M R L	238
Miram ...	(c)	M V	266½
Mirboo North	(c)	M N H	110
Mitcham ...	G.	M C H	14½
Mitiamo ...	(c)	M I T	142¼
Mitre ...	G.	M L K	227¾
Mittyack ...	G.	M Y C	266¾
Moama ...	G.	M O A	146¼
Moe ...	(c)	M O	81¼
*Moira ...	G.	M R A	160¼
Molesworth ...	(c)	M W H	90¼
Mologa ...	G.	M O L	147
Monea ...	G.	M E A	75¾
Monegeeta ...	G.	M O N	36½
Monomeith ...	G.	M O U	44
Montgomery ...	G.	M T O	134¼
Moolort ...	G.	M L T	100¾
Moondarra (N)	G.	M N D	96
Moorabbin ...	G.	M B N	11¾
Moorabool ...	G.	M B L	48¼
Moorooduc ...	(c)	M D C	35¾
Mooroolbark ...	G.	M B K	21½
Mooroopna ...	(c)	M P	110

* See *District Rates, pages* 161 *to* 163. (N) *Narrow gauge line station.*

† *Little Brooklyn is open for the despatch and receipt of livestock only.*

Station	Reference	Brand	Mileage from Melbourne
Moranding ...	G.	M O R	49¾
Mordialloc ...	G.	M X	17½
Moreland ...	(*a*)	M L D	5½
Moriac ...	G.	M R C	60
**Morkalla* ...	G.	M O K	402¾
Mornington ...	(*c*)	M Q	40¼
Mortat ...	G.	M A T	251¼
Mortlake ...	(*c*)	M T	150¼
Morton Plains	G.	M P N	207½
Morwell ...	(*c*)	M Z	90
Mossiface ...	G.	M S F	188¼
Moulamein ...	(*c*)	M M N	231½
Mount Evelyn	(*c*)	M Y L	28½
Mount Gambier Border† ...	...	...	280
Moutajup ...	G.	M J P	184¾
Moyne ...	G.	M O Y	181¼
Muckleford ...	G.	M U K	84½
**Mulwala* ...	G.	M U L	164
Munro ...	G.	M R O	145¼
Murchison ...	G.	M U	93¼
Murchison East	(*c*)	M U E	91½
Murrabit ...	(*c*)	M B T	194½
Murrayville ...	(*c*)	M V L	356¾
Murroon ...	G.	M U R	95½
Murrumbeena	(*a*)	M U B	9¼
Murtoa ...	(*c*)	M R	185¼
Musk ...	G.	M S K	72¼
Myall ...	G.	M Y A	190¼
Myamyn ...	G.	M Y N	224¼
Myrtleford ...	(*c*)	M L	177½
Mysia ...	G.	M S	160¼
Mystic Park ...	G.	M K	196¾
Mywee ...	G.	M Y E	150
Nagambie ...	(*c*)	N A	78¼
Nandaly ...	G.	N A N	255¾
Nar-Nar-Goon	G.	N N G	41
Naraghid ...	G.	N G	131¼
Narracan ...	G.	N R	88¾
Narre Warren	G.	N W	25½
Nathalia ...	(*c*)	N H	147¾
Natimuk ...	(*c*)	N T	218¾
Natya ...	G.	N A T	250¾
Nayook ...	G.	N Y K	83¾
Neerim ...	G.	N R M	80¼
Neerim South	G.	N S	75¾
Nerrin Nerrin ...	G.	N N R	126¾
Netherby ...	(*c*)	N B Y	267½
Newlyn ...	G.	N Z	90½
Newmarket ...	‡	N E W	2¾
Newport ...	(*c*)	N P	6½
Newstead ...	(*c*)	N D	92¼
Newtown ...	G.	N E	92¾
Nhill ...	(*c*)	N	248¼
Nicholson ...	G.	N I C	177½
Niemur ...	G.	N M R	216¼
Ninda ...	G.	N N A	244¼
Nobelius Siding (N)	G. a/c Nobelius	...	33½
Noojee ...	(*c*)	N J E	89¾
Nooramunga ...	G.	N I	134¼
Noradjuha ...	G.	N X	223½
North Brighton	G.	N B N	8½
Northcote ...	G.	N C E	5¾
North Creswick	G.	N C	86½
North Fitzroy	G.	N F	5
**North Geelong*	G.	N O	43½
North Monegeetta ...	(*b*)	N Q	38¼
North Williamstown	(*a*)	N W	7½
Nowa Nowa ...	(*c*)	N W N	208¼
**Nowingi* ...	G.	N O W	322¼
Nullan ...	G.	N L	202½
Nullawil ...	G.	N U	207
Numurkah ...	(*c*)	N K	133¾
Nunawading ...	(*b*)	N W G	13¼
**Nunga* ...	G.	N G A	284
Nyahwest ...	(*c*)	N X H	232½
Nyarrin ...	G.	N Y N	250
Nyora ...	G.	N Y	56½
*Oaklands ...	(*c*)	O K D	199¾
Oakleigh ...	(*c*)	O A	10½
Oakvale ...	G.	O K	185¼
Officer ...	G.	O F	32
Orbost ...	(*c*)	O B	231½
**Ouyen* ...	(*c*)	O Y	288¾
Ovens ...	G.	O V	180¾
Painswick ...	G.	P W	130½
Pakenham ...	G.	P K	36½
Panitya ...	G.	P T Y	368
Panmure ...	G.	P V	150¾
Parkwood ...	G.	P W D	215¾
Parwan ...	G.	P N	29¼
Patchewollock	(*c*)	P A T	281½
Patho ...	G.	P T H	165¼
Peechelba East	G.	P E E	161¼
Pennyroyal ...	G.	P R	93
Penshurst ...	(*c*)	P T	208½
Pental ...	G.	P E N	209¼
Perekerten ...	G.	P K T	247½
Piangil ...	(*c*)	P N G	242
Picola ...	(*c*)	P I	154½
Pier Millan ...	G.	P R N	260
Pimpinio ...	G.	P O	213½

* *See District Rates' pages 161 to 163.* (N) *Narrow gauge line station.*

† *No station is provided. In arriving at the Victorian proportion of freight charges on inter-system consignments via Mount Gambier, the mileage rates to or from Mount Gambier Border shall be charged.*

‡ (*a*) In and Out, G. In, *account Dalgety & Co. whose consignments shall be charged the same rates as to Melbourne for Inwards traffic and actual mileage rates for Outwards traffic unless otherwise specified. Empty livestock crates, In*

Station	Reference	Brand	Mileage from Melbourne
Pinnaroo Border† ...	...	...	369¼
Pine Lodge ...	G.	P L	121
Pira	G.	P R A	227¾
**Pirlta* ...	G.	P I R	360½
Pirron Yallock	G.	P Y	104½
Pisgah ...	(*b*)	P C	81¾
Pittong ...	G.	P I T	104¼
Pomborneit ...	G.	P B	113¾
Poorneet ...	G.	P O R	79¼
Porepunkah ...	G.	P U	192½
Port Fairy ...	(*c*)	P F	186½
Portland ...	(*c*)	P	251½
Portland North	G.	P L N	250½
Port Melbourne	(*a*)	P M	3¼
Port Melbourne Pier ...	G to or from steamers	...	3¾
Prairie ...	G.	P E	136¼
Pullut ...	G.	P U L	259
Puralka ...	G.	P K A	274
Pura Pura ...	G.	P P	121½
Purdeet ...	G.	P D	201½
Pyalong ...	G.	P G	56½
Pyramid ...	(*c*)	P H	155
Pyramid Creek	G.	P Y K	182¼
Quambatook ...	(*c*)	Q K	191
Quantong ...	G.	Q G	214
Queenscliff ...	(*c*)	Q	67½
Rainbow ...	(*c*)	R B W	266¼
Ravenswood ...	G.	R A	90¼
Raywood ...	G.	R V	120
*Redcliffs ...	(*c*)	R L	342½
Redesdale ...	G.	R E	76¼
Redesdale Junct.	G.	R J	60
Remlaw ...	(*a*)	R E M	208
Rennick ...	G.	R N K	279½
**Rennie* ...	G.	R N I	181
Reservoir ...	G.	R E S	9½
Riddell ...	G.	R C	35¼
Ringwood ...	G.	R D	16¼
Robinvale ...	(*c*)	R O B	293½
Rochester ...	(*c*)	R O	138¾
Rockbank ...	G.	R K	18½
Rokeby ...	(*c*)	R B	70½
Romsey ...	G.	R M	40½
Roseberry ...	(*c*)	R Y	246
Rosebrook ...	(*a*)	R B K	184¼
Rosedale ...	(*c*)	R S	112¼
Roslynmead ...	G.	R O S	157½
Rowsley ...	G.	R W	34¼
Ruby ...	G.	R U	75¾
Rupanyup ...	(*c*)	R P	184¾
Rushworth ...	(*c*)	R H	104¾
Rutherglen ...	(*c*)	R	169¼
Sale	(*c*)	S B	128¾
Salisbury ...	G.	S A L	242½
Sandford ...	G.	S A N	242½
Sandringham ...	G.	S H M	12¼
**Sangar* ...	G.	S A G	188½
Scarsdale ...	G.	S D E	90¾
Seaford ...	G.	S F O	24¾
Sea Lake ...	(*c*)	S L	238
Sebastian ...	G.	S E	116
Selby (N) ...	(*b*)	S E Y	28¼
Serviceton ...	(*c*)	S N	287
Seville ...	G.	S E V	33½
Seymour ...	(*c*)	S	61¼
Sheephills ...	(*c*)	S H P	206½
Shelbourne ...	(*c*)	S L B	99¼
Shelley ...	G.	S H Y	235½
Shepparton	(*c*)	S P	113
Shepparton Cattle Siding	...	...	114½
Sinclair ...	(*a*)	S I N	241
Skipton ...	G.	S K T	111½
**Sloane* ...	G.	S L E	169
Smythesdale ...	G.	S M I	88½
Somerton ...	G.	S M	13½
Somerville ...	(*c*)	S V	35
Southdown ...	(*a*)	S D N	182¾
*South Geelong	G.	S G	46¼
South Kensington...	‡	S K N	2¼
South Morang ...	G.	S M G	17¾
Speed ...	G.	S P E	264¾
Spotswood ...	§	S P D	5¾
Springhurst ...	G.	S T	160
Spring Vale ...	G.	S R	15½
St. Albans ...	G.	S T A	11¼
Stalker (N) ...	G.	S T L	134¾
Stanhope ...	(*c*)	S T H	115¼
St. Arnaud ...	(*c*)	S A	158½
State Mine ...	††	S M E	86½
Staughton's Siding ...	(*a*)	S T N	26½

* See District Rates, pages 161 to 163. (N) Narrow gauge line station.

† *No station is provided. In arriving at the Victorian proportion of freight charges on inter-system consignments via Pinnaroo, the mileage rates to or from Pinnaroo Border shall be charged.*

†† *Open for Goods account the State Mine only.*

‡ *South Kensington is open for—(a)* In and Out, *account Minifie & Co.,* G. In and Out, *account New Zealand Loan and The Australian Mercantile Land and Finance Co.* Inwards *traffic to South Kensington shall be charged the same rates as to Melbourne and actual mileage rates, unless otherwise specified, for* Outwards *traffic from South Kensington.*

§ *Spotswood is open for the receipt and despatch of goods in truck loads for any consignor or consignee ; agricultural machines and parts for repair ; consignments in or out in less than 3 tons, where trucks are loaded to full cubial capacity ; and for the receipt of Inwards goods in any quantity on account of the Shell Co. of Australia Ltd. and the Asphalt Cold Mix (Aust.) Pty. Ltd.*

Station	Reference	Brand	Mileage from Melbourne
Stavely ...	G.	S V Y	162
Stawell ...	(c)	S W	150
St. James ...	G.	S J	141¾
St. *Kilda* ...	(a)	S T K	4½
Stoneyford ...	G.	S D	111
Stony Creek ...	G.	S C	91
Stony Point ...	(c)	S O	46½
Strangway ...	G.	S T R	88½
Stratford ...	(c)	S T D	138½
Strathallan ...	G.	S H L	146¼
Strathkellar ...	G.	S K R	192
Strathmerton...	G.	S X	146
Sulky ...	G.	S K	80¾
Sunbury ...	G.	S I	23¾
Sunshine ...	(c)	S U N	7¾
Surrey Hills ...	G.	S Z	8¾
Sutherland ...	G.	S U	165½
Swan Hill ...	(c)	S H	214½
Swan Hill Cattle Siding ...	...	...	216
Swanwater ...	G.	S Q	170¼
Sydenham ...	G.	S Y	15
Tabilk ...	G.	T K	74
Tabor ...	G.	T O	210
Talbot ...	(c)	T Q	107½
Tallangatta ...	(c)	T A	212½
Tallarook ...	(c)	T L	56
Tallygaroopna	G.	T P	123¾
Tandarra ...	G.	T D	127
Tantonan ...	G.	T T N	179
Taradale ...	G.	T D L	67¾
Tarnagulla ...	G.	T G	136¼
Tarranginnie ...	G.	T I N	255½
Tarranyurk ...	G.	T Y K	242
Tarrawarra ...	G.	T A R	35
Tarrawingee ...	G.	T W G	157¾
Tarwin ...	G.	T W N	87¼
Tatura ...	(c)	T U	110
Tatyoon ...	G.	T Y O	144¼
Teal Point ...	G.	T P T	188¾
Teddywaddy ...	G.	T E D	178¾
Telford ...	G.	T E	154¾
Tempy ...	G.	T M Y	268¾
Terang ...	(c)	T N G	137¼
Thomastown ...	G.	T S N	12
Thorpdale ...	G.	T H	92½
**Thurla* ...	G.	T H L	348¾
Thyra ...	G.	T H Y	168¼
Timboon ...	G.	T I M	149½
Tinamba ...	G.	T I	127½
*Tocumwal ...	(c)	T C L	156¼
Tongala ...	G.	T J	130¾
Tooborac ...	G.	T B C	62¾
Toolamba ...	(c)	T L A	103¼
Toolondo ...	G.	T O O	234¾
Toongabbie ...	G.	T B	109¾
Toora ...	(c)	T O A	114½
Tooradin ...	G.	T D N	36½
Toorak ...	(c)	T O K	5¼
Tooronga ...	G.	T G A	6¼
Torrita ...	G.	T R T	312¾
Tostaree ...	G.	T S T	214¼
Tourello ...	G.	T L O	93
Trafalgar ...	G.	T W	75
Tragowel ...	G.	T R G	170½
Traralgon ...	(c)	T N	98½
Trawalla ...	G.	T R	97½
Trawool ...	G.	T R A	63
Trentham ...	(c)	T M	63¾
Tresco ...	G.	T S O	201½
**Trinita* ...	G.	T R N	302
Tungamah ...	(c)	T	149¼
Turriff ...	G.	T R F	260½
Tutye ...	G.	T T	339
Tyabb ...	G.	T V	37½
Tylden ...	G.	T Y	56½
Tynong ...	(c)	T Z	44¼
Ultima ...	(c)	U L	221¼
Underbool ...	(c)	U B	319¾
Upper Ferntree Gully ...	(c)	U G	23¾
Upwey (N) ...	(b)	U P	25¼
Urangara ...	G.	U R	218
Vasey ...	G.	V A	228
Vectis ...	G.	V	211¼
Victoria Park...	(c)	V K	3½
Violet Town ...	(c)	V T	105¼
Vite Vite ...	G.	V V	115¼
Waaia ...	G.	W A	140½
*Wahgunyah ...	(c)	W G	174
Wahring ...	G.	W Q	84½
Wail ...	G.	W A I	219¼
Waitchie ...	G.	W I E	233½
Wakool ...	G.	W A K	197½
Wallace ...	G.	W C	61½
Wallan ...	G.	W K	29½
Walpeup ...	G.	W L P	307½
Walwal ...	G.	W A L	169
Wanalta ...	G.	W L T	112½
Wandin ...	G.	W A N	31½
Wandong ...	G.	W J	34¼
**Wangamong* ...	G.	W G G	194¼
Wangaratta ...	(c)	W	145½
Wannon ...	G.	W V	210½
Waranga ...	G.	W X	99¼
Warburton ...	(c)	W A R	48¼
Warncoort ...	G.	W N T	87
Warne ...	G.	W R N	213
Warracknabeal	(c)	W L	216½
Warragamba ...	G.	W R G	140¼
**Warragoon* ...	G.	W G D	175½
Warragul ...	(c)	W I	62¼
Warra Yadin ...	G.	W Y N	136
Warrenheip ...	G.	W H P	69¾
Warrnambool	(c)	W B	166
Warrong ...	G.	W N G	179
Watchem ...	(c)	W C H	202½
Watchupga ...	G.	W H A	234¾
Watson (N) ...	(b)	W T N	97½
Waubra ...	G.	W A J	95¼

* See *District Rates, pages* 161 *to* 163. (N) *Narrow gauge line station.*

Station	Reference	Brand	Mileage from Melbourne
Waygara ...	(c)	W Y G	222½
Wedderburn ...	(c)	W E D	151
Wedderburn Junction ...	G.	W J N	146
Weeaproinah (N) ...	G.	W E A	129½
Weerite ...	G.	W E R	117¾
Wellsford ...	(b)	W L F	115¼
Welshpool ...	G.	W P L	120¾
Werribee ...	(c)	W R	19¾
**Werrimul* ...	G.	W M L	378
Wesburn ...	(c)	W E S	45½
West Footscray	(c)	W F Y	4½
Westmere ...	(c)	W S M	132½
White Hills Siding ...	†	...	103½
Whittlesea ...	G.	W S	26¾
Willaura ...	(c)	W I L	154½
Williamstown Pier ...	(c)	W P	9¾
Willowmavin ...	G.	W V N	45¾
Wimba (N) ...	(b)	W M B	118¼
Winchelsea ...	G.	W H	70¾
Windermere ...	G.	W T	83½
Windsor ...	(a) In, G. Out	W I N	5
Wingeel ...	G.	W G I	73½
Winnap ...	G.	W N P	258
Winton ...	G.	W N	127¼
*Wodonga ...	(c)	W D	187
Womboota ...	G.	W O T	163¼
Wonthaggi ...	(c)	W O N	87¼
Woodend ...	(c)	W E	48½
Woodfield ...	G.	W F	114½
Woodleigh ...	G.	W D L	63¼
Woolamai ...	G.	W L M	73¼
Woolsthorpe ...	G.	W S P	185
Woomelang ...	(c)	W M G	241¼
Woorinen ...	G.	W N N	222
Woori Yallock	G.	W Y K	38¼
Woorragee ...	G.	W O	178¾
Wunghnu ...	G.	W U	129⅛
Wycheproof ...	(c)	W Y	190¼
Wychitella ...	G.	W Z	159¾
Wyelangta (N)	G.	W Y L	133
Yaapeet ...	(c)	Y T	276¾
Yabba North ...	G.	Y N	138¼
Yabba South ...	G.	Y S	133¾
Yackandandah...	(c)	Y A	184
Yallakool ...	G.	Y K L	191
Yallourn ...	G.	Y A L	86¼
Yanac ...	(c)	Y B	279¾
Yangalake ...	G.	Y G K	266¼
Yan Yean ...	G.	Y E	23
Yarck ...	G.	Y K	97
Yarra Glen ...	(c)	Y L	31¾
Yarragon ...	G.	Y O	70
Yarra Junction	(c)	Y J T	43¼
Yarram ...	(c)	Y A R	137¼
**Yarrara* ...	G.	Y R R	388
Yarraville ...	‡	Y V	4¾
Yarrawonga ...	(c)	Y G	161½
Yarroweyah ...	G.	Y H	150½
Yarto ...	G.	Y T O	271¾
Yatchaw ...	G.	Y C	204½
**Yatpool* ...	G.	Y P	338½
Yea ...	(c)	Y	79¾
**Yelta* ...	G.	Y E L	364
Yendon ...	G.	Y D	75
Yering ...	G.	Y I	29½
Yinnar ...	G.	Y R	97¼
Youanmite ...	G.	Y M	142
Yungera ...	G.	Y G A	264½

* *See District Rates, pages 161 to 163.* (N) *Narrow gauge line station.*

† *(a) In : fruit, vegetables and gravel Out.*

‡ *Yarraville is open for:—*

Inwards—Truck loads, manure and sugar for reconditioning, empty return cases, jars, bags, &c., account Colonial Sugar Coy., Commonwealth Fertilizer Coy., and Victor Leggo & Coy.

Outwards—General goods not requiring crane power.

JUNCTION MILEAGE TABLE

The following Table shows the mileages for use in respect of goods or live stock traffic between Junction stations and from Junction stations to Intersystem Border stations.

	Albury	Albion	Ararat	Ballarat	Benalla	Bendigo	Broadmeadows	Carlsruhe	Castlemaine	Coleraine Junct.	Cressy	Dunolly	East Natimuk	Echuca	*Footscray
Local Junctions															
Albion	188¾	—	124	66¾	119¼	92¼	8¾	44½	69½	191¼	87	117¼	210	143¼	5
Ararat	312¾	124	—	57¼	243½	111¼	132¾	113¼	88½	67¼	79¾	68¼	86	165¾	127½
Ballarat	255½	66¾	57¼	—	186¼	99¼	75½	59¼	76½	124½	43½	56¼	143¼	153¾	70¼
Benalla	69¼	119½	243½	186¼	—	157¼	110¾	164	180	310¾	204¾	209¾	329½	130¼	122¾
Bendigo	226½	92¼	111¼	99¼	157¼	—	92	47¾	22¾	178½	141¼	55	197¼	54½	97¼
Broadmeadows ...	180	8¾	132¾	75½	110¾	92	—	53¼	78¼	200	94	126	218¾	134½	12
Carlsruhe	233¼	44½	113½	59¼	164	47¾	53¼	—	25	180¾	101¼	72¾	199½	102¼	49½
Castlemaine	249¼	69½	88½	76½	180	22¾	78¼	25	—	155¾	118½	47¾	174½	77¼	74½
Coleraine Junction ...	380	191¼	67¼	124½	310¾	178½	200	180¾	155¾	—	120½	135½	78¼	233	194¾
Cressy	274	87	79¾	43½	204¾	141¼	94	101¼	118½	120½	—	98¼	165¾	195¾	82
Dunolly	279	117¼	68¼	56¼	209¾	55	126	72¾	47¾	135½	98¼	—	154¼	107	122¼
East Natimuk ...	398¾	210	86	143¼	329½	197¼	218¾	199½	174½	78¼	165¾	154¼	—	251¾	213½
Echuca	199½	143¼	165¾	153¾	130¼	54½	134½	102¼	77¼	233	195¾	107	251¾	—	146½
*Footscray	192	5	127½	70¼	122¾	97¼	12	49½	74½	194¾	82	122¼	213½	146½	—
Heathcote Junction ...	157¼	31½	155½	98¼	88	69¼	22¾	76	92	222¾	116¾	121¾	241½	111¾	34¾
Inglewood	254¼	122½	93	81	185	30¼	119¾	78	53	160¼	123	24¾	179	82¼	127½
Irrewarra	280¼	93¼	99¾	63½	211	161¼	100¼	121¼	138½	135	20	118¼	185¾	215¾	88¼
Korong Vale	274¼	142½	113	101	205	50¼	139¾	98	73	180¼	143	44¾	199	102¼	147½
Linton Junction ...	258¼	69½	54½	2¾	189	100½	78¼	60½	77¾	121¾	40¾	57½	140½	155	73
Mangalore	122½	66¼	190¼	133	53¼	104	57½	110½	126¾	257½	151½	156½	276¼	77	69½
Maroona	326	137¼	13¼	70½	256¾	124½	146	126¾	101¾	54	66½	81½	99¼	179	140¾
Maryborough	283¼	103½	54½	42½	214	56¾	112¼	59	34	121¾	84½	13¾	140½	111¼	108½
Murtoa	367	178¼	54¼	111½	297¾	165½	187	167¾	142¾	110	134	122½	31¾	220	181¾
North Ballarat ...	256¼	67½	56½	¾	187	98½	76¼	58½	75¾	123¾	42¾	55½	142½	153	71
North Bendigo Junc.	225¼	93½	112½	100½	156	1¼	90¾	49	24	179¾	142½	53¾	198½	53¼	98½
North Creswick ...	268¼	79½	68½	12¾	199	86½	88¼	46½	63¾	135¾	54¾	43½	154½	141	83
North Geelong ...	232	45	110¼	53	162¾	137¼	52	89½	114½	162½	42	109¼	196¼	186½	40
Ouyen	442	280¼	231¼	219¼	372¾	218	289	235¾	210¾	298½	261¼	163	317¼	270	285¼
Toolamba	157¾	101½	207½	168¼	88½	96¼	92¾	144	119	274¾	186¾	148¾	293½	41¾	104¾
Border Junctions															
Albury	—	188¾	312¾	255½	69¼	226½	180	233¼	249¼	380	274	279	398¾	199½	192
Tocumwal	210¾	154½	260½	221¼	141½	149¼	145¾	197	172	327¾	239¾	201¾	346½	94¾	157¾
Serviceton	468¾	280	156	213¼	399½	267¼	288¾	269½	244½	175¾	235¾	224¾	97½	321¾	283¼
Mt. Gambier Border	461¾	273	149	206½	392½	260¼	281¾	262½	237½	81¾	202¼	217¼	160	314¾	276½
Pinnaroo Border ...	522½	360¾	311¾	299¾	453¼	298½	369½	316¼	291¼	379	341¾	243½	397¾	350¼	365¾
Oaklands	147¾	198	322	264¾	78½	235¾	189¼	242½	258½	389¼	283¾	288¼	408	208¾	201¼

* Junction of Northern and South-Western lines.

The following table shows the mileages for use in respect of goods or live stock traffic between Junction stations and from Junction stations to Intersystem Border stations—*continued.*

	Heathcote Junct.	Inglewood	Irrewarra	Korong Vale	Linton Junct.	Mangalore	Maroona	Maryborough	Murtoa	North Ballarat	North Bendigo Junct.	North Creswick	North Geelong	Ouyen	Toolamba
Local Junctions															
Albion	31½	122½	93¼	142½	69¼	66¼	137¼	103½	178¼	67½	93½	79½	45	280¼	101½
Ararat	155½	93	99¾	113	54½	190¼	13¼	54½	54¼	56½	112½	68½	110¼	231¼	207½
Ballarat	98¼	81	63½	101	2¾	133	70½	42½	111½	¾	100½	12¾	53	219¼	168¼
Benalla	88	185	211	205	189	53¼	256¾	214	297¾	187	156	199	162¾	372¾	88½
Bendigo	69¼	30¼	161¼	50¼	100¼	104	124½	56¾	165½	98½	1¼	86½	137¼	218	96¼
Broadmeadows ...	22¾	119¾	100¼	139¾	78¼	57½	146	112¼	187	76¼	90¾	88¼	52	289	92¾
Carlsruhe	76	78	121¼	98	60½	110¾	126¾	59	167¾	58½	49	46½	89½	235¾	144
Castlemaine	92	53	138½	73	77¾	126¾	101¾	34	142¾	75¾	24	63¾	114½	210¾	119
Coleraine Junction ...	222¾	160¼	135	180¼	121¾	257½	54	121¾	110	123¾	179¾	135¾	162½	298½	274¾
Cressy	116¾	123	20	143	40¾	151½	66½	84½	134	42¾	142½	54¾	42	261¼	186¾
Dunolly	121¾	24¾	118¼	44¾	57½	156½	81½	13¾	122½	55½	53¾	43½	109¼	163	148¾
East Natimuk ...	241½	179	185¾	199	140½	276¼	99¼	140½	31¾	142½	198½	154½	196¼	317¼	293½
Echuca	111¾	82¼	215¾	102¼	155	77	179	111¼	220	153	53¼	141	186½	270	41¾
*Footscray	34¾	127½	88¼	147½	73	69½	140¾	108½	181¾	71	98½	83	40	285¼	104¾
Heathcote Junction ...	—	97	123	117	101	34¾	168¾	126	209¾	99	68	111	74¾	284¾	70
Inglewood	97	—	143	20	82¼	131¾	106¼	38½	147¼	80¼	29	68¼	134	187¾	124
Irrewarra	123	143	—	163	60¾	157¾	86½	104½	154	62¾	162½	74¾	48¼	281¼	193
Korong Vale	117	20	163	—	102¼	151¾	126¼	58½	167¼	100¼	49	88¼	154	207¾	144
Linton Junction ...	101	82¼	60¾	102¼	—	135¾	67¾	43¾	108¾	2	101¾	14	55¾	220½	171
Mangalore	34¾	131¾	157¾	151¾	135¾	—	203½	160¾	244½	133¾	102¾	145¾	109½	319½	35¼
Maroona	168¾	106¼	86½	126¼	67¾	203½	—	67¾	67½	69¾	125¾	81¾	108½	244½	220¾
Maryborough	126	38½	104½	58½	43¾	160¾	67¾	—	108¾	41¾	58	29¾	95½	176¾	153
Murtoa	209¾	147¼	154	167¼	108¾	244½	67½	108¾	—	110¾	166¾	122¾	164½	285½	261¾
North Ballarat ...	99	80¼	62¾	100¼	2	133¾	69¾	41¾	110¾	—	99¾	12	53¾	218½	169
Nth. Bendigo Junction	68	29	162½	49	101¾	102¾	125¾	58	166¾	99¾	—	87¾	138½	216¾	95
North Creswick ...	111	68¼	74¾	88¼	14	145¾	81¾	29¾	122¾	12	87¾	—	65¾	206½	181
North Geelong ...	74¾	134	48¼	154	55¾	109½	108½	95½	164½	53¾	138½	65¾	—	272¼	144¾
Ouyen	284¾	187¾	281¼	207¾	220½	319½	244½	176¾	285½	218½	216¾	206½	272¼	—	311¾
Toolamba	70	124	193	144	171	35¼	220¾	153	261¾	169	95	181	144¾	311¾	—
Border Junctions															
Albury	157¼	254¼	280¼	274¼	258¼	122½	326	283¼	367	256¼	225¼	268¼	232	442	157¾
Tocumwal	123	177	246	197	224	88¼	273¾	206	314¾	222	148	234	197¾	364¾	53
Serviceton	311½	249	255¾	269	210½	346¼	169¼	210½	101¾	212½	268½	224½	266¼	387¼	363½
Mt. Gambier Border	304½	242	216¾	262	203½	339¼	135¾	203½	191¾	205½	261½	217½	244¼	380¼	356½
Pinnaroo Border ...	365¼	268¼	361¾	288¼	301	400	325	257¼	366	299	297¼	287	352¾	80½	392¼
Oaklands	166½	263½	289½	283½	267½	131¾	335¼	292½	376¼	265½	234½	277½	241¼	451¼	167

* Junction of Northern and South-Western lines.

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VICTORIAN RAILWAYS

VICTORIAN RAILWAYS COMMISSIONERS

BY-LAW No. 375

GOODS RATES BOOK

Volume II

RATES AND CONDITIONS FOR THE CARRIAGE OF EXPLOSIVES AND GOODS OF A DANGEROUS NATURE.

No. 927.—3266/54—Price 4/6.

VICTORIAN RAILWAYS

GOODS RATES BOOK

VOLUME II

Rates and Conditions for the Carriage of

EXPLOSIVES AND GOODS OF A DANGEROUS NATURE

To Take Effect On And From 1st December, 1954

The Commissioners hereby notify that the Rates and Conditions, Classifications, and Charges shown herein are subject to alteration at any time.

To be read and construed with Goods Rates Book—Volumes I and III.

All Rates, Conditions, Classifications and Charges for the Carriage of Explosives and Goods of a Dangerous Nature issued prior to 1st December, 1954, are cancelled.

Published by the authority of the Government of the State of Victoria :
M. L. G. McKenzie
Manager
Victorian Railways Printing Works, North Melbourne

1954

VICTORIAN RAILWAYS

VICTORIAN RAILWAYS COMMISSIONERS

BY-LAW No. 375

The Victorian Railways Commissioners, in pursuance of the powers conferred in that behalf by the Railways Acts, do hereby make the following By-Law, and do hereby repeal all previous By-Laws, Conditions, Practices, Instructions, Classifications, Rates and Charges whatsoever as relate thereto or conflict therewith.

The several Rates and Charges which shall be imposed in respect of the carriage of Goods, Wool and Live Stock over the Victorian Railways, and the Classification and Conditions under which such Goods, Wool and Live Stock will be carried and the charges for other services shall be as set forth in Volumes I., II. and III. of this By-Law.

The provisions of this By-Law shall become effective as from 1st December, 1954.

In witness whereof the Common Seal of the Victorian Railways Commissioners was affixed hereto this 29th day of September, in the year of our Lord One thousand nine hundred and fifty-four in the presence of—

(SEAL) R. G. Wishart, O. G. Meyer, E. H. Brownbill } Victorian Railways Commissioners

Confirmed by the Governor-in-Council

the fifth day of October, 1954.

A. Mahlstedt
Clerk of the Executive Council.

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PART I.

Conditions for the Carriage of Explosives and Goods of a Dangerous Nature.

1. (a) The words and expressions used in the following regulations shall respectively have and include the several meanings assigned to them or defined in the Railways Acts and any Act relating to explosives or goods of a dangerous nature and in the Regulations and By-laws made under any of the foregoing Acts.

(b) The word "Commissioners" used in this By-law shall mean the Railways Commissioners or Commissioner in whom the railway premises or railways on or over which the explosives and/or dangerous goods are to be handled or conveyed, are vested or controlled.

(c) "Explosive" means any explosive the transport of which is authorised under any Act relating to explosives or any Regulation, By-law or Order made thereunder. The carriage of any explosive which is not an "authorised" explosive is absolutely prohibited.

(d) "Goods of a Dangerous Nature" means any goods of a dangerous nature and includes any inflammable liquid or dangerous goods, other than explosives, defined as such in any Act or in any Regulation or By-law made thereunder, goods referred to in this By-law as being of a dangerous nature, and goods deemed by an officer of the Commissioners receiving the same for carriage to be of a dangerous nature.

(e) "Owner" when used in relation to explosives and to goods of a dangerous nature, includes any consignor, consignee or any agent for the sale, custody, loading or unloading thereof.

(f) "Explosives Van" means a vehicle, partitioned or otherwise (the design of which meets with the approval of the Government Authority or Department controlling explosives), owned by the Commissioners and maintained for the carriage of explosives.

(g) "Portable Magazine" means a receptacle for explosives, substantially constructed of hardwood and galvanised iron, or other approved materials, in such a manner that no iron or steel shall be exposed to view on the interior side thereof, and the lid of which is closely fitting and secured in an approved manner. Its construction shall be such as will effectively protect the explosives which it contains from accident by fire from without. It shall be capable of being carried when full by two men and shall be approved by the Government Authority or Department controlling explosives.

2. (a) The Commissioners are not common carriers of explosives or of goods of a dangerous nature, and hereby give notice that they do not undertake the carriage thereof except on the following special conditions. Explosives and goods of a dangerous nature will only be received and carried subject to the Railways Acts and any Act relating to explosives or goods of a dangerous nature and the Regulations and By-laws made under any of the foregoing Acts and upon the express condition that the Commissioners shall be

(i) exempt from all liability of whatsoever nature howsoever arising in respect of loss of or damage to the explosives or goods of a dangerous nature and

(ii) compensated by the sender for any loss or damage in any way caused by the said explosives or goods of a dangerous nature to the Commissioners' property and indemnified by the sender against any claim in respect of loss, damage or injury in any way caused by the said explosives or goods of a dangerous nature to any person or the property of any person, unless such loss, damage or injury arises from wilful misconduct on the part of the Commissioners' servants.

(b) The carriage of any explosive, other than an explosive within the meaning of this By-law, is absolutely prohibited.

3. The Commissioners do not guarantee to receive explosives or goods of a dangerous nature at any time, either on appointed days or other days, unless the necessary railway vehicles are available in which to place them and they reserve to themselves the right to delay the despatch of any consignment as long as they deem proper.

4. The Commissioners do not guarantee to despatch explosives or goods of a dangerous nature at any particular time or by any particular train, nor will they allow consignments, whether carried or to be carried, to remain on railway premises longer than is necessary. If packages containing explosives or goods of a dangerous nature carried by railway be not accepted and removed by the consignee within one working day, they may be removed by the Commissioners at the owner's risk and expense, or otherwise disposed of as they think fit, or returned to the sender who will be required to remove them at once from the premises and pay the whole charges incurred in connection therewith.

5. The Commissioners may refuse to receive, forward, carry, or allow to be brought or carried upon any railway under their control, any vehicle or package which they suspect to be packed or sent, or to contain any article or thing packed or sent in contravention of this By-law or any Acts and Regulations relating to explosives or goods of a dangerous nature; and in case any vehicle or package which the Commissioners suspect to be so packed or sent, or to contain any such article or thing as aforesaid, shall be on any railway under the control of the Commissioners, the Commissioners may open or require such vehicle or package to be opened to ascertain the fact.

6. The Commissioners may at any time during the conveyance of explosives or goods of a dangerous nature, or while they are on the premises of the Commissioners, detain and stop further handling and delivery of packages which appear defective. The Commissioners may require the owner, sender or consignee to remove the goods forthwith from their premises or may remove and store such packages at the cost and risk of the owner.

7. No package or vehicle containing explosives or goods of a dangerous nature, which the Commissioners may by any notice or regulation for the time being in force notify that they will not receive, forward, or carry, shall be delivered to the Commissioners for conveyance, or be brought, sent or forwarded to or be left upon any railway under the control of the Commissioners.

Any package or vehicle containing explosives or goods of a dangerous nature which is in a leaking condition, or unfit for transit, shall be removed immediately from the railway premises by the owner, sender or consignee on instructions issued by the Commissioners' servants.

8. No person shall send to the Commissioners any consignment of explosives for conveyance unless he has given the Commissioners such notice as the Commissioners may require, and has had an intimation from the Commissioners that they are prepared to receive such consignment. The information required of a person's intention to send such a consignment shall comprise the true name, description and quantity of the explosives proposed to be conveyed, the sender's own name and address, and the name and address of the proposed consignee.

9. Each consignment of explosives or goods of a dangerous nature shall be accompanied by a consignment note on a form provided for the purpose (see page 109), stating the true name, description and quantity to be conveyed, the name and address of the sender, the name and address of the consignee and the name of the railway station to which such explosives or goods of a dangerous nature are to be conveyed. Such consignment note shall be handed to the receiving officer to whom the goods are delivered. The consignment note for explosives shall bear a declaration by the consignor that the explosives referred to therein are in good order and condition, and are packed and marked in accordance with the Acts, By-laws and Regulations dealing with explosives.

A declaration on the consignment note for goods of a dangerous nature shall be made that the goods have been securely packed and marked as prescribed in the relevant Acts, Regulations and By-laws. Where alternative methods of packing to those prescribed in the relevant Acts, Regulations and By-laws have been permitted, the authority under which this permission was granted shall be shown on the consignment note.

Goods of a dangerous nature packed in containers which do not comply with the requirements of this By-law may be accepted for carriage when consigned direct from a ship to a railway destination station, subject to the following conditions:—

(i) That the containers are in good condition and free from leaks.

(ii) That the authority of the Chief Traffic Manager has been obtained to despatch such consignment. The terms of such authority must be shown on the consignment note.

10. Consignments of explosives or goods of a dangerous nature shall be sent to the forwarding station, and shall be loaded or unloaded only at such times during such hours between sunrise and sunset as the Commissioners may appoint, and every such consignment proposed to be carried by rail shall immediately on the arrival thereof be delivered to the employe or employes of the Commissioners authorised to receive explosives or goods of a dangerous nature, unless this provision shall be varied by the written authority of the Commissioners or the Chief Traffic Manager.

11. Goods of a dangerous nature shall not be offered for conveyance unless packed in such a manner that no part of the contents can escape under any reasonable condition of transport, and shall be packed and marked as prescribed in Parts III, IV and V of this By-law, so as to indicate clearly the nature of the contents of such packages.

In the event of senders neglecting to mark each package according to this By-law, the Commissioners may decline to carry same, and direct its removal, and if not removed the package and contents may be forthwith removed by the Commissioners, or otherwise disposed of as they think fit for securing the public safety, at the risk and expense of the owner.

12. No person shall smoke, or have in his possession or under his control while on or near any vehicle in, on, or from which explosives or goods of a dangerous nature are being loaded, conveyed or unloaded, any fire, light or matches or other means of ignition, other than a fire or light approved by the Commissioners, nor commit any act likely to cause fire or explosion.

13. All explosives shall be marked conspicuously with the word "Explosive" followed by the name of such explosive and all goods of a dangerous nature shall be marked conspicuously with the word "Danger" followed by the name of such goods.

PART II.

Explosives.

IN EXPLOSIVE VANS OR PORTABLE MAGAZINES EXCEPT WHERE OTHERWISE SPECIFIED BY MIXED AND GOODS TRAINS.

1. All explosives taken on to any railway premises or offered for transport shall be packed as required by the Explosives Acts and the Regulations thereunder and to the satisfaction of the Railway Commissioners.

2. Except in the case of Safety Cartridges, Safety Fuse for blasting, Railway Fog Signals, Percussion Caps and Manufactured Fireworks, there shall not be any iron or steel exposed on any packages containing explosives.

3. Any package which is defective or insecure shall be forthwith removed by the owner, consignor, consignee or their agents, on instructions issued by any of the Commissioners' employes.

MARKING AND LABELLING.

4. Packages of explosives shall have affixed to them in conspicuous characters by means of a brand or securely attached label, the word "EXPLOSIVE," the class and division of the explosive, the name of the explosive, the name of the sender, and the name and address of the consignee.

CLASSIFICATION OF EXPLOSIVES.

The classification, marking and examples of authorised explosives which may be conveyed are as follows:—

Class 1.—GUNPOWDER. Each package to be marked conspicuously:—

EXPLOSIVE CLASS 1 GUNPOWDER Name of Manufacturer or Sender

Class 2.—NITRATE MIXTURE. Each package to be marked conspicuously:—

EXPLOSIVE CLASS 2—NITRATE MIXTURE NAME OF EXPLOSIVE Name of Manufacturer or Sender

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Class 3.—NITRO COMPOUND (Two Divisions). **Liquid Nitro Glycerine shall not be tendered for or accepted for transport by rail under any circumstances.**

Division 1.—Division 1 comprises any explosive which consists **partly of nitro glycerine,** e.g., Blasting Gelatine, Gelatin Dynamite, Gelignite, Quarry Monobel, Ajax, and Dynobel No. 2. Each package to be marked conspicuously:—

EXPLOSIVE CLASS 3—NITRO COMPOUND Division 1 NAME OF EXPLOSIVE Name of Manufacturer or Sender

Division 2.—Division 2 comprises any explosive and any nitro compound as defined in the Explosives Acts and Regulations thereunder which is not in the first division, e.g., Amberite No. 2, E. C. Sporting Powder and Smokeless Diamond Powder. Each package to be marked conspicuously:—

EXPLOSIVE CLASS 3—NITRO COMPOUND Division 2 NAME OF EXPLOSIVE Name of Manufacturer or Sender

Class 4.—CHLORATE MIXTURE (Two Divisions).

Division 1.—Division 1 comprises any chlorate preparation which consists partly of nitro glycerine or other liquid nitro compound. Each package to be marked conspicuously:—

EXPLOSIVE CLASS 4—CHLORATE MIXTURE Division 1 NAME OF EXPLOSIVE Name of Manufacturer or Sender

Division 2.—Manufactured chlorate mixtures of this Division (Rack-a-Rock, Rend Rock and Lithyte) shall not be tendered for or accepted for transport by rail under any circumstances.

Class 5.—FULMINATE (Two Divisions). **This class, due to its dangerous nature, shall not be tendered for or accepted for transport by rail without the consent in writing of the Chief Inspector of Explosives.**

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Class 6.—AMMUNITION (Three Divisions).

Division 1.—Ammunition that will not explode in bulk, e.g., Multiple Safety Fuse Igniters, Percussion Caps, Railway Fog Signals, Safety Cartridges, Safety Fuse and Safety Instantaneous Fuse. Each package to be marked conspicuously:—

EXPLOSIVE
CLASS 6—AMMUNITION
Division 1
NAME OF EXPLOSIVE
Name of Manufacturer or Sender

Division 2.—Ammunition that may explode in bulk but does not contain its own means of ignition, e.g., Detonating Fuse (which includes Prima Cord, Cordtex and Cordeau Bickford). Each package to be marked conspicuously:—

EXPLOSIVE
CLASS 6—AMMUNITION
Division 2
NAME OF EXPLOSIVE
Name of Manufacturer or Sender

Division 3.—Ammunition that will explode in bulk and contains its own means of ignition, e.g., Detonators, Electric Detonators, and Electric Delay Action Detonators. Each package to be marked conspicuously:—

EXPLOSIVE
CLASS 6—AMMUNITION
Division 3
NAME OF EXPLOSIVE
Name of Manufacturer or Sender

Class 7.—FIREWORKS (Three Divisions).

Division 1.—Firework composition. **Not to be tendered for or accepted for transport by rail under any circumstances.**

Divisions 2 and 3.—Manufactured Fireworks. Each package to be marked conspicuously:—

EXPLOSIVE
CLASS 7—FIREWORKS
Division 2 or 3
NAME OF EXPLOSIVE
Name of Manufacturer or Sender

5. The Stationmaster or such other person or employe in charge of the station at the time of receipt and delivery is authorised by the Commissioners to carry out the By-laws, rules and instructions necessary to the safe handling and carriage of explosives, including the proper direction of all employes and other persons concerned.

6. While on railway premises to assist in the despatch or receipt of explosives, consignors, consignees and their employes shall observe strictly the instructions of the local railway employe authorised to conduct the safe handling, loading and despatch of explosives traffic, and do everything in their power to assist him in carrying out his duties.

7. While the loading, unloading, stowing or conveyance of explosives is in progress, each person engaged in such loading, unloading, stowing or conveyance must observe all necessary precautions for the prevention of accident by fire or explosion; must not allow any unauthorised person to have access to such goods; must abstain from smoking or any act whatever which may tend to cause fire or explosion, and is not necessary to the loading, unloading, stowing or conveyance of explosives; and must prevent any other person from smoking or committing any such act. Further, he shall not have upon his person any matches or means of striking a light.

NOTES ON PERMISSIBLE GROUPING OF EXPLOSIVES IN RAILWAY TRUCKS FOR TRANSPORT BY RAIL.

8. Subject to the Regulations under the Explosives Acts the following conditions will apply:—

(a) Unless otherwise stated, explosives must be loaded only into explosives vans, or, when in portable magazines, into ordinary trucks for transport by goods train.

(b) No explosive shall be loaded into a railway vehicle which contains any other type of dangerous goods or inflammable material.

(c) All explosives grouped in one Class and Division may be loaded together in the one vehicle; but unless otherwise provided for shall not be loaded with explosives of any other Class or Division.

(d) Explosives of Class 1, Class 2, Class 3 and Class 4, Division 1, and such explosives in Class 6, Division 2, as may be authorised from time to time, may be carried together in the same vehicle, providing there is no exposed iron or steel on the packages.

(e) Explosives of Class 6, Division 1, need not be loaded into an explosives van or a portable magazine, but may be accepted if packed in strong wooden cases approved by the Explosives Department and forwarded in ordinary railway goods trucks provided no other explosives are in the trucks.

(f) Explosives of Class 7, Division 2 (Large Fireworks) are only to be accepted at approved stations, and are only to be conveyed in explosives vans, unless exempted by a special written authority. These explosives to be packed in sealed tins enclosed in strong wooden cases.

(g) Explosives of Class 7, Division 3 (Small Fireworks), if packed in strong wooden cases lined with tin, galvanised iron or zinc, or wrapped in waterproof and siftproof material and enclosed in strong wooden cases, or, if imported, in their original packages, may be accepted at any station and forwarded in ordinary railway goods trucks.

(h) Sporting Powder not exceeding 5 lb. in weight if packed in tins in metal lined cases and secured against movement, may be accepted at approved stations.

(i) A limited quantity of safety cartridges Class 6, Division 1, may be received at Spencer Street and Flinders Street Parcels Offices, and despatched to country stations by passenger train.

(j) Except where otherwise specified explosives may be received for carriage only at approved stations and only on such days as may be appointed from time to time.

(k) Cordite cartridges packed in the Commonwealth Navy Department's own magazines may be accepted for despatch from Crib Point to Sydney.

PART III.

SECTION NO. 1.

Compressed Gases.

1. Compressed gases will be accepted for transport if containers comply with one of the following requirements in all details:—

(a) Steel cylinders manufactured to the appropriate Australian Standard Specification for the particular gas, tested, maintained and filled in accordance with the requirements of the Australian Standard No. CB.4—S.A.A. Code for Compressed Gas Cylinders.

(b) Steel cylinders manufactured to a Standard Specification approved by the Standards Association of Australia for the particular gas (as listed in the Australian Standard No. CB.4—S.A.A. Code for Compressed Gas Cylinders) tested, maintained and filled in accordance with the requirements of the Australian Standard No. CB.4—S.A.A. Code for Compressed Gas Cylinders.

(c) Large gas welded steel drums with capacity up to 20 cwt. manufactured to a recognised specification approved by the Commissioners and tested, maintained and filled in accordance with such specification or approval. These drums must be securely mounted on suitable flat trucks, but may be removed for filling and emptying, provided adequate lifting appliances are available. These drums are suitable for compressed liquifiable gases such as Chlorine and Sulphur Dioxide (see Packing Clause No. 138).

(d) Cylinders which do not comply fully with the requirements of the Australian Standard No. CB.4—S.A.A. Code for Compressed Gas Cylinders, but for which the Gas Cylinder Sectional Committee of the Standards Association of Australia has issued a permit under special restricted conditions for their continued use.

Such cylinders will be covered as far as rail transport is concerned by a special exemption authority. In applying for such exemption authority, to be issued by the Commissioners, the consignor must clearly state the number of the Standards Association of Australia Gas Cylinder Committee's ruling on the subject. The number of the Special Exemption Authority which may be of a tentative nature shall be quoted as part of the certificate given on the Consignment Note for Goods of a Dangerous Nature.

(e) Privately owned tank cars approved by the Commissioners.

2. Filled cylinders imported into Victoria, if constructed to an acceptable specification, but where a test is overdue on the basis of Australian practice, will be carried to their primary destination, but before refilling in this country must be tested in accordance with the Australian Standard No CB.4—S.A.A. Code for Compressed Gas Cylinders.

3. Cylinders containing Hydrocyanic Acid Gas, anhydrous—STABILISED, as well as those containing the gas in solution (see Poisonous Materials, Part III, Section No. 5), nitrogen peroxide (di-oxide and tetroxide), phosgene, and other highly poisonous gases, will not be accepted for conveyance by rail unless accompanied by the written authority of the Chief Traffic Manager.

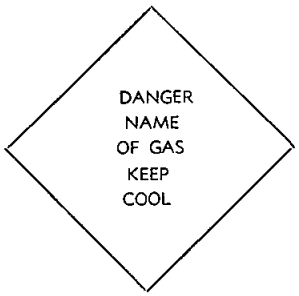
The application for the permit to carry highly poisonous gases for which written authority of the Chief Traffic Manager is required and for which Packing Clause No. 139 is prescribed, shall include a signed declaration that all the supplementary conditions in Packing Clause No. 139 have been fully complied with and shall give particulars and numbers of the cylinders involved. The Permit when granted shall be submitted with the Consignment Note for Goods of a Dangerous Nature, when cylinders are tendered for transport.

4. THE CARRIAGE OF UNSTABILISED ANHYDROUS HYDROCYANIC ACID IS STRICTLY PROHIBITED.

5. The Commissioners may at any time refuse to accept for transport by rail any cylinder or other type of container which they consider unsatisfactory.

6. Consignors of compressed gases shall complete the Consignment Note for Goods of a Dangerous Nature, giving a written certificate that the relevant conditions have been complied with in all respects.

7. LABELLING.—In addition to any label denoting the specific risk of the compressed gas (see Part III, Sections Nos. 2-6) the name of the gas and the words "Danger—Keep Cool" shall appear either in the label or as a separate label. In the case of non-inflammable, non-poisonous gases, the cylinders shall have affixed a GREEN label at least 4 inches square, with BLACK lettering thus:—



SECTION NO. 2.

INFLAMMABLE MATERIALS.

DEFINITIONS.

(I) Inflammable Liquid means any liquid having a true closed cup flash point of less than 150° Fahrenheit (65.6° Centigrade) as determined by the Standard Methods laid down by the British Institute of Petroleum, I/P. 33/44, 34/47, or subsequent revisions.

(II) Inflammable Solid means any solid or semi-solid which, because of its readily combustible properties, constitutes a fire hazard.

INFLAMMABLE LIQUIDS.

1. For the purpose of transportation by railway, inflammable liquids are divided into two groups as follows:—

Group "A": Highly inflammable, having a true Flash Point of less than 73° Fahrenheit.

Group "B": Inflammable, having a true Flash Point of not less than 73° Fahrenheit, and not more than 150° Fahrenheit.

2. All containers whether full or empty shall be sound and sealed so that they are liquid and vapour tight.

3. All inflammable liquid taken on to any railway premises shall be packed and marked as prescribed in this By-law.

4. No person shall permit, suffer or allow inflammable liquid to be unloaded unless or until the ship, boat, vehicle, or other receptacle or appliance by which the inflammable liquid is to be removed is in readiness to receive same, unless specially authorised by the Commissioners' employe.

5. No inflammable liquid shall be loaded or unloaded on any railway premises by the consignor or consignee thereof or their servants—

(a) except between the hours of sunrise and sunset without the permission in writing and under conditions approved by the proper officer of the Commissioners;

(b) unless and until the vehicle in which it is to be loaded, or from which it is to be unloaded, shall have been thoroughly ventilated by opening doors and allowing sufficient time for the escape of any vapours;

(c) unless and until all the pipes and other appliances used for handling in bulk are free from leakage;

(d) except under other conditions as may be imposed by the proper officer of the Commissioners.

6. Tank trucks for the conveyance of inflammable liquids shall be of a design approved by the Commissioners.

MARKING AND LABELLING.

7. Outer packages must be marked in conspicuous type, with the name of the contents and the name and address of the sender, and have affixed a RED label with BLACK lettering not less than 4 inches square, thus:—

8. In addition, outer packages containing inflammable liquids Group "A" must be marked in conspicuous type with the words "HIGHLY INFLAMMABLE."

INFLAMMABLE SOLIDS.

1. All containers of inflammable solids shall be sound and free from leaks.

2. All inflammable solids taken on to any railway premises shall be packed and marked as prescribed in this By-law.

3. All empty containers must be thoroughly cleansed before being offered for transport.

MARKING AND LABELLING.

The marking and labelling of containers or packages of inflammable solids shall be similar in all respects to that prescribed in this By-law for inflammable liquids.

SECTION NO. 3.

MATERIALS REACTING DANGEROUSLY WITH AIR OR WATER.

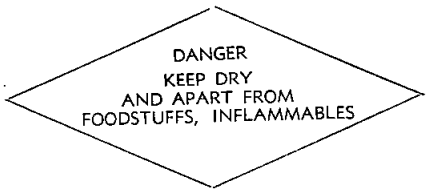
1. All containers of materials which will react dangerously when in contact with air or water shall be sound and hermetically sealed so that they will remain moisture and air-tight.

2. All materials which react dangerously with air or water taken on to any railway premises, shall be packed and marked as prescribed in this By-law.

3. All empty containers must be thoroughly cleansed before being offered for transport.

MARKING AND LABELLING.

4. Outer packages must be marked in conspicuous type with the name of the contents, the name and address of the sender, and also whether "INFLAMMABLE," "CORROSIVE" or "POISONOUS" and have affixed a YELLOW label at least 4 inches long by 2 inches wide with BLACK lettering, thus:—



SECTION NO. 4.

CORROSIVE MATERIALS - LIQUIDS AND SOLIDS.

1. All containers for corrosive materials shall be sound and leak proof. Corrosive materials shall not be placed with those classed as incompatibles (see Part IV). Nitric Acid and Sulphuric Acid must not be enclosed in the same outer package.

2. All stoppers, plugs, bungs, or lids must be luted, tied down, or secured in such a way that they cannot leak or spill any of the contents of the containers during conveyance.

3. Bottles with stoppers ground to fit, must have the stoppers held in place by Plaster of Paris covered by strong cloth, securely tied. Bottles having necks with moulded screw threads must be closed with elastic composition cushions, and with glass, porcelain or similar liners which must be impervious to the acid. Such caps must be secured in place by at least one complete continuous thread, and must be capable of preventing any leakage of the contents.

4. Cushioning materials must be of the type specified herein unless the approval of the Commissioners is first obtained for any other type to be used.

5. Returnable containers shall be thoroughly examined before re-use, and cushioning material and containers must be in an efficient condition when brought on to railway premises.

6. All empties must be thoroughly cleansed or resealed before being brought on to railway premises for transport.

7. All corrosive materials taken on to any railway premises shall be packed and marked as prescribed in this By-law.

MARKING AND LABELLING.

8. Outer packages must be marked in conspicuous type with the name of the contents, the name and address of the sender, and have affixed a BLUE label at least 4 inches square with WHITE lettering, thus:—

SECTION NO. 5.

POISONOUS MATERIALS.

1. All containers for poisonous materials shall be sound and free from leaks.

2. All poisonous materials taken on to any railway premises shall be packed and marked as prescribed in this By-law.

3. All empty containers must be thoroughly cleansed before being brought on to railway premises for transport.

MARKING AND LABELLING.

4. Outer packages must be marked in conspicuous type with the name of the contents, the name and address of the sender, and have affixed a WHITE label at least 4 inches square, with RED lettering, thus:—

SECTION NO. 6.

OXIDISING MATERIALS.

1. All containers for oxidising materials must be sound and free from leaks.

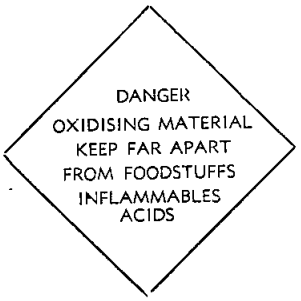
2. Packages must not contain any organic substance mixed with oxidising materials.

3. All empties must have been thoroughly cleansed before being brought on to railway premises for transport.

4. All oxidising materials taken on to any railway premises shall be packed and marked as prescribed in this By-law.

MARKING AND LABELLING.

5. Outer packages must be marked in conspicuous type with the name of the contents, the name and address of the sender, and if poisonous, with the word "POISON" and have affixed a YELLOW label at least 4 inches square, with BLACK lettering, thus:—



PART IV.

Alphabetical List and Freight Classification of Goods of a Dangerous Nature.

1. Goods of a dangerous nature are classified under six sections (see Part III hereof) and the following list contains a column showing the section which is applicable to each article of a dangerous nature

2. None of the articles specified in the following alphabetical list may be carried in a PASSENGER TRAIN OR A GUARD'S VAN with the exception of limited quantities of the undermentioned, which shall be packed and marked as described hereunder:—

ACCUMULATORS (AND CHARGED BATTERIES).

Cells constructed of glass or other material must be sealed around the top edges with pitch or other suitable material, and the filling holes securely plugged with approved splash proof stoppers.

Cells must be well insulated to prevent short circuits, and packed as a neat fit in strong wooden cases with gable tops or in strong, double walled corrugated or solid fibre-board containers with gable tops, of sufficient strength to withstand the weight of the contents.

Any packing material or dunnage used must be of a non-conductive nature, and under no circumstances shall any article or articles other than the batteries and packing material or dunnage be contained in the cases or fibre containers.

The outer container to be marked or labelled in distinctive characters "BATTERY—CONTAINING CORROSIVE ELECTROLYTE. THIS SIDE UP," and have affixed the BLUE label with WHITE lettering for corrosive materials described in Part III, Section No. 4.

The maximum weight of a package must not exceed 60 lbs. The maximum number of packages carried by any one passenger train shall not exceed six.

Consignors must certify in writing on the consignment note that the charged batteries or accumulators described thereon have been sealed and packed in accordance with the specifications of the Railways Commissioners.

AMMONIA GAS.

Cylinders must comply with the requirements of Part III, Section 1.

A permit, the terms of which must be shown on the consignment note, shall be obtained from the Chief Traffic Manager for each consignment.

The number of cylinders of ammonia gas carried on any one passenger train shall not exceed one.

Cylinders must be marked and labelled as required in Part III, Section 1.

MEDICAL GASES.

Cylinders must comply with the requirements of Part III, Section 1.

Cylinders shall not exceed 52½ lbs. water capacity.

Cylinders addressed to physicians or hospitals may be accepted without a permit.

The number of cylinders of medical gases carried on any one passenger train shall not exceed five.

Cylinders must be marked and labelled as required in Part III, Section 1, and shall also be distinctively marked to indicate that the gas is to be used for medical purposes.

ETHER (SULPHURIC)

Anaesthetic ether up to 3 lbs. in weight must be packed in securely closed glass bottles, surrounded with sawdust in closed tins and packed in a strong wooden box.

The quantity of ether carried on any one passenger train shall not exceed 12 lbs.

Packages of ether (sulphuric) shall be marked and labelled as required in Part III, Section 2, for highly inflammable liquids.

SAMPLES OF INFLAMMABLE LIQUIDS.

(a) Samples of inflammable liquids with a true flash point below 73° Fahrenheit listed as inflammable liquids Group "A" in the alphabetical list, with the exception of Carbon Bisulphide (disulphide), Nickel Carbonyl, Spirits of Nitroglycerin and Zinc Ethyl, may be accepted for despatch by passenger train when packed in metal containers not less than 30 B.G. (0.012") thickness and not exceeding one quart capacity, solder sealed, tightly fitting in individual cases, or packed with sawdust or other cushioning material in individual cases, provided that the maximum quantity of inflammable liquids with a true flash point below 150° Fahrenheit does not exceed one gallon on any one train.

(b) Samples of inflammable liquids with a true flash point of not less than 73° Fahrenheit listed as inflammable liquids Group "B" in the alphabetical list, may be accepted for despatch by passenger train when packed in metal containers not less than 30 B.G. (0.012") thickness and not exceeding one gallon capacity, positively sealed against leakage by screwed cap and gasket or soldered seal, tightly fitting in a wooden case or packed in sawdust in a wooden case, provided that the maximum quantity of inflammable liquids with a true flash point below 150 Fahrenheit° does not exceed one gallon on any one train.

(c) Outer packages must be labelled and marked in accordance with requirements of Part III, Section 2, for INFLAMMABLE MATERIALS.

PAINTS AND VARNISHES.

(a) In tins, sealed with at least three spots of solder or approved type metal clips, not exceeding 1 gallon capacity, packed in a strong wooden case or strong fibreboard container, well cushioned with sawdust or other absorbent material. Tinplate not to be less than 30 B.G. (0.012") thickness.

Quantity in any one package not to exceed 1 gallon.

(b) In sealed metal drums, not exceeding 6 gallons capacity, constructed of 24 B.G. (0.025") minimum thickness sheet steel.

Full open head drums are acceptable provided the head is securely fastened and leak proof.

Lacquers.—In tins, sealed with at least three spots of solder or approved type metal clips, not exceeding 1 gallon capacity, packed in a strong wooden case or strong fibreboard container, well cushioned with sawdust or other absorbent material. Tinplate not to be less than 30 B.G. (0.012") thickness.

Quantity in any one package not to exceed 1 gallon.

Each container must be marked and labelled as required in Part III, Section 2, of the Commissioners' By-law for Highly inflammable liquids.

The maximum quantity on one train shall not exceed 10 gallons.

Thinners.—Not accepted.

FILMS, NITROCELLULOSE BASE.

Must be packed in closed metal cans each holding not more than 2,000 feet, the metal cans to be packed in a metal box lined with material which is a non-conductor of heat. The gross weight of each metal box must not exceed 2 cwt.

or

Metal cases lined throughout with hard faced fibreboard (at least 0.125" in thickness), hinged and fastened as set out in Part VB, Specification 8A or 8B of the Commissioners' By-law. Where metal partitions are used, they are to be the same thickness as the body of the case and lined the same as the case;

or

A metal can with a securely fitting lapped detachable cover packed singly in a strong fibreboard container. When not more than 500 feet of film is in the can a lap of ½" will be permitted, but when the quantity exceeds this, and up to the permitted maximum of 2,000 feet of film, the lap shall be ¾". Each fibreboard container MUST be securely bound with a strong cord.

Outer packages must be marked and labelled as prescribed in Part III, Section 2 of the Commissioners' By-law.

RADIO ACTIVE MATERIAL.

Radio active material may be accepted for carriage by passenger train or by parcels coach provided that the approval of the Chief Traffic Manager has been granted in writing for its carriage by rail, and such approval is produced to the railways' officer in charge at the railway station at which the radio active material is tendered for despatch, and that it is packed and marked in accordance with the requirements of the Commissioners' By-law. See also Part VB, Packing Clause 94, of the Commissioners' By-law.

CHEMICALS, DRUGS AND MEDICINAL PREPARATIONS IN SMALL QUANTITIES.

For details of packaging and restrictions see Part VA, Packing Clause 107, of the Commissioners' By-law. The maximum gross weight of a case containing chemicals, drugs and medicinal preparations which will be accepted for carriage by passenger train must not exceed 14 lbs.

SAFETY CARTRIDGES.

A limited quantity of safety cartridges, Class VI, Division 1, may be received at Spencer Street and Flinders Street Parcels Offices, and despatched to country stations, provided they are securely packed in a substantial wooden case, marked "Safety Cartridges" together with the name of the manufacturer or sender.

SAFETY MATCHES.

Books and boxes of safety matches in sealed tins, each tin to be packed in a strong wooden case. Each tin to contain not more than three gross books of book safety matches or a one gross parcel of ordinary safety matches.

The outer container must be labelled and marked in accordance with requirements of Part III, Section 2, for INFLAMMABLE MATERIALS.

The maximum number of packages on any one passenger train is six.

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature.

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Acaroid Resin	INFLAMMABLE SOLID (Section 2)	Cases, barrels, bags or drums	28, 52, 55, 61, 75	..	Cool; away from Batteries, Acids and Oxidising Materials	"2"
Accellerene (Paranitrosodimethylaniline)	INFLAMMABLE SOLID (Section 2) Spontaneously if dry. Poisonous.	With 50% moisture, wooden barrels, iron not allowed	51	..	Well ventilated, cool; away from Foodstuffs, Acids, Inflammables	"2"
Accumulators (and Charged Batteries)	CORROSIVE LIQUID (Section 4). Sparks from Short Circuits.	Wooden cases, fibreboard containers L.C.L. steel containers	101 150	..	Away from Inflammables, Nitric Acid, Chlorates, Saltpetre, Powdered Metals, Carbides, Explosives	*"2"
Acetaldehyde (Ethyl Aldehyde)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 41, 71, 78	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Acetate of Lead (Sugar of Lead)	POISONOUS SOLID (Section 5)	Bottles, tins or paper bags in cases, drums	5, 22, 26, 76	..	Away from Foodstuffs	"2"
Acetic Acid (under 60%) ..	Non-Dangerous	Guarantee of strength of contents of consignment must be given, otherwise to be treated as the stronger product				*"C"
Acetic Acid (over 60%) ..	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, jars in crates, wooden barrels, drums	3, 4, 41, 45, 51, 78	10%	Cool; away from Chromic Acid, Sodium, Peroxide, Nitric Acid, Oxidising Materials	*"C"
Acetic Anhydride	REACTS WITH WATER (Section 3) Inflammable, Corrosive	Bottles in cases, carboys, jars in crates	3, 4, 41, 45	10%	Cool; away from Foodstuffs, Water, Oxidising Materials	"2"
Acetic Ether (Ethyl Acetate)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Sunlight	"2"
Acetone	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	10%	Well ventilated, cool; away from Batteries, Acids	N.O.S. "2" Produce of Commonwealth on "Up" journey "C"
Acetone Oil	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"1"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Acetyl Chloride	REACTS WITH WATER (Section 3) Corrosive, Inflammable	Bottles, carboys in cases, jars in crates, drums	3, 4, 41, 45, 74	5%	Well ventilated, cool, dry; away from Alcohol, Batteries, Water	"2"
Acetylene	COMPRESSED GAS (Section 1) Dissolved, Inflammable, Anaesthetic	Steel cylinders, with porous packing Gas dissolved in Acetone	131	..	Well ventilated, cool; away from cylinders of Chlorine Gas, Combustible Material, Batteries, Acids	N.O.S. "2" Minimum 6 tons per 4 wheeled truck "1"
Acetylene Tetrachloride	..	See "Ethane Tetrachloride"				"2"
Acids	..	See under specific name of Acid				
Acids, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Acrylonitrile	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, carboys, drums	3, 4, 21, 41, 71, 78	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Air, Compressed	COMPRESSED GAS (Section 1) Permanent Mixed	Steel cylinders — High pressure, 1,800 lb. per sq. in.	132, 133, 134	..	Cool storage; away from Acids	"2"
Alcohol, Allyl	INFLAMMABLE LIQUID Group "A" (Section 2) Poisonous.	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Alcohol, Amyl	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Alcohol, Butyl	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Alcohols, Denatured	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Alcohol, Ethyl	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Alcohol, Industrial	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Alcohol, Iso-Propyl	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Alcohols, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Alcohol, Propyl	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Footstuffs	"2"
Alcohol, Tertiary	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Alcohol, Wood, Methanol ..	INFLAMMABLE LIQUID Group "A" (Section 2) Poisonous	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Aldehyde	..	See "Acetaldehyde"				"2"
Alkaline Corrosive Battery Fluid	CORROSIVE LIQUID (Section 4)	Bottles in cases, wooden casks, carboys, drums	3, 4, 41, 51, 74	5%	Cool, away from Acids, Foodstuffs	"2"
Alkaline Corrosive Battery Fluid with Battery	..	See "Accumulators"				"2"
Allyl Alcohol	..	See "Alcohol, Allyl"				"2"
Aluminium Bronze Powder ..	..	See "Aluminium, Granulated or Powdered"				"2"
Aluminium Chloride	CORROSIVE SOLID (Section 4) When Wet	Bottles or tins in cases, drums, casks	5, 22, 52, 55, 75	..	Dry; away from Liquids	"2"
Aluminium Ferro-Silicon	REACTS WITH WATER (Section 3)	Tins in cases, casks, drums, sacks	22, 28, 52, 55, 61, 75	..	Well ventilated, dry; away from Acids, Caustic Alkalies	"2"
Aluminium, Granulated or Powdered	INFLAMMABLE SOLID (Section 2) Water assists Combustion	Bottles or tins in cases, casks, drums, cases	5, 22, 26, 28, 52, 55, 75	..	Dry; away from Acids, Caustic Alkalies, Chlorates, Nitrates, Inflammables	"2"
Aluminium Liquid	..	See "Paints containing Inflammable Liquid"				"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*Continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Aluminium Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Sulphuric Acid, Sources of Ignition	"2"
Aluminium Paint or Lacquer	..	See "Paints containing Inflammable Liquid"				"2"
Aluminium Powder	..	See "Aluminium, Granulated or Powdered"				"2"
Ammonia, Liquefied, Anhydrous	COMPRESSED GAS (Section 1) Medium Pressure Liquefiable, Poisonous and Suffocating	Steel cylinders, medium pressure 412 lb. per sq. in.	134, 137	Filling Ratio 0.51	Well ventilated, cool; away from Cylinders of Chlorine, Oil	"2"
Ammonia Solution (Specific Gravity .880-.941)	POISONOUS LIQUID (Section 5)	Bottles in cases, jars in crates, drums, carboys Tank trucks	1, 3, 4, 41, 45, 74	10%	Well ventilated, cool; away from Bromine, Acids, Foodstuffs	*"2"
Ammonium Arsenate	POISONOUS SOLID (Section 5)	Bottles, tins or paper bags in cases, casks, drums	5, 22, 26, 52, 55, 76	..	Dry; away from Foodstuffs	"2"
Ammonium Bichromate	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, wooden casks, drums	11, 23, 52, 76	..	Dry; away from Combustibles, Acids, Foodstuffs	"2"
Ammonium Fluoride	REACTS WITH AIR AND WATER (Section 3) Corrosive	Bottles in cases, casks, drums	51, 52, 53, 77	..	Dry; away from Acids	"2"
Ammonium Nitrate Fertiliser	..	See "Fertiliser containing Oxidising Materials"			..	..
Ammonium Nitrate Pure, free from added Organic Matter	OXIDISING MATERIAL (Section 6) Liable to explode	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustibles, Acids, Trucks containing Explosives	"2"
Ammonium Perchlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustibles, Acids, Sulphur, Fire (may explode)	"2"
Ammonium Permanganate ..	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, casks, drums	11, 23, 29, 52, 55, 76	..	Dry; away from Combustibles, Acids	"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*Continued.*

Article.	Sectional Classification (see Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Ammonium Picrate—Wet ..	INFLAMMABLE SOLID (Section 2)	Bottle in a case	102	..	Cool; away from Mechanical Damage	"2"
Amyl Acetate	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool, away from Acids, Batteries	"2"
Amyl Alcohol	..	See "Alcohol, Amyl"				
Amyl Chloride	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries	"2"
Amyl Nitrite	INFLAMMABLE LIQUID Group "A" (Section 2)	Capsules in cases	10	..	Well ventilated, cool; away from Acids, Batteries	"2"
Anhydrous Ammonia	..	See "Ammonia, Liquefied, Anhydrous"				
Anhydrous Hydrofluoric Acid		See "Hydrofluoric Acid, Anhydrous"				
Aniline Oil (Aniline)	POISONOUS LIQUID (Section 5) Gives off Poisonous Vapours	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Foodstuffs	"2"
Aniline Salt	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Away from Foodstuffs, Alkalies	"2"
Anti-Freeze Compounds, Liquid	INFLAMMABLE LIQUID (Section 2)	Bottles in cases, drums, carboys	3, 4, 41, 71	10%	Well ventilated, cool; away from Acids, Batteries	"2"
Antimony Chloride	CORROSIVE LIQUID (Section 4) Poisonous	Bottles in cases, jars in crates, carboys	13, 14, 42, 45	5%	Well ventilated, cool; away from Foodstuffs, Water	"2"
Antimony Compounds and Preparations	POISONOUS SOLID OR LIQUID (Section 5)	Bottles or tins in cases, casks, drums, carboys	Liquid: 3, 4, 21, 41, 51, 71 Solid: 5, 22, 52, 55, 76	5% if Liquid	Well ventilated, cool; away from Foodstuffs	"2"
Antimony Pentachloride	CORROSIVE LIQUID (Section 4) Poisonous	Bottles in cases, carboys, drums	13, 14, 42, 77	5%	Well ventilated, cool; away from Foodstuffs	"2"
Aqua Fortis	..	See "Nitric Acid"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Argon	COMPRESSED GAS (Section 1) Permanent	Steel cylinders, high pressure 1,800 lb. per sq. in.	132, 133, 134	..	Cool, well ventilated; away from Acids	"2"
Argon + 20% Nitrogen	COMPRESSED GAS (Section 1) Permanent Mixed	Steel cylinders, high pressure 1,800 lb. per sq. in.	132, 133, 134	..	Cool, well ventilated; away from Acids	"2"
Arsenate of Lead (Paste or Solid)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, barrels, fibre-board containers Bags in cases	5, 22, 51, 76, and if dry: 26, 28, 52, 55, 64 (in tins only)	..	Dry; away from Foodstuffs	"A"
Arsenic Acid with under .05% Nitric Acid	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys, jars	3, 4, 21, 41, 45, 71	5%	Dry; away from Foodstuffs	"2"
Arsenic Acid	POISONOUS LIQUID (Section 5)	Bottles in cases, carboys, drums, jars	3, 4, 41, 45, 77	5%	Dry; away from Foodstuffs	"2"
Arsenic Acid	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs	"2"
Arsenic, Compounds of	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys, jars	3, 4, 21, 41, 45, 71	5%	Away from Foodstuffs	"2" See page 137, Volume I
Arsenic, Compounds of	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs	"2" See page 137, Volume I
Arsenic, Refined	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs	*"2" See pages 136 and 137, Volume I
Arsenic, Crude	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs	"A" Minimum 10 tons 9 cwt. per 11 ton truck and 15 tons 4 cwt. per 16, 22 or 27 ton truck, "A" 25%
Arsenic, Pentoxide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs	N.O.S. "2" When declared for the destruction of noxious weeds—"A"
Arsenical Compounds	..	See "Arsenic, Compounds of"		..	..	
Arsenious Oxide	..	See "Arsenic, Compounds of"		..	..	

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Bags, Nitrate of Soda	..	Must be thoroughly washed and cleansed before being offered for transport				"A," minimum 2/-
Bags, Waste, Oily	..	See "Cotton Waste, Cloth, Oily"		..		
Barium Carbonate	..	See "Barium, Compounds of"		..	..	"C"
Barium Chlorate	OXIDISING MATERIAL (Section 6)	*Bottles or tins in cases,* drums	11, 23, 76	..	Cool, dry; away from Ammonium Compounds, Sulphur, Sugar, Acids, Combustibles, Foodstuffs	"2"
Barium Chloride	..	See "Barium, Compounds of"			..	"C"
Barium, Compounds of	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, cases, fibreboard containers	5, 22, 26, 28, 52, 55, 64, 76	..	Dry; away from Foodstuffs, Acids, Batteries	"2"
Barium Cyanide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 29, 54, 76	..	Dry; away from Foodstuffs, Acids, Batteries	"2"
Barium Metallic	REACTS WITH AIR, WATER OR ACIDS (Section 3) Poisonous	Bottles or tins in cases, airtight drums	5, 22, 76	..	*Dry; away from Foodstuffs,* Acids, Inflammables, Batteries	"2"
Barium Nitrate	OXIDISING MATERIAL (Section 6) Poisonous	*Bottles or tins in cases,* drums	11, 23, 76	..	Dry; away from Foodstuffs, Acids, Combustibles, Batteries	"2"
Barium Oxide	..	See "Barium, Compounds of"		..	..	
Barium Perchlorate	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Foodstuffs, Acids, Combustibles, Batteries	"2"
Barium Permanganate	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, casks, drums, cases	11, 23, 28, 52, 55, 76	..	Dry; away from Foodstuffs, Acids, Batteries, Combustibles	"2"
Barium Peroxide (Binoxide, Dioxide)	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Foodstuffs, Acids, Combustibles, Batteries, Cylinders of Chlorine and Sulphur Dioxide	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Batteries, Wet (charged) ..	..	See "Accumulators"				
Bengal Matches	INFLAMMABLE SOLID	Tin boxes in wooden cases	1, 22	..	Dry; away from Inflammable Liquids	"2"
Benzaldehyde	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Benzene	..	See "Benzol"				
Benzine	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	Same as Motor Spirit
Benzol	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	Same as Motor Spirit See page 137, Volume I
Benzoline	.	See "Benzine"				
Benzoyl Chloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 41, 72, 77	5%	Well ventilated, cool; away from Foodstuffs	"2"
Benzoyl Peroxide	INFLAMMABLE SOLID (Section 2)	Tins in cases, drums	103	..	Cool; away from Inflammables	"2"
Benzyl Chloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums, jars in crates	13, 14, 41, 77	5%	Well ventilated, cool; away from Foodstuffs, Inflammables	"2"
Bisulphide of Carbon	..	See "Carbon Bisulphide"				
Bituminous Paints	..	See "Paints, containing Inflammable Liquid"				
Black Carbon, Lamp or Vegetable	INFLAMMABLE SOLID (Section 2)	Drums, casks, cases	28, 52, 55, 75	..	Dry; away from Acids, Batteries	"2"
Bleaching Liquid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys	3, 4, 41	5%	Cool; away from Foodstuffs	"C"
Bleaching Powder	CORROSIVE SOLID (Section 4)	Bottles or tins in cases, drums	5, 22, 75	..	Dry; away from Acids, Foodstuffs	"C"
Bleaching Solution (Sodium Hypochlorite)	CORROSIVE LIQUID (Section 4)	Bottles in cases, jars in crates, carboys with vented closures	3, 4, 41, 45	5%	Well ventilated, cool; away from Foodstuffs	*"C"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Bordeaux Arsenite (Liquid) ..	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums	3, 4, 21, 71	5%	Well ventilated, cool; away from Foodstuffs	"2"
Bordeaux Arsenite (Solid) ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64 (in tins only), 76	..	Dry; away from Foodstuffs	"2"
Boron Trichloride	CORROSIVE LIQUID (Section 4)	Steel cylinders	92	..	Cool; away from Foodstuffs	"2"
Bromacetone Liquid	POISONOUS LIQUID (Section 5)	Steel cylinders, bottles in cases	18, 92	5%	Cool; away from Acids, Foodstuffs	"2"
Brombenzyl Cyanide, Liquid ..	POISONOUS LIQUID (Section 5)	Steel cylinders, drums, tins in cases	21, 74, 92	5%	Cool; away from Acids, Foodstuffs	"2"
Bromine	CORROSIVE LIQUID (Section 4) Poisonous	Bottles in cases	16	15%	Well ventilated, cool; away from Foodstuffs, Organic Material, Ammonia	"2"
Bronzing Liquid (Inflammable Type)	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Tins in cases, drums	21, 22, 71	5%	Well ventilated, cool; away from Acids, Batteries	"2"
Brucine, Solid (Dimethoxy Strychnine)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks	5, 17, 22, 26, 52, 53, 55, 76	..	Dry; away from Foodstuffs	"2"
Burnt Cotton	INFLAMMABLE SOLID (Section 2)	Bales	104	..	Dry; away from Inflammables ..	"2"
Burnt Fibre	INFLAMMABLE SOLID (Section 2)	Cases, drums	29, 76	..	Dry; away from Inflammables ..	"2"
Butane	..	See "Hydrocarbon Gas, Liquefied"				
Butanol	..	See "Alcohol, Butyl"				
Butyl Acetate	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Butyl Alcohol	..	See "Alcohol, Butyl"				
Butyl Cellosolve	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Butyl Ether	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Butyl Formate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Butyl Propionate	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Butyraldehyde	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Cacodylic Acid (Dimethylarsenic)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks, fibreboard containers	5, 17, 22, 26, 28, 52, 53, 55, 64 (in tins only), 73, 76	..	Dry; away from Foodstuffs, Acids	"2"
Calcium Arsenate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks, fibreboard containers	5, 17, 22, 26, 28, 52, 53, 55, 64 (in tins only), 73, 76	..	Dry; away from Foodstuffs, Acids	"2"
Calcium Arsenite	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks, fibreboard containers	5, 17, 22, 26, 28, 52, 53, 55, 64 (in tins only), 73, 76	..	Dry; away from Foodstuffs, Acids	"2"
Calcium Carbide	REACTS WITH AIR AND WATER (Section 3)	Tins in cases, drums	22, 75, 105	..	Well ventilated, dry; away from Acids, Batteries, Inflammables	N.O.S. "1" Minimum 8 tons per 11 ton truck and 12 tons per 16, 22 or 27 ton truck—"B"
Calcium Chlorate, in Solution	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 73, 76	..	Cool; away from Sulphur, Acids, Combustible Materials	"2"
Calcium Chlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 73, 76	..	Dry; away from Acids, Combustible Materials	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Calcium Cyanamide (Nitrolim) containing Calcium Carbide)	REACTS WITH WATER (Section 3)	Tins in cases, drums, sacks	22, 62, 75	..	Well ventilated, dry; away from Acids, Oxidising Materials, Batteries	"2"
Calcium Cyanide	POISONOUS SOLID (Section 5)	Tins in cases, casks, drums, fibreboard containers	5, 22, 54, 65, 76	..	Well ventilated, dry; away from Foodstuffs, Acids	N.O.S. "2" For rabbit destruction or fumigation of fruit trees or for combating weevils in wheat silos or storage bins on declaration —"1"
Calcium, Metallic	REACTS WITH AIR AND WATER (Section 3)	Bottles or tins in cases, drums	5, 22, 76	..	Well ventilated, dry; away from Combustible Material, Acids	"2"
Calcium Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Foodstuffs, Combustibles	"2"
Calcium Peroxide	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Combustibles	"2"
Calcium Permanganate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums, casks	11, 23, 52, 55, 76	..	Dry; away from Acids, Combustibles	"2"
Calcium Phosphide	REACTS WITH WATER (Section 3)	Tins or drums in cases	22, 80	..	Well ventilated, dry; away from Acids, Inflammables, Foodstuffs	"2"
Calcium Silicide	REACTS WITH AIR OR WATER (Section 3)	Tins in cases, drums	22, 76	..	Well ventilated, dry; away from Acids, Inflammables, Foodstuffs	Minimum 10 tons 9 cwt. per 11 ton truck or 15 tons 4 cwt. per 16, 22 or 27 ton truck—"A"
Canned Heat (Solidified Alcohol)	INFLAMMABLE SOLID (Section 2)	Tins in cases	22	..	Cool; away from Acids, Batteries	"2"
Carbethox (Carbon Dioxide and Ethylene Oxide), approx. 10%	COMPRESSED GAS (Section 1) High Pressure Liquefiable, Mixed	Steel cylinders, high pressure 1,800 lb. per sq. in.	136.	..	Well ventilated, cool; away from Batteries, Acids	"2"
Carbide	..	See "Calcium Carbide"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Carbogen (Carbon Dioxide, 5% in Oxygen)	COMPRESSED GAS (Section 1) Permanent Mixed	Steel cylinders, high pressure, 1,800 lb. per sq. in.	132, 133, 134	..	Well ventilated, cool; away from Batteries, Acids	"2"
Carbolic Acid (Phenol)	POISONOUS LIQUID OR SOLID (Section 5) Corrosive	Bottles or tins in cases, drums, carboys	3, 4, 5, 21, 22, 41, 71	5% if Liquid	Well ventilated, cool; away from Foodstuffs	"2"
Carbon Bisulphide (Carbon Disulphide)	INFLAMMABLE LIQUID Group "A" (Section 2) Poisonous	Bottles or tins in cases, drums	9, 74, 81, 85	10%	Well ventilated, cool; away from Foodstuffs, Acids, Batteries, Combustibles	N.O.S. "2" For rabbit killing or for use in combating weevils in wheat silos and storage bins on declaration—"1"
Carbon Dioxide	COMPRESSED GAS (Section 1) High Pressure Liquefiable	Steel cylinders, high pressure	134, 136	Filling Ratio 0.667	Well ventilated, cool; away from Acids	*"2"
Carbon Monoxide	COMPRESSED GAS (Section 1) Permanent Poisonous Inflammable	Steel cylinders, high pressure, 1,800 lb. per sq. in. Special authority required	139	..	Well ventilated, cool; away from Acids, Batteries, Foodstuffs	"2"
Carbon Remover Liquid	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71.	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Carbonyl Chloride	.. .	See "Phosgene"				
Cartridges—Small Arm and Safety	..	See List of Authorised Explosives (Sporting Ammunition), Part II, Section I				"2"
Caustic Potash	CORROSIVE SOLID (Section 4)	Tins in cases, drums, bottles in cases	5, 22, 75	..	Dry; away from Acids, Foodstuffs	"2"
Caustic Soda	CORROSIVE SOLID (Section 4)	Bottles or tins in cases, fibreboard containers, drums	5, 22, 64, 75	..	Dry; away from Acids, Foodstuffs	*"A"
Cellosolve	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Celluloid	INFLAMMABLE SOLID (Section 2)	Cases	29	..	Away from Acids, Batteries	"2"
Celluloid Films	..	See "Films, Nitrocellulose base"				
Celluloid Solution and Varnish	..	See "Paints, containing Inflammable Liquids"				"2"
Cellulose Enamels	..	See "Paints, containing Inflammable Liquids"				"2"
Cement, Leather	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles, tins or tubes in cases, drums	4, 19, 21, 22, 71, 73	..	Cool; away from Batteries, Acids	"2"
Cement, Liquid, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Cement, Roofing, Liquid	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles, tins or tubes in cases, drums	4, 19, 21, 22, 71, 73	..	Cool; away from Batteries, Acids	"2"
Cement, Rubber	..	See "Rubber Cement and Solution"				
Charcoal .. .. } Char Dust .. .. }	INFLAMMABLE SOLID (Section 2)	Paper bags, sacks, drums, barrels, cases, bulk	106	..	Dry	Charcoal—Minimum, 7 tons per 11 ton truck, 8½ tons per high sided truck, 10 tons per swing door, 10½ tons per drop door 22 or 27 ton truck—"M" Char Dust—Minimum, 10 tons 9 cwt. per 11 ton truck, 15 tons 4 cwt. per 16, 22 or 27 ton truck—"M"
Chemicals, Drugs and Medicinal Preparations, N.O.S., in Small Quantities	POISON (Section 5)	Containers in cases	107	..	Dry; cool	"2"
Chile Saltpetre (Nitre)	..	See "Sodium Nitrate"				
Chloracetyl Chloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, drums, carboys	14, 44, 77	5%	Well ventilated, cool; away from Foodstuffs, Combustibles	"2"
Chlorates, N.O.S.	..	Packaging to the approval of the Railways Commissioners				N.O.S. "2" When declared to be used for destroying noxious weeds — "A"
Chlorate of Potash	..	See "Potassium Chlorate"				
Chlorate of Soda	..	See "Sodium Chlorate"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Chlorbenzene (Chlorbenzol)	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Chloride of Sulphur	..	See "Sulphur Monochloride"				
Chlorine	COMPRESSED GAS (Section 1) Medium Pressure Liquefiable, Poisonous	Steel cylinders, medium pressure, large welded steel removable drums —mounted on rail wagons	134, 137, 138	Filling Ratio 1.19 (Cylinders)	Well ventilated, cool; Isolated Stowage	N.O.S. "2" For rabbit killing on declaration —"1"
Chloropicrin	POISONOUS GAS OR LIQUID (Section 5)	Bottles or tins in cases, steel cylinders	21, 32, 33, 92	5%	Open air stowage; away from Foodstuffs, Acids	"2"
Chlorosene	REACTS WITH WATER (Section 3) Poisonous	Bottles or tins in cases, drums	5, 22, 76	..	Well ventilated, dry	"2"
Chlorosulphonic Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, jars in crates, carboys, drums	13, 14, 42, 74	5%	Well ventilated, cool; away from Nitric Acid, Foodstuffs	"2"
Chromic Acid, Liquid	CORROSIVE LIQUID (Section 4)	Bottles in cases, jars in crates, carboys, drums, plastic containers	13, 14, 42, 45, 74 Up to 50% 37 and 38	10%	Well ventilated, cool; away from Foodstuffs, Combustibles	"2"
Chromic Acid	CORROSIVE SOLID (Section 4) Poisonous	Bottles in cases, drums	11, 73, 76	..	Well ventilated, dry; away from Foodstuffs, Combustibles, Acetic Acid, Alcohol	"2"
Chromium Anhydride	..	See "Chromic Acid, Solid"				
Cinematograph Films	..	See "Films, Nitrocellulose base"				
Cleaning Fluid	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys, plastic capsules Tank trucks	1, 3, 4, 21, 22, 36, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Batteries, Acids	"2"
Coal Gas, Compressed	COMPRESSED GAS (Section 1) Permanent, Poisonous Inflammable	Steel cylinders, high pressure 1,800 lb. per sq. in.	132, 134	..	Well ventilated, cool; away from Foodstuffs, Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Coal (Ground)	INFLAMMABLE SOLID (Section 2)	Cases, drums	108	..	Dry; away from Inflammables, Acids, Batteries	Minimum, 7½ tons per 11 ton truck and 12 tons 15 cwt. per 16, 22 or 27 ton truck—Firewood plus 20%
Coal Tar Oils	..	See "Tar Oil Compounds"				N.O.S. *"C" Used directly for fuel in open furnaces on declaration. Minimum, 8 tons per 11 ton truck or 12 tons per 16, 22 or 27 ton truck—"M"
Cobalt Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids	"2"
Cobalt Resinate (Precipitated)	INFLAMMABLE SOLID (Section 2)	Tins in cases, drums	22, 75	..	Dry; away from Acids, Batteries	"2"
Cocculus Solid (Fish Berry) ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums	5, 22, 52, 55, 76	..	Dry; away from Foodstuffs	"2"
Cold Starters	INFLAMMABLE SOLID (Section 2)	Tins in cases	24	..	Well ventilated, cool; dry	"2"
Collodion Cotton—(1) Saturated with Methylated Spirits or other Inflammable Liquid .. .. (2) Wet with Water	INFLAMMABLE LIQUID (Section 2) (1) Explosive if dry (2) Non-dangerous	Bottles or tins in cases, drums	3, 4, 21, 71, 84	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Cologne Spirits (Alcohol) ..	..	See "Alcohol, Industrial"				
Columbian Spirits	..	See "Alcohol, Wood"				
Compounds, Cleaning Liquid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 44, 77	5%	Well ventilated, cool; away from Foodstuffs, Combustibles	"2"
Compounds, Cleaning Liquid	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Compounds, Enamel	..	See "Paints, containing Inflammable Liquids"				"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Compounds, Lacquer or Varnish, Removing, Reducing or Thinning	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	10%	Well ventilated, cool; away from Batteries, Acids	"2"
Compounds, Paint or Varnish, Removing (Acid type) ..	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys	13, 14, 42	5%	Well ventilated, cool; away from Foodstuffs	"2"
Compounds, Paint or Varnish, Removing (Caustic type) ..	CORROSIVE LIQUID OR SOLID (Section 4)	Bottles or tins in cases, carboys, drums	Liquid: 1, 3, 4, 21, 41, 71; Solid: 5, 22, 51, 76	5% if Liquid	Dry; away from Foodstuffs, Acids	"2"
Compounds, Polishing, Liquid	..	See "Polishes, Metal, Stove, Furniture and Wood—Solvent Base"				
Compounds, Type Cleaning .	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums	3, 4, 21, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Compounds, Vulcanising, Liquid, Alkaline	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, casks, drums	13, 14, 42, 44, 51, 74	5%	Well ventilated, cool; away from Foodstuffs, Combustibles	"2"
Compounds, Vulcanising Liquid (Solvent)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Compressed Gases	..	See under name of particular Gas				
Copper Aceto Arsenite (Paris Green, Emerald Green, Imperial Green, Kings Green, Moss Green, Meadow Green, Nitis Green, Parrot Green, Vienna Green)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 17, 22, 53, 76	..	Dry; away from Foodstuffs	"2"
Copper Arsenite (Scheeles Green, Cupric Green, Mineral Green, Orthoarsenite Swedish Green)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	15, 17, 22, 53, 76	..	Dry; away from Foodstuffs	"2"
Copper Cyanide	..	See "Cyanide of Copper, Zinc, Lead and Silver"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Copper Sulphate	POISONOUS SOLID (Section 5)	Bottles in cases, casks Jute sacks in truck-loads only	5, 26, 28, 52, 55, 63	..	Dry; away from Food-stuffs	N.O.S. "1" For spraying purposes and pick-ling wheat on declaration—"A" For Fluke prevention or treat-ment of foot rot in sheep on declaration—"B"
Copper Nitrate	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Combustibles, Food-Stuffs	"2"
Corrosive Liquids, N.O.S. ..	..	Packing to the approval of the Railways Commissioners				"2"
Corrosive Sublimate	..	See "Mercuric Chloride"				
Cotton, Burnt	..	See "Burnt Cotton"				
Cotton Waste, Cloth, Oily or Wet	INFLAMMABLE SOLID (Section 2)	Cases, drums	29, 75	..	Away from Batteries, Acids, inflammables	"A"
Cresol, Liquid	..	See "Carbolic Acid"				
Cresylic Acid	..	See "Carbolic Acid"				
Croton Aldehyde	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Crude Oil	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	Same as Fuel Oils. See page 44
Cyanamide	..	See "Calcium Cyanamide"				
Cyanide of Calcium	..	See "Calcium Cyanide"				
Cyanides of Copper, Zinc, Lead and Silver	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 29, 54, 76	..	Well ventilated, dry; away from Foodstuffs, Acids	"2"
Cyanide of Potassium, Liquid	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums Plastic containers	3, 4, 21, 71 Up to 40%: 37, 38	5%	Well ventilated, cool; away from Foodstuffs, Acids	"1"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Cyanide of Potassium, Solid ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 29, 54, 76	..	Well ventilated, cool; away from Foodstuffs, Acids	"1"
Cyanides, N.O.S.	..	Packing to the approval of the Railways Commissioners			..	"2"
Cyanide of Sodium, Liquid ..	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums Plastic containers	3, 4, 21, 71 Up to 40%: 37 and 38	5%	Well ventilated, cool; away from Foodstuffs, Acids	"1"
Cyanide of Sodium, Solid ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 29, 54, 76	..	Well ventilated, cool; away from Foodstuffs, Acids	"1"
Cyanogas	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks, tins in fibreboard containers	5, 22, 29, 54, 65, 76	..	Well ventilated, dry; away from Foodstuffs, Acids	See Calcium Cyanide
Cyclopentane	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Cyclopentane, Methyl	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Cyclopropane	COMPRESSED GAS (Section 1) Medium Pressure Liquefiable, Inflammable	Steel cylinders, medium pressure	134, 137	..	Well ventilated, cool; away from Batteries, Acids	"2"
Decahydronapthalene	..	See "Decalin"				
D.D.T.	POISONOUS SOLID OR LIQUID (Section 5)	Bottles or tins in cases, drums	Solid: 5, 17, 22, 53, 76; Liquid: 71	..	Dry; away from Foodstuffs, Acids	"2"
Decalin	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Detonators	..	See Authorised List. Part II, Section I		..	..	See Explosives
Diacetone Alcohol	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Dichlorodiphenyl-trichlorethane	..	See "D.D.T."				
Dichlorethylene	POISONOUS LIQUID AND VAPOUR (Section 5)	Bottles or tins in cases, drums, carboys	3, 21, 41, 71	10%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Dichlorodifluoromethane (F. 12), Freon, Kinetic No. 12	COMPRESSED GAS (Section 1) Liquefiable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 1.05	Well ventilated, cool; away from Acids	"2"
Dichloromonofluoromethane (F. 21), Thermon	COMPRESSED GAS (Section 1) Liquefiable	Steel cylinders, low pressure	134, 135, 137	..	Well ventilated, cool; away from Acids	"2"
Dichlorotetrafluoromethane (F. 114)	COMPRESSED GAS (Section 1) Liquefiable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 1.30	Well ventilated, cool; away from Acids	"2"
Dichromate of Potash	..	See "Potassium Bichromate"				
Diesel Oil	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	Same as Fuel Oil, excepting Clauses 3 and 4. See page 44
Diethylamine	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Difluoroethane	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable, Inflammable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 0.68	Well ventilated, cool; away from Batteries, Acids	"2"
Dimethylamine	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable, Inflammable	Steel cylinders, low pressure	134, 135, 137	..	Well ventilated, cool; away from Batteries, Acids	"2"
Dimethyl Aniline	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Acids, Foodstuffs	"2"
Dimethylarsenic Acid		See "Cacodylic Acid"				
Dimethyl Ether	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable, Inflammable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 0.55	Well ventilated, cool; away from Acids, Batteries	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Dimethyl Sulphate	CORROSIVE LIQUID (Section 4) Poisonous Vapours	Bottles in cases, drums	12, 81	10%	Well ventilated, cool; away from Acids, Foodstuffs and liability to mechanical damage	"2"
Dimethyl Sulphide	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries	"2"
Dinitrobenzene (Dinitrobenzol)	OXIDISING SOLID (Section 6) Poisonous	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Material, Foodstuffs	"2"
Dinitro-orthocresol (Dinoc) ..	POISONOUS SOLID (Section 5)	Tins in cases	22	..	Dry; away from Foodstuffs	"2"
Dinitrotoluene	..	See "Dinitrotoluol"				
Dinitrotoluol, Liquid (Dinitrotoluene)	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Acids, Foodstuffs, Combustibles	"2"
Dinitrotoluol, Solid (Dinitrotoluene)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks	5, 21, 22, 52, 55, 76	..	Dry; away from Acids, Foodstuffs, Combustibles	"2"
Diphenylamine	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Acids, Batteries	"2"
Distillate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	Same as Fuel Oil, excepting Clauses 3 and 4. See page 44
Disinfectant Fluids	POISONOUS LIQUIDS (Section 5)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids	N.O.S. "1" Produce of Commonwealth on "Up" journey "Smalls Minimum"—*"C"
Dressing, Leather	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Driers, Paint or Varnish	..	See "Paints, containing Inflammable Liquids"				"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Drugs, Chemicals and Medicinal Preparations	..	See "Chemicals, Drugs and Medicinal Preparations"				
Dye Intermediates	POISONOUS LIQUIDS OR SOLIDS (Section 5) Inflammable	Bottles or tins in cases, drums, casks, carboys	Liquid: 3, 4, 21, 41, 71; Solid: 5, 21, 22, 52, 55, 76	5% if liquid	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Electric Storage Batteries ..	..	See "Accumulators"				
Enamels	..	See "Paints, containing Inflammable Liquids"				"2"
Eradicators, Paint or Grease	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Ethane	COMPRESSED GAS (Section 1) High Pressure Liquefiable Inflammable	Steel cylinders, high pressure 1,800 lb. per sq. in.	134, 136	Filling Ratio 0.33	Well ventilated, cool; away from Batteries, Acids	"2"
Ethane Tetrachloride	POISONOUS LIQUID AND VAPOUR (Section 5)	Bottles or tins in cases, drums	3, 4, 21, 71	10%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Ether Ethyl, Sulphuric	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, bottles in fibreboard containers	6, 25, 35, 74	7½%	Well ventilated, cool; away from Batteries, Acids, Sunlight	"2"
Ethyl Acetate	..	See "Acetic Ether"				
Ethyl Alcohol	..	See "Alcohol, Ethyl"				
Ethyl Aldehyde	..	See "Acetaldehyde"				
Ethyl Aniline	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Ethyl Butyrate	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Ethyl Chloride	COMPRESSED GAS (Section 1) Liquefiable Inflammable	Steel cylinders, low pressure Bottles in fibreboard containers, each bottle not to exceed 4 fluid oz.	64, 134, 135, 137	Filling Ratio 0.79	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Ethylene	COMPRESSED GAS (Section 1) High Pressure Liquefiable Inflammable	Steel cylinders, high pressure	134, 136	Filling Ratio 0.27	Well ventilated, cool; away from Batteries, Acids	"2"
Ethylene Dichloride	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Ethylene Oxide	COMPRESSED GAS (Section 1) Liquefiable Inflammable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 0.75	Well ventilated, cool; away from Batteries, Acids	"2"
Ethyl Fluid	...	See "Lead Tetra Ethyl"				
Ethyl Formate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries	"2"
Ethyl Lactate	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries	"2"
Ethyl Methyl Ether	COMPRESSED GAS (Section 1) Low Pressure Inflammable	Steel cylinders, low pressure	134, 135, 137	..	Well ventilated, cool; away from Acids, Batteries	"2"
Ethyl Methyl Ketone	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Ethyl Nitrate (Nitric Ether)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Ethyl Nitrite (Nitrous Ether)	COMPRESSED GAS (Section 1) Liquefiable Inflammable	Steel cylinders, low pressure, capsules	10, 134, 135, 137	..	Well ventilated, cool; away from Batteries, Acids	"2"
Ethyl Silicate	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Eucalyptus Oil	INFLAMMABLE LIQUID *Group "B" (Section 2)*	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	N.O.S.—"1" Minimum, 7 tons per 11 ton *truck and 10½ tons per 16,* 22 or 27 ton truck—"C" Produce of Commonwealth, on "Up" journey—*"C" Consigned direct to the seaboard for immediate export beyond the State— Minimum, 7 tons per 11 ton truck and 10½ tons per 16, 22 or 27 ton truck—"A" Smaller quantities— "Down" journey—"1" "Up" journey—*"C" (See page 148, Volume I)
Explosives	..	See "List of Authorised Explosives, Part II, Section I" ..				(a) **In Explosives Vans—** 1. Explosives forwarded from one station to one destination station in an explosives van by an ordinary goods train shall be charged Class "2" mileage rate, minimum 2 tons per van, provided that in no case shall the charge be less than 28s. 10d. for each consignment. 2. Explosives forwarded from one station to two or more destination stations in the same explosives van by an ordinary goods train shall be charged Class "2" mileage rate, subject to a minimum of 2 tons per van for any portion of the journey, provided that in no case shall the charge be less than 28s. 10d. for each consignment. Where the weight in an explosives van for any portion of the journey is less than the minimum of 2 tons, the weight short of 2 tons is to be charged for at the Class "2" mileage rate for the portion of the journey concerned. (See Addenda, page 21, for example.)

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Explosives—continued						(b) **In Portable Magazines or Cylinders—** Explosives forwarded from Ardeer, Deer Park or Ravenhall in portable magazines or cylinders shall be charged Class 2 mileage rates for a minimum weight of 1 cwt. per container, and a minimum charge of 9s. 0d. for each portable magazine or cylinder. Magazines or cylinders shall be returned free. **District Rates.** — Safety Cartridges and Sporting Gunpowder shall obtain the benefit of District Rates where such are in operation, but in no other case shall any explosive consignments be given the benefit of any Tariff rates lower than the mileage rates. **Consignments from One or More Consignors to One or More Consignees.** — Consignments of Explosives forwarded from one consignor to different consignees shall be charged for separately. Consignments from different consignors to one consignee may however be treated and charged for as one consignment.
Extracts, Liquid Flavoring ..	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys	9, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Fabric or Fibre, Oiled	INFLAMMABLE SOLID (Section 2) Reacts with Air	Metal lined cases, drums	31, 75	..	Dry; away from Acids, Inflammables	"2"
Felt Waste, Wet	..	See "Waste Wool, Wet"				
Ferric Arsenate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, barrels	5, 17, 22, 26, 52, 53, 55, 76	..	Dry; away from Foodstuffs, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Ferric Arsenite	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, barrels	5, 17, 22, 26, 52, 53, 55, 76	..	Dry; away from Foodstuffs, Acids	"2"
Ferro Silicon (30%-75% Silicon)	REACTS WITH AIR AND WATER (Section 3) Poisonous Inflammable	Barrels, drums Bulk, truckloads only, unsheeted	52, 55, 75	..	Well ventilated, dry; away from Foodstuffs, Batteries, Acids	Full truck loads—"C" Smaller quantities—"1"
Ferrous Arsenate (Iron Arsenate)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, barrels	5, 17, 22, 26, 52, 53, 55, 76	..	Dry; away from Foodstuffs, Acids	Same as "Arsenate of Lead." See page 24
Fertiliser and Artificial Manures, containing Oxidising Material	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, sacks, drums	11, 23, 66, 76	..	Dry; away from Inflammables, Acids, in steel trucks only	Same as "Manures." See page 56, Volume 1
Fibre, Burnt	..	See "Burnt Fibre"				
Fibre or Fabric, Oiled	..	See "Fabric or Fibre, Oiled"				
Films (Nitrocellulose base) ..	INFLAMMABLE SOLID (Section 2)	Metal boxes	109	..	Cool, dry; away from Acids, Batteries and Electrical Equipment	"2"
Film Scrap (Nitrocellulose base)	..	See "Celluloid"				
Fireworks	..	See "List of Authorised Explosives, Part II, Section I"				Small—"2" mileage rates Large—Same as "Explosives"
Fish Meal or Fish Scrap, containing less than 6% or more than 13% moisture ..	INFLAMMABLE SOLID (Section 2)	Drums	76	..	Cool; away from Inflammables	Same as "Manure"
Fluorides, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Fly Sprays (flash point below 150° F.).	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys, fibre-board containers	3, 4, 21, 22, 41, 64, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	N.O.S., "2" Minimum 12 tons per 4 wheeled truck—"C"
Formic Acid	CORROSIVE LIQUID (Section 4) Inflammable	Bottles in cases, jars, carboys, drums, barrels, plastic containers	3, 4, 41, 45, 51, 74 37, 38	5%	Well ventilated, cool; away from Foodstuffs, Batteries	*"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume 1.

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Freon	..	See "Dichlorodifluoromethane"				
‡Fuel and Residual Oil	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	1. Used as fuel for motive power in internal combustion engines—"A" 2. In bulk in owner's tank trucks subject to a minimum of full carrying capacity of truck—"A" (Empty tank trucks returned free) 3. Used for fuel in open furnaces or for roadmaking —§"A" 4. For combating frosts—‡"A"
Fungicides, N.O.S.	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 17, 22, 52, 55, 64, 76	..	Dry; away from Foodstuffs	N.O.S. "2" If declared as orchard pests specifics—"A"
	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, carboys, drums	3, 4, 21, 41, 64 (Arsenic and Nicotine compounds in tins only), 71	5%	Cool; away from Foodstuffs	
	INFLAMMABLE LIQUID Group "A" or "B" (Section 5)	Bottles or tins in cases, carboys, fibreboard containers, drums	3, 4, 21, 22, 41, 64, 71	5%	Well ventilated, cool; away from Batteries and Acids	
Furniture Polishes	..	See "Polishes, Metal, Stove, Furniture and Wood, Liquid"				
Furniture or Wood Stains, Liquid	..	See "Paints, containing Inflammable Liquids"				"2"
Fusees, Safety	INFLAMMABLE SOLID (Section 2)	Tin boxes in wooden cases	122	..	Dry; away from Inflammable Liquid	"2"
Fuse Lighters (Fireworks)	..	See "List of Authorised Explosives, Part II, Section I"				"2"
Fuses (for Explosives)	..	See "List of Authorised Explosives, Part II, Section I"				"2"
Fusel Oil	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

§ Oil, crude or residual, used directly for fuel in open furnaces in place of wood or coal, also for roadmaking, shall be charged the ordinary rate in the first instance, viz., Class "A." On production of a Statutory Declaration to the Chief Commercial Manager within six months from the date the consignments are forwarded that the oil has been used for either of the purposes mentioned, a rebate shall be granted to Class "M" rate, minimum 8 tons per 11 ton truck, 12 tons per 16, 22 or 27 ton truck.

‡ Oil residual consigned to the country for combating frosts in vineyards and orchards shall be charged Class "A" rate and conditions in the first instance. On application to the Chief Commercial Manager within six months from the date the consignments are forwarded and production of a declaration that the oil has been used for the purpose indicated a rebate may be allowed to Class "AP" rate, minimum 8 tons per 11 ton truck, 12 tons per 16, 22 or 27 ton truck.

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Garbage, Tankage containing less than 8% moisture ..	INFLAMMABLE SOLID (Section 2)	Metal lined cases, drums	31, 75	..	Away from Acids, Inflammables	Same as "Manure"
Gas, Compressed or Liquefied, Not otherwise specified ..	..	Packaging to the approval of the Railways Commissioners				"2"
Gas Drips, Hydrocarbon	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Gas Identification Sets	POISONOUS LIQUID OR GAS (Section 5)	Cylinders, cases	110	..	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Gasoline	..	See "Motor Spirit"	..	..	..	Same as Motor Spirit. See page 56
Gold Paint	..	See "Paints, containing Inflammable Liquid"			..	"2"
Guanidine Nitrate	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Acids	"2"
Gunpowder	..	See "List of Authorised Explosives, Part II, Section I"				Same as Explosives
Gutta Percha in Solution ..	..	See "Rubber Cement and Solution"				
Hair Waving Materials, Self Heating	..	See "Self-heating Hair-waving Pads"				
Hair (Wet)	INFLAMMABLE SOLID (Section 2)	Cases, drums	31, 75	..	Away from Acids, Inflammables	"2"
Helium	COMPRESSED GAS (Section 1) Permanent	Steel cylinders, high pressure, 1,800 lb./sq. in.	132, 133, 134	..	Well ventilated, cool; away from Acids	"2"
Heptane	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Hexane	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Hydriodic Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 44, 77	5%	Well ventilated; away from Foodstuffs	"2"
Hydrobromic Acid	CORROSIVE LIQUID (Section 4) Strong Fuming Acid	Bottles in cases, carboys	13, 14, 42	5%	Well ventilated; away from Foodstuffs, Sulphuric Acid	"2"
Hydrocarbon Gas, Non-liquefied	..	See "Methane"				
Hydrocarbon Gas, Liquefied ..	COMPRESSED GAS (Section 1) Liquefiable Inflammable	Steel cylinders, medium or low pressure	134, 135, 137 (low pressure)	Filling Ratio variable	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Hydrocarbon Oil, for gas manufacture	INFLAMMABLE LIQUID Group "A" (Section 2)	Steel drums Tank trucks	1, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Hydrochloric Acid	CORROSIVE LIQUID (Section 4) Strong Fuming Acid	Bottles in cases, carboys, plastic containers	13, 14, 42 37, 38	5%	Well ventilated; away from Foodstuffs	*"A"
Hydrochloric Acid Mixtures ..	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys	13, 14, 42	5%	Well ventilated; away from Foodstuffs	"2"
Hydrocyanic Acid, Anhydrous, stabilised	COMPRESSED GAS (Section 1) Liquefiable, Inflammable, Poisonous, Low Pressure, Special	Steel cylinders, medium pressure Special authority required	139	Filling Ratio 0.57	Open air stowage; away from Batteries, Acids, Foodstuffs	"2"
Hydrocyanic Acid Solution (Prussic Acid), 5% maximum	POISONOUS LIQUID AND VAPOUR (Section 5)	Bottles in tin-lined cases	20	10%	Open air stowage; away from Batteries, Acids, Foodstuffs	"2"
Hydrofluoric Acid, maximum 65%	CORROSIVE LIQUID (Section 4) Dangerous in contact with the skin	Containers in cases, drums, carboys	44, 77, 111	10%	Well ventilated, cool; away from Foodstuffs. Handle with care	"2"
Hydrofluosilicic Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, drums	112	10%	Well ventilated, cool; away from Foodstuffs. Handle with care	"2"
Hydrogen	COMPRESSED GAS (Section 1) Permanent, Inflammable, Suffocating	Steel cylinders, high pressure, 1,800 lb./sq. in.	132, 134	..	Well ventilated, cool; away from Batteries, Acids	N.O.S.—"2" Minimum 6 tons per 4 wheeled truck—"1"
Hydrogen Chloride	COMPRESSED GAS (Section 1) Liquefiable, Non-Inflammable	Steel cylinders, high pressure	134, 136	..	Well ventilated, cool; away from Acids	"2"
Hydrogen Peroxide, up to 20 volumes	Non-dangerous	Any suitable container	..	10%	Cool	*"2"
Hydrogen Peroxide, between 20 and 100 volumes	OXIDISING LIQUID (Section 6)	Bottles in cases, carboys, drums All containers must be vented	2, 3, 4, 41, 78	10%	Well ventilated, cool; away from Combustible Materials, Resins, Oils	
Over 40% Strength	**	Not acceptable for transport without approval of Railways Commissioners				

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume 1.

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Hydrogen Sulphide	COMPRESSED GAS (Section 1) Liquefiable, Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.667	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Igniters A.L.T.	INFLAMMABLE SOLID (Section 2)	Cases, fibreboard containers	114	..	Dry; away from Inflammables, Acids	"2"
Igniters (Fireworks)	..	See "List of Authorised Explosives, Part II, Section I"				"2"
Inflammable Liquids, N.O.S. .	..	Packaging to the approval of the Railways Commissioners				"2"
Inflammable Solids, N.O.S. ..	..	Packaging to the approval of the Railways Commissioners				"2"
Ink, Printers'	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Insecticide, Dry	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks	5, 22, 52, 55, 76	..	Dry; away from Foodstuffs	"2"
Insecticide, Liquid	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys	3, 4, 21, 41, 71	5%	Cool; away from Foodstuffs	"2"
Insect Spray	..	See "Fly Spray"				
Iron Mass, Spent	INFLAMMABLE SOLID (Section 2)	Metal containers	113	..	Open steel trucks	"2"
Iron Powder	..	See "Iron Mass, Spent"				
Iron Sponge, Spent	..	See "Iron Mass, Spent"				
Isobutane (Freezol)	..	See "Hydrocarbon Gas, Liquefied"				
Isobutylene	COMPRESSED GAS (Section 1) Liquefiable Inflammable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 0.53	Well ventilated, cool; away from Batteries, Acids	"2"
Iso-octane (Iso-octene)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums	3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Isopentane	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums	3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Isoprene	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums	3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Isopropanol	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Isopropyl Alcohol (Isopropanol)	..	See "Alcohol Iso-propyl"				
Kerosene	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases or fibreboard containers, drums Tins in truck lots Tank trucks	1, 3, 4, 21, 22, 64, 67, 71, 83, 84	5%	Well ventilated, cool; away from Batteries, Acids	N.O.S. "C" Minimum, 8 tons per 11 ton truck or 12 tons per 16, 22 or 27 ton truck—"B" Minimum, 6 tons per 11 ton truck or 8 tons per 16, 22 or 27 ton truck—"B" plus 12½% In bulk in owner's tank trucks, subject to a minimum of full carrying capacity of truck—"B" Empty tank trucks shall be returned free In mixed loads with Motor Spirit (see General Condition No. 15, page 13, Volume 1)
Lacquer	..	See "Paints, containing Inflammable Liquid"				*"2"
Lacquer, Base or Chips, Dry	INFLAMMABLE SOLID (Section 2)	Tins in cases, drums	22, 76	..	Away from Batteries, Acids	"2"
Lacquer Base or Chips, Plastic (wet with Alcohol or Solvent)	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, carboys, drums	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Lacquer Removing, Reducing and Thinning Compounds	..	See "Compounds, Lacquer or Varnish, etc., Removing, Reducing or Thinning"				
Lactic Acid (80% and over)	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, jars in crates, wooden barrels, drums	3, 4, 37, 38, 41, 45, 51, 76	10%	Cool; away from Acids and Oxidising Materials	"A"
Lead Arsenate, Paste or Solid	..	See "Arsenate of Lead (Paste or Solid)"				

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I.

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Lead Arsenite	*POISONOUS SOLID* (Section 5)	*Bottles, tins or paper* bags in cases, drums, barrels, fibreboard containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	*Dry; away from Foodstuffs* and Foodstuff Empties	Same as arsenate of Lead. See page 24
Lead Cyanide	..	See "Cyanide of Copper, Zinc, Lead or Silver"				
Lead Nitrate	OXIDISING SOLID (Section 6)	Bottles or tins in cases, drums, lined cases	11, 23, 28, 76	..	Dry; away from Inflamcables, Acids, Foodstuffs	"2"
Lead Tetra-ethyl	POISONOUS LIQUID and VAPOUR (Section 5) Dangerous to skin	Bottles in cases, drums, cylinders Special authority required	12, 74, 92	10%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Leather Bleach	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys, plastic containers Tank trucks	1, 3, 4, 21, 22, 37, 38, 41, 71	5%	Well ventilated, cool; away from Batteries, *Acids, Foodstuffs*	"2"
Leather Dressing	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 71	5%	Well ventilated, cool, away from Batteries, Acids, Foodstuffs	"2"
Lime, Quick or Unslaked ..	REACTS WITH WATER (Section 3)	Tins in cases, sacks, drums, bulk	22, 63, 75, 91	..	Dry stowage. Trucks to be covered with two thoroughly waterproof tarpaulins	N.O.S. "M" Minimum, 3 tons—"A" Smaller quantities, "Smalls Minimum"—"B" See pages 155 and 159, Volume I
Liquefied Carbon Dioxide ..	..	*See "Carbon Dioxide"*				
Liquefied Hydrocarbon Gas ..	..	See "Hydrocarbon Gas, Liquefied"				
London Purple Solid	POISONOUS SOLID (Section 5)	Bottles, tins or paper bags in cases, casks, drums, fibreboard containers	5, 17, 22, 26, 28, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Lithium Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustibles, Acids	"2"
Lysol	*POISONOUS LIQUID* (Section 5) Corrosive	Bottles in cases, carboys, jars in crates	3, 4, 41, 45	5%	Well ventilated, cool; away from Foodstuffs	Same as Disinfectant Fluids

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Magnesium Arsenate	POISONOUS SOLID (Section 5)	Bottles, tins or paper bags in cases, casks, drums, fibreboard containers, sacks	5, 17, 22, 26, 28, 52, 53, 55, 61, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Magnesium, Metallic and Alloys, Powder, Turnings ..	INFLAMMABLE SOLID (Section 2) Evolves Hydrogen with Water	Tins in cases, drums, casks, cases, fibreboard containers	22, 29, 52, 55, 64, 73, 75	..	Dry; away from Inflammable Liquids, Acids, Batteries, Water Alkalis, Oxidising Materials	"2"
Magnesium Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Inflammables, Acids, Batteries, Foodstuffs	"2"
Magnesium Perchlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Inflammables, Acids, Batteries, Foodstuffs	"2"
Magnesium Peroxide, Solid ..	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Inflammables, Acids, Batteries, Foodstuffs	"2"
Matches, Bengal	INFLAMMABLE SOLID (Section 2)	Tins in cases	122	..	Dry; away from Inflammables, Liquids	"2"
Matches, Safety	INFLAMMABLE SOLID (Section 2)	Cases, fibreboard conboard containers	114	..	Dry; away from Inflammables, Acids	"2"
Matches, Safety Fusee	..	See "Fusees, Safety"				
Matches N.O.S. (Other than fireworks)	INFLAMMABLE SOLID (Section 2)	Cases	115	..	Dry; away from Inflammables, Acids	"2"
Medicines	..	See "Chemicals, Drugs and Medicinal Preparations"				
Mercuric Acetate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibreboard containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Ammonium Chloride, Solid	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks. drums, fibreboard containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Benzoate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibreboard containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Mercuric Bromide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Chloride	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Cyanide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks	5, 22, 29, 54, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Iodide	POISONOUS SOLID (Section 5)	Bottles or tins in cases casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Iodide Solution ..	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, carboys, barrels, drums Tank trucks	1, 3, 4, 21, 41, 51, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Mercuric Oleate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Oxide (Red and Yellow)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Oxy-Cyanide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Potassium Cyanide .	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums	5, 22, 29, 54, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Potassium Iodide ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Salicylate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Mercuric Subsulphate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Cyanamide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercuric Sulphocyanate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurol (Mercury Nucleate)	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurous Bromide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22. 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurous Gluconate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurous Iodide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurous Nitrate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurous Oxide (Black) ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercurous Sulphate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercury Acetate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercury Bichloride	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Mercury Bisulphate	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums, fibre-board containers	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mercury Compounds, N.O.S. .	..	Packing to the approval of the Railways Commissioners				"2"
Mercury Cyanide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums, casks	5, 22, 29, 54, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Mesityl Oxide	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Metaldehyde	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases, drums, wooden kegs	5, 22, 52, 55, 76	..	Cool, dry; away from Batteries, Acids	"2"
Metal Polishes	..	See "Polishes, Metal, Stove, Furniture and Wood, Solvent Base"				
Metallic Sodium or Potassium	..	See "Potassium, Metallic"				
Metaphenylenediamine	..	See "Dyes Intermediates"				
Metatoluylenediamine	..	See "Dyes Intermediates"				
Methane	COMPRESSED GAS (Section 1) Permanent, Inflammable	Steel cylinders, high pressure 1,800 lb. per sq. in.	132, 134	..	Well ventilated, cool; away from Acids, Batteries	"2"
Methanol (Wood Alcohol) ..	..	See "Alcohol, Wood"				
Methyl Acetate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries, Foodstuffs	"2"
Methyl Acetone	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries	"2"
Methyl Alcohol	..	See "Alcohol, Wood"				"2"
Methyl Amyl Acetate	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids, Batteries	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Methyl Amyl Acetone	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids, Batteries	"2"
Methyl Aniline	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Acids, Batteries, Foodstuffs	"2"
Methylated Spirit	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	N.O.S., *"2" In drums or rail tank trucks for power purposes—Same rate and conditions as Motor Spirit.
Methyl Bromide	COMPRESSED GAS (Section 1) Low Pressure, Special, Liquefiable, Poisonous, Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 1.43	Well ventilated, cool; away from Acids, Batteries, Foodstuffs	"2"
Methyl Chloride	COMPRESSED GAS (Section 1) Low Pressure, Special, Liquefiable, Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.79	Well ventilated, cool; away from Acids, Batteries, Foodstuffs	"2"
Methyl-Cyclo Hexonone	INFLAMMABLE LIQUID (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated; away from Batteries, Acids	"2"
Methyl Ethyl Ketone	..	See "Ethyl Methyl Ketone"				
Methyl Formate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Mineral Oil	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, or fibre-board containers, drums. Tank trucks	1, 3, 4, 21, 22, 64, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	N.O.S. "2" Consigned to Country Tanneries in Victoria— Minimum: 6 tons per 4 wheeled truck—*"M" Minimum: 10 cwt.—*"A" less 25%

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Mineral Turpentine	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys, fibre-board containers Tank trucks	1, 3, 4, 21, 22, 41, 64, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	N.O.S., "1" Minimum: 8 tons per 11 ton truck or 10 tons per 16, 22 or 27 ton truck—"C"
Mixed Acids (Nitrating)	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys	13, 14, 116	5%	Well ventilated; away from Combustible Material, Foodstuffs	"2"
Monochlorobenzene (Monochlorobenzol)	..	See "Chlorbenzene"				
Monochlorodifluoroethane ..	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable Inflammable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 0.86	Well ventilated, cool; away from Batteries, Acids	"2"
Monochlorodifluoromethane ..	COMPRESSED GAS (Section 1) Medium Pressure, Liquefiable, Non-Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.93	Well ventilated, cool; away from Acids	"2"
Monochlorotetrafluoroethane .	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable Non-Inflammable	Steel cylinders, low pressure	134, 135, 137	Filling Ratio 1.15	Well ventilated, cool; away from Batteries, Acids	"2"
Monochlorotrifluoroethylene .	COMPRESSED GAS (Section 1) Medium Pressure, Liquefiable Non-inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 1.00	Well ventilated, cool; away from Batteries, Acids	"2"
Monochlorotrifluoromethane .	COMPRESSED GAS (Section 1) High Pressure, Liquefiable Non-Inflammable	Steel cylinders, high pressure	134, 136	Filling Ratio 0.86	Well ventilated, cool; away from Batteries, Acids	"2"
Monofluorotrichloromethane (F. 11)	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable	Steel cylinders, low pressure	134, 135, 137	..	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Monomethylamine (Methylamine)	COMPRESSED GAS (Section 1) Low Pressure, Special, Liquefiable Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.51	Well ventilated, cool; away from Acids, Batteries	"2"
Mortar Stain, Liquid	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Motion Picture Film	..	See "Film, Nitrocellulose Base"				
Motor Fuel, Anti-knock Compound	..	See "Lead Tetra Ethyl"				
Motor Spirit	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	N.O.S., "1" Minimum: 8 tons per 11 ton truck or 10 tons per 16, 22 or 27 ton truck—"C" Minimum: 6 tons per 11 ton truck or 8 tons per 16, 22 or 27 ton truck—"C" plus 12½% In bulk in owner's truck subject to full carrying capacity of truck—"C" Empty tank trucks shall be returned free. For mixed loads with Kerosene, see General Condition No. 15, page 13, Volume I
Muriatic Acid	..	See "Hydrochloric Acid"				
Naphtha	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"1"
Naphtha Distillate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"1"
Naphtha Petroleum	..	See "Petroleum, Naphtha"				
Naphtha, Solvent	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"1"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Neohexane	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Ngon	COMPRESSED GAS (Section 1) Permanent	Steel cylinders, high pressure 1,800 lb./sq. in.	132, 133, 134	..	Well ventilated, cool; away from Acids	"2"
Nickel Carbonyl	INFLAMMABLE LIQUID Group "A" (Section 2) Extremely Poisonous	Steel cylinders. Special authority required	139	10% filled with CO_2 or an inert gas	Well ventilated, cool; away from Acids, Batteries, Foodstuffs	"2"
Nickel Cyanide	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 29, 54, 76	..	Dry; away from Foodstuffs, Acids	"2"
Nickel Nitrate	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Organic and Combustible Materials, Foodstuffs, Acids. Protect against mechanical injury	"2"
Nicotine and Compounds, Liquid	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, or fibreboard containers drums, carboys Tank trucks	1, 3, 4, 21, 41, 64, 71	5%	Away from Foodstuffs, Acids	"2"
Nicotine Compounds, Solid ..	POISONOUS SOLID (Section 5)	Bottles or tins in cases, or fibreboard containers, drums, casks	5, 17, 22, 26, 52, 53, 55, 64, 73	..	Dry; away from Foodstuffs and Foodstuff Empties, Acids	"2"
Nitrate of Aluminium	..	See "Aluminium Nitrate"				
Nitrate of Ammonia	..	See "Ammonium Nitrate"				
Nitrate of Lead	..	See "Lead Nitrate"				
Nitrate of Potash	..	See "Potassium Nitrate"				
Nitrate of Soda	..	See "Sodium Nitrate"				
Nitrate of Strontia	..	See "Strontium Nitrate"				
Nitrates, N.O.S.	..	Packing to the approval of the Railways Commissioners				"2"
Nitrating (Mixed) Acids ...	..	See "Mixed Acids (Nitrating)"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Nitre	..	See "Potassium Nitrate"				
Nitre Cake	CORROSIVE SOLID (Section 4)	Tins in cases, drums	22, 76	..	Dry; away from Water, Foodstuffs	Minimum: 8 tons per 11 ton truck or 12 tons per 16, 22 or 27 ton truck—"M"
Nitric Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums, plastic containers	13, 14, 42, 78, 79 Up to 50%: 37 and 38	10%	Well ventilated, cool; away from Combustible Materials, Hydrogen Sulphide, Foodstuffs, other Acids	"2"
Nitric Ether	..	See "Ethyl Nitrate"				
Nitrite of Soda	..	See "Sodium Nitrite"				
Nitro-Benzol (Nitrobenzene) .	POISONOUS LIQUID (Section 5) Inflammable Vapour, Oxidising	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids, Batteries, Chlorates, Nitrates	"2"
Nitro-Carbo-Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Foodstuffs, Combustible Material	"2"
Nitrocellulose, Scrap	..	See "Celluloid"				
Nitrocellulose, Wet	..	See "Collodion Cotton"				
Nitrochlorbenzene, Ortho, Liquid	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, carboys, drums Tank trucks	1, 3, 4, 21, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Nitrochlorbenzene, Meta or Para, Solid	POISONOUS SOLID (Section 5)	Bottles or tins in cases, or fibreboard containers, drums, casks	5, 17, 22, 26, 52, 53, 76	..	Dry; away from Foodstuffs, Acids	"2"
Nitrogen	COMPRESSED GAS (Section 1) High Pressure, Permanent	Steel cylinders, high pressure 1800 lb./sq. in.	132, 133, 134	..	Well ventilated, cool; away from Acids	"2"
Nitrogen, plus 5% Hydrogen	COMPRESSED GAS (Section 1) High Pressure, Permanent, Mixed	Steel cylinders, high pressure 1800 lb./sq. in.	132, 133, 134	..	Well ventilated, cool; away from Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Nitrogen Peroxide (Dioxide and Tetroxide)	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable (Special), Poisonous	Steel cylinders, medium pressure. Special authority required	139	..	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Nitroglycerin, Liquid	..	Transport strictly prohibited				
Nitroglycerin, Spirits of	..	See "Spirits of Nitroglycerin"				
Nitrolim	..	See "Calcium Cyanamide"				
Nitrosyl Chloride	COMPRESSED GAS (Section 1) Low Pressure, Special, Liquefiable, Poisonous, Corrosive	Steel cylinders, medium pressure	134, 137	Filling Ratio 1.07	Well ventilated, cool; away from Acids	"2"
Nitrous Ether	..	See "Ethyl Nitrite"				
Nitrous Oxide	COMPRESSED GAS (Section 1) High Pressure, Liquefiable, Anaesthetic	Steel cylinders, high pressure	134, 136	Filling Ratio 0.667	Well ventilated, cool; away from Combustibles, Acids, Oil	"2"
Oiled Fabric	..	See "Fabric or Fibre, Oiled"				
Oil, Kerosene	..	See "Kerosene"				
Oil, N.O.S.	..	Packing to the approval of the Railways Commissioners				"2"
Oil of Mirbane	..	See "Nitro-Benzol"				
Oil of Vitriol	..	See "Sulphuric Acid (over 1.84 gravity)"				"2"
Oil, Paraffin	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2" See page 158, Volume 1
Oil, Petroleum	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	"1"
Oily Cloths, Waste	..	See "Cotton Waste, Cloth, Oily"				
Oleum	..	See "Sulphuric Acid (over 1.84 gravity)"				"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Orthocresol	POISONOUS SOLID (Section 5)	Bottles or tins in cases, casks, drums	5, 17, 22, 52 53, 55, 73, 76	..	Dry; away from Foodstuffs	"2"
Orthonitrotoluol	..	See "Dye Intermediates"				
Orthotoluidine	..	See "Dye Intermediates"				
Oxalic Acid	POISONOUS SOLID (Section 5)	Lined sacks, bottles, tins, paper bags in cases, casks, drums	5, 22, 52, 55, 73, 76, and if dry 26, 61	..	Dry; away from Foodstuffs	"2"
Oxidising Materials, N.O.S. ..	..	Packaging to the approval of the Railways Commissioners				"2"
Oxygen	COMPRESSED GAS (Section 1) Permanent	Steel cylinders, high pressure 1,800 lb./sq. in.	132, 133, 134	..	Well ventilated, cool; away from Acids, Oil, Combustibles	N.O.S. "2" Minimum 6 tons per 4 wheeled truck—"1"
Oxygen, Liquid	COMPRESSED GAS (Section 1) Liquefied, Inflammable	Insulated metal containers. Special authority required	117	..	Cool; open air, isolated stowage; away from naked lights or other sources of ignition, inflammables	"2"
Paint or Varnish Driers	..	See "Paints, containing Inflammable Liquids"				"2"
Paint Remover (Acid Type) .	..	See "Compounds, Paint or Varnish Removing, Acid Type"				
Paint Remover (Caustic Type)	..	See "Compounds, Paint or Varnish Removing, Caustic Type"				
Paint Remover (Solvent Type)	..	See "Compounds, Lacquer or Varnish Removing, Reducing, Thinning"				
Paints, containing Inflammable Liquids (flash point not exceeding 150° Fahrenheit)	INFLAMMABLE LIQUID Group "A" (Section 2)	Jars or tins in cases, drums	3, 4, 5, 21, 22, 71, 73	5%	Well ventilated, cool; away from Acids, Foodstuffs, Batteries	*"2"
	Group "B" (Section 2)	Jars or tins in cases, fibreboard containers, drums	3, 4, 5, 21, 22, 64, 71, 73			
Paper Stock, Wet	INFLAMMABLE SOLID (Section 2)	Cases, drums	31, 75	..	Away from Inflammables	"2"
Paraffin	..	See "Oil, Paraffin"				

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Paraldehyde	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, or fibreboard containers, carboys, drums Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Paranitraniline	POISONOUS SOLID (Section 5)	Bottles or tins in cases, or fibreboard containers, drums, casks	5, 17, 22, 26, 52, 53, 55, 64, 73, 76	..	Dry; away from Foodstuffs and Foodstuff Empties	"2"
Paranitrophenol	..	See "Dye Intermediates"				
Paranitrosodimethylanaline ..	..	See "Accellerene"				
Paranitrotoluol	..	See "Dye Intermediates"				
Paris Green, Solid	..	See "Copper Acetoarsenite"				
Pentane	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Pentane Methyl	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Perchlorate of Ammonia	..	See "Ammonium Perchlorate"				
Perchlorate of Potash	..	See "Potassium Perchlorate"				
Perchlorates, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Perchlorethylene	POISONOUS LIQUID AND VAPOUR (Section 5)	Bottles or tins in cases, drums	3, 4, 21, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Perchloric Acid, over 72% strength	..	Must not be offered for transport				
Perchloric Acid, not exceeding 72% strength	CORROSIVE LIQUID (Section 4)	Bottles in cases	16	10%	Well ventilated, cool; away from Inflammables, Oxidising Materials, Nitric Acid	"2"
Permanganate of Potash ..	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, or fibreboard containers, cases, casks, drums	11, 23, 28, 52, 55, 64, 76	..	Dry; away from Combustible Materials, Foodstuffs, Acids	"1"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Permanganate of Soda	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, or fibreboard containers, cases, casks, drums	11, 23, 28, 52, 55, 64, 76	..	Dry; away from Combustible Materials, Foodstuffs, Acids	"2"
Permanganate, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Peroxide of Sodium	..	See "Sodium Peroxide"				
Pesticides, N.O.S.	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 17, 22, 52, 55, 64, 76	..	Dry; away from Foodstuffs	N.O.S. "2" If declared as Orchard Pests Specifics, see page 61, Volume I
	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, carboys, drums	3, 4, 21, 41, 64 (Arsenic and Nicotine compounds in tins only), 71	5%	Cool; away from Foodstuffs	
	INFLAMMABLE LIQUID Group "A" or "B" (Section 5)	Bottles or tins in cases, carboys, fibreboard containers, drums	3, 4, 21, 22, 41, 64, 71	5%	Well ventilated, cool; away from Batteries and Acids	
Petrol	..	See "Motor Spirit"				
Petroleum (Crude)	..	See "Fuel Oil"				
Petroleum Distillate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	See Distillate
Petroleum Ether	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Petroleum Gas, Liquid	..	See "Hydrocarbon Gas, Liquefied"				
Petroleum Naphtha	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids	"1"
Phenol	..	See "Carbolic Acid"				
Phenylcarbylamine Chloride ..	POISONOUS LIQUID AND GAS (Section 5)	Steel cylinders	93	..	Well ventilated, cool; away from Acids, Foodstuffs	"2"
Phenyldichlorarsine Liquid ..	POISONOUS LIQUID (Section 5)	Steel cylinders, drums	81, 93	..	Well ventilated, cool; away from Acids, Foodstuffs	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Phenyle	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, or fibreboard containers, drums	3, 4, 21, 22, 64, 71	5%	Cool; away from Acids, Foodstuffs	Same as Disinfectant Fluid
Phosgene (Diphosgene)	COMPRESSED GAS (Section 1) Low Pressure, Special; Liquefiable, Poisonous	Steel cylinders, medium pressure. Special authority required	139	Filling Ratio 1.19	Well ventilated, cool; away from Acids, Foodstuffs	"2"
Phosphide of Calcium	..	See "Calcium Phosphide".				
Phosphoric Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, jars in crates, plastic containers	3, 4, 37, 38, 41, 45	5%	Well ventilated; away from Acids, Foodstuffs	"2"
Phosphoric Anhydride (Phosphorus Pentoxide)	CORROSIVE SOLID (Section 4)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Foodstuffs, Batteries	"2"
Phosphorus Amorphous, Red .	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases, drums	5, 22, 76	..	Cool; away from Foodstuffs, Chlorates, Acids, Oxidising Materials, Inflammables	"2"
Phosphorus, Black	..	Not acceptable for transport				
Phosphorus Oxychloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 44, 77	10%	Well ventilated, cool; away from Foodstuffs, Combustible Materials	"2"
Phosphorus Pentachloride ...	INFLAMMABLE SOLID (Section 2) Corrosive	Bottles in cases, drums, casks, carboys	11, 44, 51, 77	..	Dry; away from Foodstuffs, Acids, Combustible Materials	"2"
Phosphorus Pentasulphide (Sesquisulphide, Trisulphide)	INFLAMMABLE SOLID (Section 2) Liable to Spontaneous Ignition unless free from White Phosphorus	Tins in cases, drums	21, 22, 75	..	Well ventilated, cool; away from Acids, Foodstuffs, Inflammables	"2"
Phosphorus Tribromide	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 44, 77	10%	Well ventilated, cool; away from Foodstuffs, Combustible Materials	"2"
Phosphorus Trichloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 44, 77	10%	Well ventilated, cool; away from Foodstuffs, Combustible Materials	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Phosphorus, White or Yellow, in Water	INFLAMMABLE SOLID (Section 2) Ignites Spontaneously in Air	Under water in tins in cases, drums	21, 22, 71, 76	..	Cool; away from Inflammables, Acids, Foodstuffs, Oxidising Materials	"2"
Photographic Flash Powder ..	INFLAMMABLE SOLID (Section 2)	Tins in cases	34	..	Dry; away from Inflammable Materials, Acids, Foodstuffs	"2"
Photographic Film	..	See "Films, Nitrocellulose Base"				
Picrate of Ammonia	..	See "Amonium Picrate, Wet"				
Picric Acid, Mixed with not less than half its weight of Water	INFLAMMABLE SOLID (Section 2) Explosive when Dry.	Bottles in cases	102	..	Cool; away from Batteries, Acids, Inflammables, Lead or Metallic Oxides, Lime, Potash, Soda or Compounds of Oxidising Materials	"2"
Plastic, Wood	INFLAMMABLE LIQUID Group "A" (Section 2)	Tins and collapsible tubes in cases, drums	19, 21, 22, 71	..	Well ventilated, cool; away from Batteries, Acids	"2"
Poisonous Gases, Liquids, Solids, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Polishes, Metal, Stove, Furniture and Wood, Solvent Base	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums	3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	Stove N.O.S. "2" Stove—Produce of Commonwealth on "Up" journey—*"A" N.O.S. "2"
Potash, Caustic	..	See "Caustic Potash"				
Potash, Caustic Solution ..	CORROSIVE LIQUID (Section 4)	Bottles or tins in cases, drums, carboys	3, 4, 21, 22, 41, 72, 74	5%	Cool; away from Acids, Foodstuffs	"2"
Potassium Arsenate Solid ..	POISONOUS SOLID (Section 5)	Paper bags, bottles or tins in cases or fibreboard containers, casks, drums	5, 22, 26, 52, 55, 64, 76	..	Dry; away from Foodstuffs	"2"
Potassium Arsenite Solid ..	POISONOUS SOLID (Section 5)	Paper bags, bottles or tins in cases or fibreboard containers, casks, drums	5, 22, 26, 52, 55, 64, 76	..	Dry; away from Foodstuffs	"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Potassium Bromate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Combustible Materials, Inflammables	"2"
Potassium Bichromate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 29, 54, 76	..	Dry; away from Acids, Combustible Materials, Inflammables	"A"
Potassium Chlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Foodstuffs, Combustible Materials, Sulphur, Sugar, Ammonia Compounds, Oil of Mirbane	"1"
Potassium Cyanide	..	See "Cyanide of Potassium"				
Potassium Hydroxide	..	See "Caustic Potash"				
Potassium Metallic	INFLAMMABLE SOLID (Section 2)	In kerosene or mineral oil in bottles or tins in cases, drums in cases	7, 82	..	Dry, cool; away from Acids, Water, Combustible Material, Foodstuffs, Batteries, Inflammable Liquids	"2"
Potassium Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums, sacks	11, 23, 63, 76	..	Dry; away from Acids, Foodstuffs, Combustible Material	N.O.S. "1" To Powder Factories on declaration—"B" For use in Bacon Factories on declaration—"B"
Potassium Nitrate, Mixed (fused) with Sodium Nitrite	OXIDISING MATERIAL (Section 6)	Drums	76	..	Dry; away from Acids, Foodstuffs, Combustible Material	"2"
Potassium Nitrite	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Foodstuffs, Combustible Material	"2"
Potassium Oxide	CORROSIVE SOLID (Section 4)	Bottles or tins in cases, drums	5, 22, 75	..	Dry; away from Acids, Foodstuffs	"2"
Potassium Perchlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Foodstuffs, Combustible Material	"2"
Potassium Permanganate ..	..	See "Permanganate of Potash"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Potassium Peroxide	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Foodstuffs, Combustible Material	"2"
Potassium Sulphide, Anhydrous	INFLAMMABLE SOLID (Section 2) Liable to ignite in air	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Acids, Foodstuffs, Combustible Material	"2"
Printers' Ink	..	See "Ink, Printers'"				
Propyl Acetate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, carboys, drums	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Propyl Alcohol	..	See "Alcohol Propyl"				
Propyl Alcohol-iso	..	See "Alcohol-Iso Propyl"				
Propane	..	See "Hydrocarbon Gas, Liquefied"				
Propylene	COMPRESSED GAS (Section 1) Inflammable, Liquefiable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.42	Well ventilated, cool; away from Batteries, Acids	"2"
Prussic Acid	..	See "Hydrocyanic Acid Solution"				
Pyridine	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Pyroligneous Acid	..	See "Acetic Acid"				
Pyrophosphoric Acid	..	See "Phosphoric Acid"				
Pyrosulphuryl Chloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 74	10%	Well ventilated, dry; away from Foodstuffs, Nitric Acid, Combustible Material, Ammonia, Caustic and Alkaline Material	"2"
Pyroxylin Paints	..	See "Paints, containing Inflammable Liquids"				
Pyroxylin Plastics	..	See "Celluloid"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Pyroxylin Solutions	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Pyroxylin Solvent	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, carboys, drums Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Pyroxylin Solvent, N.O.S. ..	..	Packaging to the approval of the Railways Commissioners				"2"
Quick Lime	..	See "Lime, Quick, Unslaked"				
Rabbit Poisons	POISONOUS SOLID (Section 5) Inflammable	Tins in cases or fibre-board containers, drums	22, 64, 76	..	Dry; away from Foodstuffs, Batteries, Acids	"1"
Rabbit Poisons containing Poisonous and Irritant Gas	POISONOUS GAS (Section 5)	Tins in cases, steel cylinders	21, 92	..	Well ventilated, cool; away from Foodstuffs, Acids	"1"
Rags, Oily	..	See "Cotton Waste, Cloth, Oily"				
Radio-active Materials	VARIOUS	To approval of Railways Commissioners	94	..	Dry; away from Photographic Film	"2"
Rags, Wet	..	See "Cotton Waste, Cloth, Wet"				
Rat Poison	POISONOUS SUBSTANCE (Section 5)	Tins in cases or fibre-board containers, drums	22, 64, 76	..	Dry; away from Foodstuffs, Batteries, Acids	"2"
Rectified Spirit	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, carboys, drums Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Reducing Compounds	..	See "Compounds, Lacquer or Varnish, etc., Removing, Reducing or Thinning"				
Removing Compounds	..	See "Compounds, Paint or Varnish, etc., Removing, Reducing (Acid or Caustic Type"				
Resinate of Cobalt	..	See "Cobalt Resinate"				
Rockets, Schermuly (Fireworks)	..	See List of Authorised Explosives, Part II				Same as Fireworks

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Rubber Scrap or Buffings ..	INFLAMMABLE SOLID (Section 2)	Tins in cases, drums, cases, casks	22, 28, 52, 55, 75	..	Away from Batteries	"A" "Smalls" minimum
Rubber Cement and Solution	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles, tins or tubes in cases, drums	4, 19, 21, 22, 71, 73	..	Cool; away from Batteries, Acids	"2"
Rum, Denatured	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Safety Fusees	..	See "Fusees, Safety"				
Safety Matches	..	See "Matches, Safety"				
Salicyclic Acid	INFLAMMABLE SOLID (Section 2) Dust Explosive	Bottles, tins or paper bags in cases or fibre-board containers, casks, drums	5, 22, 26, 52, 55, 64, 76	..	Dry; away from Batteries, Acids	"2"
Saltpetre	..	See "Potassium Nitrate"				N.O.S. "1" To Powder Factories on declaration—"B" For use in Bacon Factories on declaration—"B"
Saltpetre Chile	..	See "Sodium Nitrate"				
Self-heating Hair Waving Pads	INFLAMMABLE SOLID (Section 2)	Containers in cases	118	..	Dry; away from Combustible Materials, Inflammable Liquids	"2"
Sewer Sludae	..	See "Garbage, Tankage, containing less than 8% moisture"				
Scheeles Green	..	See "Copper Arsenite"				
Sheep Dips	POISONOUS LIQUIDS PASTES OR SOLIDS (Section 5)	Containers in cases, tins in cases or fibre-board containers, drums	22, 71, and if dry, 27, 64, 76	5% if Liquid	Dry; away from Foodstuffs, Acids	.. *"B"
Shellac (Spirit Varnish)	..	See "Paints, containing Inflammable Liquid"				"2"
Silicon Chloride (Tetrachloride)	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums Tank trucks	1, 3, 4, 41, 74	5%	Well ventilated, cool; away from Foodstuffs, Inflammables	"2"

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Silver Cyanide	..	See "Cyanides of Copper, Zinc, Lead and Silver"				
Silver Nitrate	OXIDISING MATERIAL (Section 6) Corrosive, Poisonous	Bottles in cases	11	..	Well ventilated, cool; away from Foodstuffs, Combustible Materials, Direct Rays of Sun	"2"
Sludge Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys	120	10%	Well ventilated, cool; away from Foodstuffs, Combustible Materials, Inflammables	"A"
Soap Flakes	REACTS WITH AIR (Section 3)	If packed in accordance with Packing Clause 119 may be treated as non-dangerous goods	..	..	..	Same as Soap. See page 70, Volume 1
Soda, Caustic	..	See "Caustic Soda"				
Sodium Arsenate	POISONOUS SOLID (Section 5)	Containers in cases or fibreboard containers, casks, drums, bags in cases	5, 17, 22, 26, 52, 53, 55, 64, 76	..	Dry; away from Foodstuffs, Acids	"2"
Sodium Arsenite Solution ..	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys Tank trucks	1, 3, 4, 21, 22, 41, 71	5%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Sodium Azide	POISONOUS SOLID (Section 5)	Containers in cases or fibreboard containers, casks, drums	5, 17, 22, 26, 52, 53, 55, 64, 76	..	Dry; away from Foodstuffs, Acids	"2"
Sodium Bisulphite Solution ..	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, casks	3, 4, 51, 71	10%	Well ventilated, cool; away from Foodstuffs, Acids	"2"
Sodium Cacodylate (Sodium Dimethylarsenate)	POISONOUS SOLID (Section 5)	Containers in cases or fibreboard containers, casks, drums	5, 17, 22, 26, 52, 53, 55, 64, 76	..	Dry; away from Foodstuffs, Acids	"2"
Sodium Chlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Acids, Combustible Materials, Foodstuffs, Batteries	N.O.S. "2" When declared to be used for destroying noxious weeds—"A"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Sodium Chlorite	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Batteries, Foodstuffs	"2"
Sodium Cyanide	..	See "Cyanide of Sodium"				
Sodium Hydrosulphite	REACTS WITH AIR OR WATER (Section 3)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs, Acids, Inflammable Liquids	"2"
Sodium Hydroxide	..	See "Caustic Soda"				
Sodium Hypochlorite Liquid ..	..	See "Bleaching Solution"				*"C"
Sodium Metabisulphite	REACTS WITH WATER (Section 3) Corrosive	Bottles in cases, casks, drums	5, 52, 55, 76	..	Dry; away from Acids, Foodstuffs	"2"
Sodium Metallic	..	See "Potassium Metallic"				
Sodium Nitrate (Chile Saltpetre)	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums, sacks in truck loads only	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Inflammables	"A" For manurial purposes, see Manures, page 56, Volume I
Sodium Nitrate Bags, Empty	..	See "Bags, Nitrate of Soda"				
Sodium Nitrite	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Foodstuffs, Inflammables	"2"
Sodium Nitrite (Mixed)	..	See "Potassium Nitrate (Mixed), Fused with Sodium Nitrite"				
Sodium Percarbonate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, or fibreboard containers, casks, drums	11, 23, 29, 64, 75	..	Dry; away from Combustible Materials, Foodstuffs, Acids	"2"
Sodium Perborate	OXIDISING MATERIAL (Section 4)	Bottles or tins in cases, or fibreboard containers, cases, casks, drums	11, 23, 28, 52, 55, 64, 76	..	Dry; away from Combustible Materials, Foodstuffs, Acids	"2"
Sodium Perchlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Foodstuffs	"2"

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Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Sodium Permanganate	..	See "Permanganate of Soda"				
Sodium Peroxide	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Foodstuffs	"2"
Sodium Picromate Wet (with 20% water)	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases	11, 23	..	Cool; away from Inflammables, Liquids, Acids, Foodstuffs	"2"
Sodium Potassium Cyanide ..	..	See "Cyanide of Sodium"				
Sodium Sulphide, Anhydrous .	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases, drums. Containers must be full	5, 22, 76	.. ..	Dry; away from Acids, Combustible Materials, Foodstuffs	"2"
Soldering Fluxes, Flaked or Liquid	CORROSIVE LIQUID OR SOLID (Section 4)	Bottles or tins in cases, jars in crates, carboys, drums	3, 4, 41, 45, and if dry: 5, 22, 76	5% if Liquid	Well ventilated, cool; away from Foodstuffs	"2"
Solution, Rubber	..	See "Rubber Cement and Solution"				
Solvents, N.O.S.	..	Packaging to the approval of the Railways Commissioners				"2"
Spent Acid	..	See "Sludge Acid"				
Spent Iron Mass	..	See "Iron Mass, Spent"				
Spent Iron Sponge (Spent Oxide)	..	See "Iron Mass, Spent"				
Spent Sulphuric Acid	..	See "Sludge Acid"				
Spirits of Nitroglycerin (not over 5% by weight)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases	121	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Spirits of Salts	..	See "Hydrochloric Acid"				"A"
Spirits of Wine	..	See "Alcohol, Ethyl"				
Spirit Varnish	..	See "Paints, containing Inflammable Liquid"				"2"
Stain	..	See "Paints, containing Inflammable Liquid"				"2"
Stannic Chloride	..	See "Tin Tetrachloride"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Storage Batteries, Wet	..	See "Accumulators"				
Stove Polish (Solvent Type) .	..	See "Polishes, Metal, Stove, Furniture and Wood Solvent"				
Strike Anywhere Matches ..	..	See "Matches, N.O.S."				
Strontium Arsenite	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 22, 76	..	Dry; away from Foodstuffs	"2"
Strontium Chlorate, Wet ..	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Foodstuffs, Combustible Materials, Acids	"2"
Strontium Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Foodstuffs, Acids	"2"
Strontium Peroxide	OXIDISING MATERIAL (Section 6)	Bottles or tins in tin-lined cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Foodstuffs	"2"
Strychnine, and Salts thereof	POISONOUS SOLID (Section 5)	Containers in cases or fibreboard containers, casks, cases, drums	5, 17, 28, 52, 53, 55, 64, 76	..	Dry; away from Foodstuffs	"2"
Sulphide of Potassium	..	See "Potassium Sulphide"				
Sulphide of Sodium	..	See "Sodium Sulphide"				
Sulphur Chloride (Mono- and Di-)	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 45, 74, 77	10%	Well ventilated, cool, dry; away from Foodstuffs, Nitric Acid, Oxidising Materials	"2"
Sulphur Dioxide	COMPRESSED GAS (Section 1) Low Pressure, Special; Liquefiable, Irritant	Steel cylinders, medium pressure; large welded steel drums, removable, mounted on rail trucks	134, 137, 138	Filling Ratio 1.19 Cylinders only	Well ventilated, cool; away from Acids, Moisture, Foodstuffs	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Sulphur Trioxide	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42 74	5%	Well ventilated, cool, dry; away from Foodstuffs, Nitric Acid, Oxidising Materials	"2"
Sulphuric Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, plastic containers	13, 14, 42, 45 Up to 50%: 37 and 38	5%	Well ventilated; away from Nitric Acid, Oxidising Materials, Combustible Materials, Alkaline Materials, Foodstuffs, Moisture	*"A"
Sulphuric Acid (over 1.84 specific gravity)	CORROSIVE LIQUID (Section 4)	Steel drums Tank trucks	1, 45, 74	5%		
Sulphuric Acid, Fuming (Oleum or Nordhausen)	..	See "Sulphuric Acid"				"2"
Sulphurous Acid	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, casks	3, 4, 41, 51	10%	Well ventilated, dry; away from Nitric Acid, Foodstuffs	*"A"
Sulphuryl Chloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 74	10%	Well ventilated, dry; away from Nitric Acid, Foodstuffs, Combustible Materials, Caustic and Alkaline Materials	"2"
Tankage	..	See "Garbage Tankage"				
Tar Oil Compounds	INFLAMMABLE LIQUID Group "A" or "B" (Section 2)	Tins in cases, casks, drums Tank trucks	1, 22, 51, 71 83	5%	Well ventilated; away from Batteries, Acids, Foodstuffs	The name of the commodity to be shown on consignment note and charged accordingly
Terebene	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, or fibreboard containers, drums Tank trucks	1, 3, 4, 21, 22, 64, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Tetrachlorethane	..	See "Ethane, Tetrachloride"				
Tetra Ethyl Lead	..	See "Lead, Tetra Ethyl"				

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Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Tetrafluoroethylene (inhibited)	COMPRESSED GAS (Section 1) High Pressure, Liquefiable, Non-Inflammable	Steel cylinders, high pressure	134, 136	Filling Ratio 0.78	Well ventilated, cool; away from Batteries, Acids	"2"
Textile Waste, Wet	..	See "Cotton Waste, Cloth, etc., Wet"				
Thallium Salts	POISONOUS SOLID (Section 5)	Containers in cases or fibreboard containers, casks, drums, cases	5, 17, 22, 26, 52, 53, 55, 64, 76	..	Dry; away from Acids, Foodstuffs	"2"
Thallium Sulphate	POISONOUS SOLID (Section 5)	Containers in cases or fibreboard containers, casks, drums, cases	5, 17, 22, 26, 52, 53, 55, 64, 76	..	Dry; away from Acids, Foodstuffs	"2"
Thermon	..	See "Dichloromonofluoromethane"				
Thinning Compounds, Paints, etc.	..	See "Compounds, Lacquer or Varnish, etc., Removing, Reducing or Thinning"				
Thionyl Chloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, jars in crates, carboys, drums	13, 14, 42, 45, 74, 77	10%	Well ventilated, dry, cool; away from Nitric Acid, Foodstuffs, Combustible Materials	"2"
Tin Tetrachloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 74	10%	Well ventilated, dry, cool; away from Nitric Acid, Combustible Materials, Foodstuffs	"2"
Titanium Powder (Wet with not less than 20% water)	INFLAMMABLE SOLID (Section 2)	Tins in cases	25	..	Well ventilated, cool; away from Batteries, Acids	
Titanium Tetrachloride	CORROSIVE LIQUID (Section 4)	Bottles in cases, carboys, drums	13, 14, 42, 74	10%	Well ventilated, dry, cool; away from Nitric Acid, Combustible Materials, Foodstuffs	"2"
Toluene (Toluol)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Trinitrobenzene, Wet (not over 16 oz.)	INFLAMMABLE SOLID (Section 2)	Bottles in cases	102	..	Well ventilated, cool; away from Batteries, Acids	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Trinitrotoluene (not over 16 oz.)	INFLAMMABLE SOLID (Section 2)	Bottles in cases	102	..	Well ventilated, cool; away from Batteries, Acids	"2"
Turpentine and Substitutes	INFLAMMABLE LIQUID Group "B" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 22, 64, 71, 83	5%	Well ventilated, cool; away from Batteries, Acids	*"2"
Turpentine (Mineral)	..	See "Mineral Turpentine"				
Urea Nitrate, Wet (with not less than 10% of water)	INFLAMMABLE SOLID (Section 2)	Bottles in cases	8	...	Well ventilated, cool; away from Batteries, Acids	"2"
Varnish	..	See "Paint, containing Inflammable Liquid"				"2"
Varnish Driers	..	See "Paint, containing Inflammable Liquid"				"2"
Varnish Remover or Reducer	..	See "Compounds, Paint or Varnish Removing (Acid or Caustic Type)"				
Varnish Thinning Compounds	..	See "Compounds, Lacquer or Varnish Removing, Reducing or Thinning"				
Vestas	..	See "Matches, N.O.S."				
Vinyl Acetate	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 71	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Vinyl Chloride (Inhibited)	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable, Special, Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.75	Well ventilated, cool; away from Batteries, Acids	"2"
Vinyl Ether	INFLAMMABLE LIQUID Group "A" (Section 2) Anaesthetic	Bottles or tins in cases, drums	3, 4, 21, 72	5%	Well ventilated, cool; away from Batteries, Acids	"2"
Vinyl Methyl Ether (Inhibited)	COMPRESSED GAS (Section 1) Low Pressure, Liquefiable, Special, Inflammable	Steel cylinders, medium pressure	134, 137	Filling Ratio 0.60	Well ventilated, cool; away from Batteries, Acids	"2"
Waste Cloths, Rags, Cotton, Oily	..	See "Cotton Waste, Cloth, Oily or Wet"				

* For Concessions to Victorian Country Industries, see pages 139 to 148, Volume I

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Waste Paper, Wet	..	See "Paper Stock, Wet"				
Waste, Textile, Wet	..	See "Cotton Waste, Cloth, Wet"				
Waste Wool, Wet	INFLAMMABLE SOLID (Section 2)	Cases, drums	31, 75	..	Away from Inflammables, Acids	"A"
Weedicides, N.O.S.	POISONOUS SOLID (Section 5)	Bottles or tins in cases, drums	5, 17, 22, 52, 55, 64, 76	..	Dry; away from Foodstuffs	"A"
	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, carboys, drums	3, 4, 21, 41, 64 (Arsenic and Nicotine compounds in tins only), 71	5%	Cool; away from Foodstuffs	
	INFLAMMABLE LIQUID Group "A" or "B" (Section 5)	Bottles or tins in cases, carboys, fibreboard containers, drums	3, 4, 21, 22, 41, 64, 71	5%	Well ventilated, cool; away from Batteries and Acids	
Weed Killer, Arsenical	POISONOUS LIQUID (Section 5)	Bottles or tins in cases, drums, carboys, jars	3, 4, 21, 41, 45, 71	5%	Away from Foodstuffs	"A"
Weed Killer, containing Chlorates	OXIDISING MATERIAL (Section 6) Poisonous	Bottles or tins in cases, drums, casks, cases	11, 23, 28, 52, 55, 76; Liquid: 71	..	Cool, dry; away from Ammonia Compounds, Sulphur, Sugar, Acids, Combustible Materials, Foodstuffs	"A"
Westron	..	See "Ethane Tetrachloride"				
Wet Hair	..	See "Hair, Wet"				
Wet Nitrocellulose	..	See "Collodion Cotton"				
Wet Nitroguanidine (with at least 20% water)	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases, casks, drums	3, 4, 21, 51 71, 73	..	Cool; away from Batteries, Acids	"2"
Wet Nitrostarch (with at least 20% water)	INFLAMMABLE SOLID (Section 2)	Bottles or tins in cases casks, drums	3, 4, 21, 51 71, 73	..	Cool; away from Batteries, Acids	"2"
Wet Paper Stock	..	See "Paper Stock, Wet"				
Wet Rags	..	See "Cotton Waste, Cloth, Wet"				
Wet Textile Waste	..	See "Waste, Textile, Wet"				
Wet Wool Waste	..	See "Waste Wool, Wet"				
Wheat, Poisoned	..	See "Rat Poison"				

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
White Spirit	..	See "Turpentine and Substitute"				Minimum 8 tons per 11 ton truck or 10 tons per 16, 22 or 27 ton truck—"C" Smaller quantities—"1"
Wood Alcohol	..	See "Alcohol, Wood"				
Wood Filler (Surfacer)	..	See "Paints, containing Inflammable Liquid"				"2"
Wood, Plastic	..	See "Plastic Wood"				
Wood Polish	..	See "Polish, Metal, Stove, Furniture and Wood (Solvent Type)"				
Wood Stain, Liquid	..	See "Paints, containing Inflammable Liquid"				"2"
Wool Waste, Wet	..	See "Waste Wool, Wet"				
X-Ray Films	..	See "Films, Nitrocellulose Base"				
Xylene (Xylol)	INFLAMMABLE LIQUID Group "A" (Section 2)	Bottles or tins in cases, drums Tank trucks	1, 3, 4, 21, 71	5%	Well ventilated, cool; away from Batteries, Acids, Foodstuffs	"2"
Xylidene	..	See "Dye Intermediates"				
Xylyl Bromide	POISONOUS LIQUID AND VAPOUR (Section 5)	Steel cylinders, drums	74, 92	..	Well ventilated, cool; away from Acids, Foodstuffs	"2"
Zinc Ammonia Nitrite	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Acids, Foodstuffs	"2"
Zinc Arsenate	POISONOUS SOLID (Section 5)	Bottles, tins, paper bags in cases or fibre-board containers, casks, cases, drums	5, 17, 22, 26, 28, 52, 53, 55, 64, 76	..	Dry; away from Foodstuffs	"2"
Zinc Arsenite	POISONOUS SOLID (Section 5)	Bottles, tins, paper bags in cases or fibre-board containers, casks, cases, drums	5, 17, 22, 26, 28, 52, 53, 55, 64, 76	..	Dry; away from Foodstuffs	"2"
Zinc Chlorate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Combustible Materials, Foodstuffs	"2"

Alphabetical List and Freight Classification of Explosives and Goods of a Dangerous Nature—*continued.*

Article.	Sectional Classification (See Part 3) and Properties.	Packing.	Packing Clause No.	Air Space Ullage	Packing, Stowage and Incompatibles.	Freight Classification Class
Zinc Chloride	CORROSIVE LIQUID OR SOLID (Section 4)	Bottles in cases, carboys, jars in crates	3, 4, 41, 45	5% if Liquid	Dry; away from Foodstuffs, Acids	"2"
Zinc Cyanide	..	See "Cyanide of Copper, Zinc, Lead or Silver"				
Zinc Dust	REACTS WITH WATER (Section 3)	Bottles or tins in cases, drums	3, 4, 21, 22, 76	..	Dry; away from Acids, Foodstuffs	"2"
Zinc Ethyl	INFLAMMABLE LIQUID Group "A" (Section 2)	Steel cylinders	92	..	Well ventilated, cool; away from Batteries, Acids	"2"
Zinc Nitrate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, drums	11, 23, 76	..	Dry; away from Foodstuffs, Combustible Materials	"2"
Zinc Permanganate	OXIDISING MATERIAL (Section 6)	Bottles or tins in cases, casks, drums	11, 17, 23, 52, 53, 55, 76	..	Dry; away from Combustible Materials, Acids, Foodstuffs	"2"
Zinc Phosphide	REACTS WITH WATER (Section 3)	Tins or drums in cases	22, 80	..	Dry, well ventilated; away from Acids, Inflammables, Foodstuffs	"2"
Zirconium, Metallic, Dry ..	INFLAMMABLE SOLID (Section 2)	Tins in cases	23	..	Dry; away from Foodstuffs, Batteries, Acids	"2"
Zirconium, Metallic, Wet or Sludge	INFLAMMABLE SOLID (Section 2)	Tins in cases	23	..	Dry; away from Foodstuffs, Batteries, Acids	"2"
Zirconium Picramate, Wet (with 30% water)	OXIDISING MATERIAL (Section 6)	Bottles in cases, casks	11, 51	..	Cool; away from Combustible Materials, Foodstuffs, Acids	"2"

PART V.

(A) PACKAGING CLAUSES FOR GOODS OF A DANGEROUS NATURE.

Clause No.

1 Tank cars constructed and maintained in accordance with the Australian and New Zealand Railways Manual of Standard and Recommended Practice, Section G.

2 Tanks or vessels, constructed, maintained, and conveyed, in accordance with the requirements of the Railways Commissioners for the carriage of substances in bulk. All such vessels to be conspicuously marked with the words DANGER and the name of the contents.

All requirements as set out in the appropriate section of PART III must be strictly observed.

3 Glass or earthenware bottles securely stoppered, each containing not more than ½ gallon, packed in sawdust or other cushioning material, in cases to Specification No. 1A. Each bottle to be surrounded with not less than 1½ inches of cushioning material.

4 Glass or earthenware bottles securely stoppered, each containing not more than 1 gallon, packed in sawdust or other cushioning material, in cases to Specification No. 1A. Each bottle to be separated by a wooden partition not less than ½ inch thick.

5 Glass or earthenware containers with screw caps, clip-on lids, tight-fitting bungs or other non-leaking type seal, packed in sawdust or other cushioning material in cases to Specification No. 1A. Each container to be surrounded with not less than 1 inch of cushioning material.

6 Glass bottles, securely stoppered, each containing not more than 1 quart, packed in sawdust or other cushioning material, in cases to Specification No. 1A. Each bottle to be surrounded with not less than 1 inch of cushioning material. Maximum gross weight of each case, 2 cwt.

7 Immersed in kerosene or mineral oil in glass or earthenware bottles, securely stoppered, not over ½ gallon capacity, packed in sawdust in a wooden case to Specification No. 1A. Maximum gross weight of each case, 1 cwt.

8 Glass or earthenware bottles, securely stoppered, packed in sawdust or other suitable cushioning material, in cases to Specification No. 1A.

Maximum nett dry weight of contents of any outside container, 25 lb.

9 Sealed glass or earthenware bottles not over 5 pints capacity or sealed tinplate containers not over 1 gallon capacity, packed in sawdust or other cushioning material, in cases to Specification No. 1A.

Gross weight not to exceed 1 cwt.

10 Sealed glass capsules, packed in sawdust or other cushioning material, in cases to Specification No. 1A.

11 Glass or earthenware containers with screw caps, clip-on lids, tight-fitting bungs or other leakproof seal of non-combustible material, packed in kieselguhr or other non-combustible cushioning material, in cases to Specification No. 1A. Each container to be surrounded with not less than 1 inch of cushioning material.

12 Wooden cases to Specification No. 1A with each case containing a single glass inside container not over 1 quart capacity, closed by a ground-glass stopper securely fastened in place, and cushioned with incombustible cushioning material in the outside container.

13. Glass or earthenware bottles, securely stoppered, each containing not more than ½ gallon, packed in whiting, kieselguhr, asbestos, powdered china clay or sifted ashes, in cases to Specification No. 1A. Each container to be surrounded with not less than 1½ inches of cushioning material. All stoppers must be luted and tied down.

Clause No.

14 Glass or earthenware bottles, securely stoppered, each containing not more than 1 gallon, packed in whiting, kieselguhr, asbestos, powdered china clay or sifted ashes, in cases to Specification No. 1A. Each bottle to separated by a wooden partition not less than ½ inch thick. All stoppers must be luted and tied down.

15 Wooden cases to Specification No. 1A, with inside glass bottles not over 7 lb. capacity each, individually enclosed in tightly-closed metal cans and cushioned therein with whiting, kieselguhr, asbestos, powdered china clay or sifted ashes. All stoppers must be luted and tied down.

16 Glass bottles not over 2 lb. capacity or earthenware bottles not over 5 lb. capacity, securely stoppered, packed in whiting, kieselguhr, asbestos, powdered china clay or sifted ashes, in cases to Specification No. 1A. Each bottle to be surrounded with not less than 1½ inches of cushioning material. All stoppers must be luted and tied down.

17 Inside glass, earthenware or metal containers, or lock corner, sliding lid, wooden boxes, of not over 5 lb. capacity each, or chip board, pasteboard or cardboard cartons, cans or boxes of not over 1 lb. capacity each, packed in outside wooden cases to Specification No. 1A.

18 Hermetically sealed glass tubes or bottles containing not more than 1 lb. of liquid, each surrounded with not less than ½ inch of absorbent material in sealed metal cans, packed in wooden cases to Specification No. 1A. Total amount of liquid per case must not exceed 30 lb.

19 Collapsible tubes, in cardboard cartons, packed in cases to Specification No. 1B.

20 Glass bottles, luted tied-down stoppers, up to 1 lb. capacity, wrapped in straw and packed in tin-lined sealed cases, with sufficient slaked lime to neutralise the whole of the contents of the case. Maximum contents of the case 20 lb. Cases must be made of sound timber, minimum thickness ½ inch, close jointed, bound with hoop iron or wire.

21 Sealed tins not over 1 gallon capacity, packed in wooden cases to Specification No. 1B. Tins to be a neat fit within the case or packed in sawdust or other cushioning material in cases to Specification No. 1A.

22 Sealed tins not over 4 gallons capacity, packed in wooden cases to Specification No. 1B. Tins to be a neat fit within the case or packed in sawdust or other cushioning material in wooden cases to Specification No. 1A. Tinplate to be a minimum of 30 B.G. (0.012") thickness. Tins with press-on type lids must have lids secured by at least three spots of solder.

23 Metal tins or cans securely closed, press-on type lids to be secured in place by at least two spots of solder, packed in cases to Specification No. 1B. Tins to be a neat fit within the case or packed in kieselguhr or other non-combustible cushioning material in cases to Specification No. 1A.

24 Hermetically sealed tins, each containing not more than 150 starters, with a maximum of 10 tins in a felt-lined case to Specification No. 1B.

25 Sealed tins not over 1 gallon capacity, packed in cases to Specification No. 1B. Tins to be a neat fit within the case, or packed in sawdust or other cushioning material in cases to Specification No. 1A. Maximum gross weight of each case, 2 cwt.

26 Double paper bags of sift-proof construction, the inner bag or lining to be made of waterproof paper, packed in waterproof paper-lined wooden cases to Specification No. 1A.

27 Strongly made cardboard or fibreboard boxes or cartons with waterproof inside lining, packed in wooden cases to Specification No. 1B.

28 Wooden cases to Specification No. 1A, lined with waterproof paper to render them sift-proof.

29 Strongly constructed wooden cases, lined with tinplate or sheet metal, which must completely protect the wood of the case and be sift-proof.

30 Wooden cases to Specification No. 1B, lined with sheet zinc so as to be sift-proof.

Clause No.

31 Hermetically sealed, strongly made, metal-lined wooden cases or boxes.

32 Glass or plastic inside containers, maximum capacity 1 pint, packed in individual tins with sawdust. Tins to be a neat fit or packed with cushioning material in cases to Specification No. 1A. Maximum contents 12 pints.

33 Glass or plastic inside containers, maximum capacity 1 pint, in individual envelopes of corrugated fibreboard packed in cushioning material in cases to Specification No. 1A. Maximum contents 12 pints.

34 Sealed tins, maximum capacity 5 lb., packed in whiting, kieselguhr, asbestos, powdered china clay, sifted ashes, in cases to Specification No. 1A. Each tin to be surrounded with not less than 1½ inches of cushioning material. Maximum of four tins per case in any one consignment is imposed on this material.

35 28 x 1 lb. glass bottles, securely stoppered, packed in a partitioned fibreboard container to Specification No. 6A. Maximum gross weight 70 lb.

36 Sealed plastic capsules in packets, packed in a wooden case to Specification No. 1A or fibreboard container to Specification No. 6A.
Maximum gross liquid content, 1½ gallons.

37 Approved polythene containers, securely stoppered, each containing not more than half a gallon, packed as a neat fit in a fibreboard container to Specification No. 6A.

38 Approved polythene containers, securely stoppered, each containing not more than one gallon, packed as a neat fit in a fibreboard container to Specification No. 6A.

41 Carboys packed to Specification No. 3A. Cushioning material to satisfactorily withstand tests.

42 Carboys packed to Specification No. 3A. Cushioning material to be whiting, kieselguhr, asbestos, powdered china clay, sifted ashes, or rubber cushioning pads.

43 Carboys packed to Specification No. 3A. Cushioning material to be whiting, asbestos or other non-combustible material.

44 Metal jacketed lead carboys to Specification No. 3B.

45 Earthenware jars to Specification No. 4A.

51 Wooden barrels (wet type), lined with pitch, to Specification No. 5A.

52 Wooden casks (dry type), lined with waterproof paper, to Specification No. 5B.

53 Inside glass, earthenware or metal containers, or lock corner, sliding lid, wooden boxes of not over 5 lb. capacity each, or chip board, pasteboard, or fibre cartons, cans or boxes of not over 1 lb. capacity each, packed in outside wooden casks to Specification Nos. 5B or 7A.

54 Wooden casks (dry type) to Specification No. 5B, lined with sheet metal (soldered) so as to be sift-proof, maximum capacity 2 cwt.

55 Wooden casks, glued plywood, to Specification No. 1A, lined with waterproof paper.

61 Double thickness hessian sacks with waterproof lining, such as tar impregnated paper, for solids in lump form.

62 Double thickness hessian sacks with waterproof lining, such as tar impregnated paper, for calcium cyanamide, with not more than 5 per cent. calcium carbide.

63 Strong jute sacks.

64 Fibreboard containers, with inside glass, earthenware or tinplate containers, packed in accordance with Specification No. 6A.

65 Fibreboard containers to Specification No. 6A, with inside tinplate containers. Maximum contents of outside container 5 lb.
Maximum quantity permitted in fibreboard containers, in any one consignment, 30 lb. Consignments in excess of 30 lb. must be packed in wooden cases to Specification No. 1B, lined with waterproof paper.

Clause No.

66 Fertilisers containing oxidising materials will be accepted for transport when packed in strong jute sacks or double thickness hessian sacks, provided that the sacks are lined with a waterproof material such as tar impregnated paper, and that no organic material is present in the salt.

Sacks must be labelled and marked in accordance with requirements of Section No. 6, Part III.

Steel trucks only are to be used for this traffic.

67 4 gallon sealed tins, minimum thickness 30 B.G. (0.012"), crated or uncrated, in truck lots between Oil Companies' depots. Tins to be packed so as to eliminate movement within the truck.

Fresh, dry sawdust to be used to fill in any spaces between the tins, and with at least 1½ inches of sawdust on the floor of the truck, and between each tier of tins. The load must be covered by a sound tarpaulin, supported by a ridge and well tied down to prevent sagging.

71 Sealed metal drums to Specification Nos. 2A, 2B, 2C.

72 Sealed metal drums to Specification Nos. 2A, 2B.

73 Sealed metal drums to Specification No. 2D.

74 Sealed metal drums to Specification No. 2E.

75 Sealed metal drums to Specification Nos. 2F, 2G.

76 Sealed metal drums to Specification No. 2G.

77 Sealed lead-lined drums to Specification No. 2H.

78 Sealed aluminium or aluminium-lined drums to Specification Nos. 2 "I," 2J.

79 Sealed stainless steel drums to Specification No. 2K.

80 Sealed metal drums to Specification Nos. 2F, 2G, packed in strong wooden cases. Weight of contents not to exceed 1 cwt.

81 Sealed metal drums to Specification Nos. 2A, 2B, not over 25 gallons capacity each, enclosed in strong crates of timber at least ⅝ inch thick.

82 Under kerosene or mineral oil in steel drums to Specification Nos. 2A, 2B, protected by a stout wooden case or crate.

83 Non-returnable (single trip) sealed metal drums, with a maximum capacity of 45 gallons, body and ends constructed of 18 B.G. steel sheet, double seamed or welded and provided with two swedged rolling hoops and leak-proof closures.

These containers are authorised for bulk traffic in truck lots only.

84 Drums to the Interstate Commerce Commission, U.S.A., Specifications No. 5, 5B, 6B, 6C.

85 Terneplate drums of maximum capacity 5 gallons with minimum 20 B.G. (0.039") bodies and 22 B.G. (0.031") ends. Closure to be a screw cap of a minimum 27 B.G. (0.017") tinplate with asbestos neoprene gasket.

Drums to be a neat fit in ½ inch timber cases, not more than two drums per case, subject to a maximum gross weight of 165 lb. Packing pieces between drums and batten over the caps to be of ½ inch thick timber. Cases to be wired with at least two strands of wire.

91 In bulk in steel trucks only. Trucks to be covered with two thoroughly waterproof tarpaulins.

92 Steel cylinders as used for compressed gases, except acetylene, fitted with valve protection cap and gasket.

93 Steel cylinders as used for compressed gases, except acetylene, fitted with valve protection cap and gasket. Cylinders must not exceed 125 lb. water capacity. Gaskets must be renewed before each consignment, even though such gaskets may appear undamaged.

Clause No.

94 Radio-active materials are not acceptable for transport by rail without the consent in writing of the Chief Traffic Manager. Packages shall be conspicuously marked with the words "DANGER," "RADIO-ACTIVE MATERIAL," and shall also display the coloured label described in Section 5, Part III, for poisonous materials.

Packing of radio-active material to conform with accepted standards of safety as recommended by the Commonwealth X-Ray and Radium Laboratory.

101 Cells constructed of glass or other material must be sealed around the top edges with pitch or other suitable material, and the filling holes securely plugged with approved splash proof stoppers.

Cells must be well insulated to prevent short circuits, and packed as a neat fit in strong wooden cases with gable top or in strong, double walled corrugated or solid fibreboard containers with gable tops, of sufficient strength to withstand the weight of the contents.

Any packing material or dunnage used must be of a non-conductive nature, and under no circumstances shall any article or articles other than the batteries and packing material or dunnage be contained in the cases or fibreboard containers.

The outer container to be marked or labelled in distinctive characters, "BATTERY—CONTAINING CORROSIVE ELECTROLYTE—THIS SIDE UP."

Consignors are required to endorse on consignment notes a certificate that the charged batteries or accumulators described thereon have been sealed and packed in accordance with the specifications of the Railway Commissioners.

102 Trinitrobenzene and trinitrotoluene, wet with not less than 10 per cent. water, and ammonium picrate and picric acid, wet with not less than 33 per cent. water, in quantities not exceeding 16 ounces in one outside package, may be carried when in glass bottles, securely stoppered, each bottle being well cushioned within a strong fibreboard carton, which must be packed in cushioning material inside a wooden case to Specification No. 1A.

103 Benzol peroxide, if in powder or crystalline form, must be wet with at least 30 per cent. water by weight, and be packed in containers as follows:—

(a) Tinplate containers, not over 10 lb. gross weight, securely sealed, packed in wooden cases to Specification No. 1B. Tins to be a neat fit within the case or packed in cushioning material in cases to Specification No. 1A.

(b) Aluminium or aluminium-lined drums, securely sealed, to Specification Nos. 2 "I" or 2J. Gross weight of drum not to exceed 200 lb.

Benzoyl peroxide in granular form may be despatched in the dry condition when packed as follows:—

Metal containers, not over 1 lb. capacity, securely sealed and packed in wooden cases to Specification No. 1A. Gross weight of each case not to exceed 50 lb.

Note.—Benzoyl peroxide shall not be considered granular if more than 25 per cent. passes through a 20-mesh sieve.

104 "Burnt cotton" is cotton that has been on fire and from which the burnt portions have not been removed by repicking. It must not be offered for transportation until not less than ten days have elapsed since the last evidence of fire in it. It must be marked and described on consignment note as "Burnt Cotton," and the bales LABELLED with the appropriate label for Inflammable Solids. (See Section 2.)

105 "Calcium Carbide." Each package must bear in conspicuous character the words "CARBIDE OF CALCIUM," "DANGEROUS IF NOT KEPT DRY." "The contents of this package are liable, if brought into contact with moisture, to give off a highly inflammable gas. Name and address of the consignor." See also Section 3, Part 3.

106 Charcoal, when freshly prepared, must be exposed to the air for at least three days before being consigned. The charcoal must be dry. Wet or damp charcoal must not be offered for transport. Charcoal conforming to these requirements may be packed in strong triple thickness paper bags, strong sacks, metal drums, wooden barrels, stout cases, or in truck lots.

Clause No.

107 This clause concerns small quantities of goods of a dangerous nature, having properties described in Section Nos. 2 to 6 included in mixed, cased consignments of chemicals, drugs and medicinal preparations.

LIQUIDS.—The requisite air space must be allowed in each container as shown in the table. Glass and earthenware bottles must be securely stoppered, luted and tied down, and cushioned with sawdust or other cushioning material to prevent containers coming in contact with each other or the sides of the case. Tins must be effectively closed to prevent leakage and packed in cushioning material. The contents of any one container must not exceed 1 gallon.

SOLIDS.—Glass, earthenware or tin containers, securely stoppered to prevent leakage of contents or the admission of moisture, must be packed in sawdust or other cushioning material in cases. Paper bags must be double thickness, new, and of heavy quality paper; the inner bag is to be made of waxed paper. The contents of any one container must not exceed 10 lb.

Special care must be taken to see that substances which react dangerously are not packed in the same case. Each case must not contain more than 1 lb. of 1 per cent. solution of nitro-glycerine in ethyl alcohol or 1 lb. of picric acid, dry.

Maximum gross weight of each case 2 cwt.

Cases must be constructed of sound timber, close jointed, lined with paper and bound with hoop iron or wire, and comply with requirements of Specification No. 1A.

EXCEPTIONS.—Over 4 per cent. hydrofluoric acid, commercial or concentrated nitric, perchloric, sulphuric, or hydrochloric acids, sodium peroxide, carbon bisulphide, phosphorus, metallic barium, calcium, potassium or sodium and 100 volume hydrogen peroxide, must not be included in mixed consignments, but must be consigned separately in accordance with requirements shown in the table. THESE MUST NOT BE CONSIGNED BY PASSENGER TRAIN.

CONVEYANCE BY PASSENGER TRAIN.—In general the conveyance of dangerous goods by passenger train is strictly prohibited and their inclusion in mixed consignments of chemicals, drugs and medicinal preparations should be avoided.

The maximum gross weight of a case containing any dangerous goods for conveyance by passenger train is 14 lb. and all containers must be adequately cushioned.

The contents of any one container for liquids must not exceed 1 quart.

The restrictions outlined in preceding paragraph titled "EXCEPTIONS" must be strictly observed.

MARKING.—Each case must be conspicuously marked "DANGER," "CHEMICALS," "HANDLE WITH CARE," "THIS SIDE UP," and have affixed a white label with red lettering not less than 3 inches square:—

108 Coal, ground bituminous, sea coal, coal facings, 90 per cent. of which will pass through a 100-mesh sieve, must be stored for at least six days after grinding, or if not so stored, must be shipped in tight metal containers or in tight, covered, steel trucks. Coal, ground bituminous, which has been dried by heating before grinding, must be packed in hermetically sealed metal-lined wooden boxes or air-tight drums.

Clause No.

109 **FILMS, NITROCELLULOSE BASE.**

(a) Must be packed in closed metal cans each holding not more than 2,000 feet, the metal cans to be packed in a metal box lined with material which is a non-conductor of heat. The gross weight of each metal box must not exceed 2 cwt.

(b) Metal cases lined throughout with hard faced fibreboard (at least .125 inches in thickness), hinged and fastened as set out in Specification Nos. 8A or 8B, Part VB of the Commissioners' By-laws Where metal partitions are used, they are to be the same thickness as the body of the case and lined the same as the case.

(c) A metal can with a securely fitting lapped detachable cover packed singly in a strong fibreboard carton. When not more than 500 feet of film is in the can a lap of ½ inch will be permitted, but when the quantity exceeds this, and up to a permitted maximum of 2,000 feet of film, the lap shall be ¾ inch. Each carton MUST be securely bound with a strong cord.

110 Gas identification sets, containing poisonous liquids and gases, and poisonous articles may be consigned in amounts not exceeding 5 mls., if a liquid, or 5 grams if a solid when mixed with or absorbed in activated charcoal or silica-gel, or other absorbent medium, and packed in strong glass bottles of not less than 4 fluid ounces capacity. The poisonous gases and chlorine may be consigned if the gas itself is absorbed in, activated charcoal or silica gel, or other absorbent medium—this material to be filled into 4-ounce bottles as above; each bottle must be surrounded with at least 1 inch of appropriate absorbent cushioning material, and enclosed in a hermetically sealed metal can; each can must be surrounded on all sides by at least 1 inch of dry, fine sawdust or wood pulp, and enclosed in a wooden case to Specification No. 1A; the bottles must be closed with ground-in glass stoppers securely fastened; the cans must be made of material not less than 30 B.G (0.012") thickness; and there must not be more than a total of 100 grams or mls., or a combination of both in each outside wooden case.

OR

Gas identification sets, containing poisonous liquids or liquefied gases, may be consigned in hermetically sealed glass tubes containing not more than 40 cubic centimeters each; each tube securely cushioned and packed in an individual mailing tube with screw thread metal cover; not more than twelve of such mailing tubes, cushioned with corrugated fibreboard packed in a closed fibreboard container; and not more than four such fibreboard containers, containing an aggregate of not more than forty-eight glass tubes; cushioned and packed in an outside steel cylinder of not less than 0.145 inches wall thickness, which is closed by a plate, bolted to a flange, welded to cylinder wall. Suitable gasket shall be placed between flange and head plate, and closure shall be such as to prevent leakage of any gas.

111 Approved gutta percha, wax, plastic or lead bottles, securely sealed, packed in sawdust, whiting, straw or other approved absorbent material in strong wooden cases, bound with hoop iron or wire.

Unlined metal drums, to Specification No. 2E, which have been subjected to adequate passification or neutralization process (see Note). Authorised only for acids of not less than 60 per cent strength. If any containers are washed out with water, they must be repassified before consigning.

Note.—Each drum before being put into service must be passified by the following or an equally efficient method:—By filling the drum to 90 per cent. capacity with hydrofluoric acid of 58 per cent strength and allowing drum to stand 48 hours at a temperature of 80° F., and then 7 hours at 140° F., the internal pressure being maintained at atmospheric pressure by means of a ventilating bung. All drums must be tested before being consigned by subjecting them to an internal pressure of 30 lb./sq. in., then filling to not over 80 per cent. of their capacity, held for at least 8 hours after filling, and then vented just before consigning.

Tank trucks constructed and maintained in accordance with Australian and New Zealand Railways Manual of Standard and Recommended Practice, Section G, unlined, which have been passified or neutralized as described above (see Note), are authorised for acid of 60 per cent. to 80 per cent. strength If tanks are washed out with water, they must be repassified before consigning. Tanks lined with rubber are authorised only for acids not over 40 per cent. strength.

All drums must be loaded and unloaded, and tank trucks filled and emptied, by Consignor and Consignee.

Clause No.

112 Gutta-Percha or wax bottles, securely sealed, packed in sawdust, whiting, straw or other approved absorbent material in strong wooden cases to Specification No. 1A, bound with hoop iron or wire.

Metal drums to Specification No. 2E, lined with rubber, are authorised for acids not exceeding 40 per cent. strength.

Hydrofluosilicic acid containing no free hydrofluoric acid or other ingredient that will attack glass, may be consigned in glass carboys, cushioned with sawdust, whiting, or other approved absorbent material, to Specification No. 3A. Rubber stoppers and gaskets are authorised.

OR

In glass bottles with rubber or ground-in glass stoppers, each containing not more than ½ gallon, packed in sawdust, whiting, or other approved absorbent material, in cases to Specification No. 1A, bound with hoop iron or wire. Each bottle to be surrounded with not less than 1½ inches of cushioning material.

113 Iron mass, spent, spent oxide, iron powder or iron sponge that has not been properly oxidised must be packed in airtight metal containers, or strongly made airtight metal-lined case.

Spent oxide, spent iron mass, iron powder or spent iron sponge must be loaded into open steel cars or trucks.

A written declaration must be obtained from the sender that these articles have been thoroughly oxidised by exposure to air before consigning.

114 Packets of one dozen boxes packed in strong wooden cases or solid or corrugated fibreboard containers to Specification No. 6A, sealed so as to exclude free passage of air.

Flaps of fibreboard containers to be sealed with an approved adhesive and seams and joints to be covered with gummed paper.

Label "SAFETY MATCHES," "INFLAMMABLE."

Note.—Up to one gross boxes of safety matches may be accepted as ordinary merchandise if packed in with groceries or tobacconist's sundries, provided that both the packets of matches and outer containers are sealed to exclude free passage of air, and that no other inflammable solids or liquids are included with the matches.

115 Packets of one dozen boxes packed in strong wooden cases, lined with waxed paper to exclude air.

Cases containing up to 2 cwt. must be constructed of wood not less than ⅜ inch thick at top, bottom and sides, and ⅝-inch ends strengthened with two battens across the lid outside, and two at each end inside of 2½ in. x ½ in. timber.

Cases containing from 2 to 3 cwt. must be constructed of timber not less than ⅝-inch thick at top, bottom and sides, and ¾-inch ends, strengthened with battens as for 2-cwt. case.

116 Carboys packed to Specification No. 3A, cushioned with whiting, kieselguhr, asbestos, powdered china clay or sifted ashes or sponge rubber buffers, are authorised only for mixed nitric and sulphuric acid containing not over 17 per cent. nitric acid and containing at least 38 per cent. water.

117 Liquid oxygen may be carried in suitable insulated metal containers of sufficient strength to withstand without damage the extremly low temperature of the liquid.

Adequate relief arrangements must be provided to vent off any excess pressure arising from vapourisation of the liquid.

The containers must be adequately protected against shock or damage during transit and fitted with permanent lifting slings.

All containers must be labelled conspicuously with the name:—

"Liquid Oxygen." "Highly Inflammable."

"Not to be handled by Unauthorised Persons."

Before consigning liquid oxygen, application must be made to the Chief Traffic Manager so that arrangements can be made for the allocation of an appropriate vehicle or vehicles.

The consignor must provide facilities for loading and unloading the vehicles containing liquid oxygen, and details of the arrangements communicated to the Chief Traffic Manager.

Clause No.

118 Self-heating hair-waving materials must be packed in strong wooden cases. When offered for transport the activating liquid or substance must not be packed in the same package with the sachets.

119 Soap flakes must be thoroughly cooled and aired before being packed for transport, may be packed in cardboard packets in strong wooden cases to Specification No. 1B, fibreboard containers to Specification No. 6A or in strong hessian or jute sacks.

120 Acid sludge, sludge acid, spent sulphuric acid, or spent mixed acid resulting from the use of sulphuric acid in various processes, except when containing hydrofluoric acid, which will not corrode interior of tank trucks at sufficiently rapid rate to cause leakage during transportation, must be packed in specification containers as follows:—

(a) Wooden boxes to Specification No. 1A, with inside glass or earthenware containers not over 1 gallon capacity, except that glass or earthenware containers of not over 3 gallons capacity are authorised when one only is packed in each outside container, cushioned in kieselguhr, whiting, asbestos, sifted ashes, or powdered china clay.

(b) Boxed carboys to Specification No. 3A, cushioning material to be kieselguhr, whiting, asbestos, sifted ashes, powdered china clay, or sponge rubber buffers.

(c) Tank trucks to the approval of the Railway Commissioners, provided the product is sufficiently liquid to be unloaded through the dome.

121 Spirits of nitroglycerin, consisting of not over 5 per cent. by weight of nitroglycerin in ethyl alcohol. Wooden cases to Specification No. 1A, lined with paraffined paper, with inside metal containers, securely closed with rubber stoppers tied in place. The inside container must be entirely surrounded with at least 2 inches of dry, fine sawdust or kieselguhr. There must be not more than 6 quarts of the mixture in each outside container.

Spirits of nitroglycerin, consisting of not over 1 per cent. by weight of nitroglycerin in ethyl alcohol, may be packed in wooden boxes to Specification No. 1A, with glass inside containers not over 1 quart capacity each, securely stoppered and surrounded with at least 2 inches of dry, fine sawdust or kieselguhr.

122 Fifteen matches individually paper wrapped in a box. One dozen boxes in a paper packet. Ten gross boxes in a tin box. Three tin boxes in a strong wooden case to Specification No. 1A.

131 Containers for the storage and transport of compressed acetylene dissolved in acetone shall have been constructed and marked in accordance with Australian Standard Specification B.13 (see A.S.S. Code CB.4) for containers of this description.

No imported cylinders shall be used for the transport of this commodity unless they conform to the Specification requirements of Clause 2—2 and Table 4 of the Australian Standard Maintenance Rules, Part II, S.A.A. Code CB.4.

Such containers shall be maintained, tested and filled in accordance with the Australian Standard Rules for the Maintenance, Testing and Filling of Containers for the Storage and Transport of Compressed Acetylene dissolved in Acetone, S.A.A. Code CB.4, Part II.

In addition, the cylinders, their construction, marking, maintenance, filling and testing, etc., shall satisfy the Statutory Regulations which may be in force in any State in which the cylinders may require to be transported, and which are designed to ensure the safe handling of this gas when under pressure.

132 Seamless steel cylinders designed and manufactured for the storage and transport of permanent gases to the following standard specifications:—

(a) S.A.A. Specification B.10—High carbon steel cylinders for the storage and transport of permanent gases—see S.A.A. Code CB.4.

(b) Cylinders manufactured in accordance with the First Report of the British Gas Cylinders Research Committee published in 1921 (cylinders branded GCRC.21).

(c) B.S.S.399—"High carbon" steel cylinders for the storage and transport of permanent gases.

(d) B.S.S.400—"Low carbon" steel cylinders for the storage and transport of permanent gases.

(e) British and Australian Standard Cylinders as designed for high pressure liquefiable gases (see Packing Clause 136).

Clause No.

133 Manganese steel gas cylinders (for atmospheric gases) to B.S.S. 1045-1942 (carbon .40 per cent. maximum, manganese 1.30 to 1.70 per cent.).

134 Approved for use but not for manufacture in Australia. Cylinders manufactured to specifications of the Interstate Commerce Commission, U.S.A. Procedure in respect to inspection, testing, records, etc., during original manufacture shall be in accordance with that in force in the U.S.A. at the time of manufacture, and cylinders shall have been made to the satisfaction of the Bureau of Explosives, U.S.A. Specifications and service pressures shall be in accordance with those given in Tables 1 and 2 of the Australian Standard Rules S.A.A. Code CB.4 for the appropriate gas.

135 S.A.A. Specification B.15—Welded steel cylinders for the storage and transport of low pressure liquefiable gases—see S.A.A. Code CB.4. N.B.—New specifications not to be used until formally approved and issued by Standards Association of Australia, and any list of gases which may be carried in same confirmed.

136 Seamless steel cylinders for carriage of high pressure liquefiable gases which are in a liquid condition when charged into cylinders at a pressure not exceeding 1,800 lb./sq. in. to specifications as follows:—

(a) Australian Standard Specification B.11—High carbon steel cylinders for the storage and transport of high pressure liquefiable gases (see S.A.A. Code CB.4 for Compressed Gas Cylinders).

(b) B.S.S. 401-1931—Steel cylinders for storage and transport of liquefiable gases designed for a maximum working pressure of 1,800 lb./sq. in., calculated by the formula given for pressures above 1,500 lb./sq. in.

(c) B.S.S. 1287-1946—High carbon steel gas cylinders for carbon dioxide, nitrous oxide and ethylene.

(d) B.S.S. 1288-1946—Manganese steel gas cylinders for carbon dioxide, nitrous oxide and ethylene.

137 Seamless steel cylinders for medium and low pressure liquefiable gases to specification as follows:—

(a) Australian Standard Specification B.12-1931—Cylinders for storage and transport of anhydrous ammonia, chlorine and sulphur dioxide. See S.A.A. Code CB 4.

(b) British Standard Specification 401-1931—Steel cylinders for the storage and transport of liquefiable gas. Cylinders for any particular gas shall be designed for a pressure at least equal to the maximum vapour pressure of the gas at an assumed working temperature of 150° Fahrenheit, using the formula applicable to the pressures on which the design is calculated.

138 Large gas-welded drums for the transport of chlorine and sulphur dioxide, maximum capacity of the drums to be 20 cwt., designed for secure attachment to railway (flat top) trucks, but removable for filling or emptying at the works of either consignor or consignee.

The following specifications for the drums will be acceptable:—

(a) Welded containers (drums) which comply in every respect with all the recommendations contained in the Report of the British Welded Containers Committee issued in 1930.

(b) Association of American Railroads Specification for Tank Trucks having forged lap-welded steel tanks Class I.C.C. 106 A. 500.

The drums shall be tested and maintained in accordance with the requirements of the relevant specification. The following filling ratios shall be observed for gases carried in such welded drums:—

Chlorine	1.16
Sulphur dioxide	1.16

The appropriate railway authorities should be consulted in respect to the selection or approval of a suitable truck or trucks, the arrangement and construction of the supports and anchoring arrangements to secure drum to truck when ready for transport. Particular attention should be paid to the anchoring being duly secured before the vehicle is offered as ready for transport. Inquiries should also be made as to the lifting facilities available at loading, unloading and transhipping stations or sidings in respect of their ability to handle drums.

Clause No.

139 Highly **poisonous gases** will only be accepted for transport in cylinders approved by the Standards Association of Australia, for the particular gas as specified in the Standards Association of Australia Safety Code CB.4, and providing the following supplementary conditions are observed:—

(1) A Special Authority referred to in Part III, Section 1, must be obtained from the Chief Traffic Manager, authorising the carriage of highly poisonous gas by rail.

(2) The valve must be adequately protected as provided in Clauses 1-6 (Protection of Valves (a) Poisonous Gases) of the Standard Rules, Standards Association of Australia Safety Code CB.4 for Compressed Gases.

(3) Each valve opening must be fitted with a plug or cap with a suitable gasket to make it gas tight.

(4) The valve must show absolutely no leakage when tested as specified in the Standard Rules, Standards Association of Australia Safety Code CB.4 for Compressed Gases.

(5) Hydrocyanic acid, anhydrous, must be effectively stabilised by the addition of a suitable quantity of efficient stabiliser, which has not been stored in the cylinder for more than nine months when tendered for conveyance. (A tag must be attached to the cylinder showing the filling date.) This does not apply to "returning empty" cylinders.

A signed declaration certifying that all these supplementary conditions have been met shall accompany the Application for a Permit to ship highly poisonous gas.

150 Departmental steel containers. Sheets of plywood or fibreboard supported on 1½ inch thick timber to be placed between each tier of accumulators, the timber being placed so as to protect the pole pieces and filling caps. Lengths of 4 inch x 2 inch timber to be placed across the door opening and used to partition off any portion of a tier on the top layer.

Any spaces left between the accumulators to be filled with fibreboard or timber to eliminate movement within the steel container.

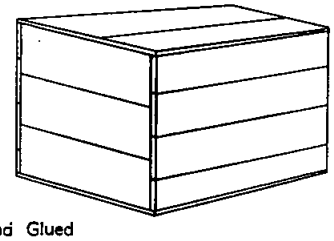
Any spilt acid to be suitably neutralized and the steel container thoroughly cleaned out immediately the accumulators are removed.

90

(B) SPECIFICATIONS FOR CONTAINERS FOR GOODS OF A DANGEROUS NATURE

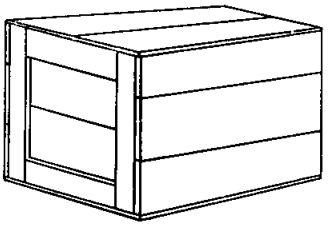
TYPES OF NAILED WOODEN CASES OR BOXES.

Equivalent one piece end case—
100 lb. gross weight.

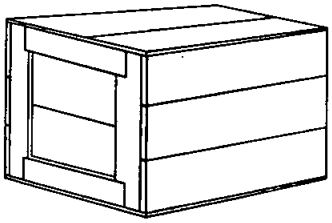


Tongued and Grooved and Glued
or Linderman Jointed and Glued.

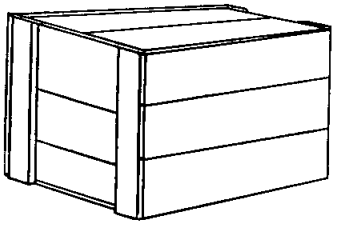
Cleated end Case.



Cleated end Case.



Cleated end Case.



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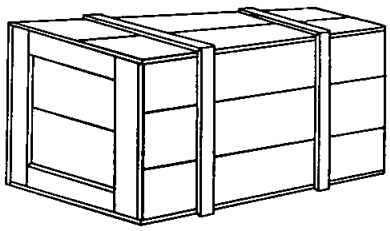

Strengthening Battens

SPECIFICATION No. 1A.

Nailed Wooden Cases or Boxes, close jointed, for use with cushioning material.

(A) MATERIAL.—

The dimensions described herein apply only to cases constructed of timbers considered best quality for packing case construction.

Timber must be well seasoned and dry, and free from knots with a diameter over one-quarter the width of the piece.

The outside surface of the timber shall be sufficiently smooth to permit legible marking.

(B) CONSTRUCTION.—

Ends: One piece or equivalent; parts at least 2½ inches wide; Linderman jointed and glued with two or more corrugated fasteners on each joint.

OR

Tongued and grooved and glued with two or more corrugated fasteners on each joint.

OR

Cleated construction (two vertical and two horizontal); parts to be close fitting along whole length of joint; width of cleats to be equal to twice the prescribed thickness plus ¾ inch; thickness of cleats to be the same as thickness of end parts.

MINIMUM THICKNESS OF END PARTS FOR ONE PIECE OR EQUIVALENT TYPES (NOT CLEATED).

Gross Weight	Smallest dimension of end.						
	Not over 8 ins.	Not over 9 ins.	Not over 10 ins.	Not over 11 ins.	Not over 12 ins.	Not over 14 ins.	Over 14 ins.
lb.	ins.	ins.	ins.	ins.	ins.	ins.	ins.
25	½	½	½	½	½	½	½
50	¾	⅝	⅝	9/16	⅝	½	½
75	⅞	⅞	¾	¾	11/16	⅝	9/16
100	1	15/16	⅞	⅞	13/16	¾	11/16

Cases over 100 lb. gross weight to be of cleated construction.

MINIMUM THICKNESS OF END PARTS (CLEATED TYPE).

Gross Weight	Smallest dimension of end.						
	Not over 8 ins.	Not over 9 ins.	Not over 10 ins.	Not over 11 ins.	Not over 12 ins.	Not over 14 ins.	Over 14 ins.
lb.	ins.	ins.	ins.	ins.	ins.	ins.	ins.
25	7/16	7/16	7/16	3/8	3/8	3/8	3/8
50	9/16	1/2	1/2	1/2	1/2	1/2	7/16
75	5/8	5/8	5/8	9/16	1/2	1/2	9/16
100	3/4	11/16	11/16	5/8	5/8	9/16	1/2
125	13/16	3/4	3/4	11/16	11/16	5/8	9/16
150	15/16	7/8	13/16	3/4	3/4	11/16	5/8
175	1	15/16	7/8	13/16	13/16	3/4	11/16
200	1 1/16	1	15/16	15/16	7/8	3/4	11/16
250	1 1/8	1 1/16	1	13/16	15/16	7/8	3/4
300	1 5/16	1 3/16	1 1/8	1 1/16	1	15/16	7/8
350	..	1 5/16	1 1/4	1 3/16	1 1/8	1	15/16
400	..	..	..	..	1 1/4	1 1/8	1
500	..	..	..	..	..	1 1/4	1 1/8

Sides, Tops and Bottoms: One piece or equivalent; parts at least 2½ inches wide; parts to be close fitting.

OR

If timber is over ½ inch thick, tongued and grooved and glued, with two or more corrugated fastenings on each joint.

OR

Linderman jointed and glued with two or more corrugated fastenings on each joint.

MINIMUM THICKNESS OF SIDES.

Gross Weight	Depth of Case or Box.						
	Not over 8 ins.	Not over 9 ins.	Not over 10 ins.	Not over 11 ins.	Not over 12 ins.	Not over 14 ins.	Over 14 ins.
lb.	ins.	ins.	ins.	ins.	ins.	ins.	ins.
25	5/16	5/16	5/16	5/16	5/16	5/16	5/16
50	11/32	11/32	11/32	11/32	11/32	5/16	5/16
75	7/16	3/8	3/8	11/32	11/32	11/32	11/32
100	1/2	7/16	7/16	3/8	3/8	3/8	11/32
125	9/16	1/2	1/2	7/16	7/16	3/8	3/8
150	5/8	9/16	9/16	1/2	7/16	7/16	3/8
175	5/8	5/8	9/16	1/2	1/2	7/16	7/16
200	11/16	5/8	5/8	9/16	9/16	1/2	7/16
250	45/32	3/4	11/16	5/8	5/8	9/16	1/2
300	13/16	13/16	3/4	3/4	11/16	5/8	9/16
350	15/16	7/8	13/16	13/16	3/4	11/16	5/8
400	1	15/16	7/8	13/16	13/16	3/4	11/16
500	1 1/8	1 1/16	1	15/16	7/8	7/8	3/4

MINIMUM THICKNESS OF TOP AND BOTTOM.

Gross Weight	Width of Case or Box.						
	Not over 8 ins.	Not over 9 ins.	Not over 10 ins.	Not over 11 ins.	Not over 12 ins.	Not over 14 ins.	Over 14 ins.
lb.	ins.	ins.	ins.	ins.	ins.	ins.	ins.
25	5/16	5/16	5/16	5/16	5/16	5/16	9/16
50	11/32	11/32	11/32	11/32	11/32	5/16	5/16
75	7/16	3/8	3/8	11/32	11/32	11/32	11/32
100	1/2	7/16	7/16	3/8	3/8	3/8	11/32
125	9/16	1/2	1/2	7/16	7/16	3/8	3/8
150	5/8	9/16	9/16	1/2	7/16	7/16	3/8
175	5/8	5/8	9/16	1/2	1/2	7/16	7/16
200	11/16	5/8	5/8	9/16	9/16	1/2	7/16
250	23/32	3/4	11/16	5/8	5/8	9/16	1/2
300	13/16	13/16	3/4	3/4	11/16	5/8	9/16
350	15/16	7/8	13/16	13/16	3/4	11/16	5/8
400	1	15/16	7/8	13/16	13/16	3/4	11/16
500	1 1/8	1 1/16	1	15/16	7/8	7/8	3/4

Sides, tops and bottoms to be nailed into cleats on cleated end type.

Nailing tops and bottoms to sides is permitted.

Cases are to be bound with hoop-iron or wire, or battened.

(C) TESTING.

All cases when packed should withstand a drop of one foot on to each corner, on solid concrete, without damage to contents, distortion to the case or loss of packing material.

(D) BATTENS.—

Dividing the interior of the case into separate compartments by wooden partitions to separate containers (such as winchester bottles) in recommended as an added precaution against contact between containers.

SPECIFICATION No. 1B.

Nailed Wooden Cases or Boxes for tin containers without cushioning material.

(A) MATERIAL.—

The dimensions described herein apply only to cases constructed of timbers considered best quality for packing case construction.

Timber must be well seasoned and dry, and free from knots with a diameter over one-quarter the width of the piece.

The outside surface of the timber shall be sufficiently smooth to permit legible marking.

(B) CONSTRUCTION.—

The internal dimensions of the case must be such that the tin or tins are a close fit and unable to move about within the case after closing.

Ends: One piece or equivalent; parts at least 2½ inches wide.

Jointing for equivalent one-piece construction to be adequate to withstand tests.

Cases over 100 lb. gross weight to be cleated construction as for Specification No. 1A.

Minimum thickness of end parts as for Specification No. 1A.

Sides, Tops and Bottoms: Parts to be at least 2½ inches wide; parts to be close fitting.

MINIMUM THICKNESS OF SIDES.

Gross Weight	Depth of Case or Box.						
	Not over 8 ins.	Not over 9 ins.	Not over 10 ins.	Not over 11 ins.	Not over 12 ins.	Not over 14 ins.	Over 14 ins.
lb.	ins.	ins.	ins.	ins.	ins.	ins.	ins.
25	1/4	1/4	1/4	1/4	1/4	1/4	1/4
50	11/32	5/16	5/16	5/16	9/32	1/4	1/4
75	7/16	3/8	3/8	11/32	11/32	5/16	9/32
100	1/2	7/16	7/16	3/8	3/8	3/8	11/32
125	9/16	1/2	1/2	7/16	3/8	3/8	3/8
150	5/8	9/16	9/16	1/2	7/16	7/16	3/8
175	5/8	5/8	9/16	1/2	1/2	7/16	7/16
200	11/16	5/8	5/8	9/16	9/16	1/2	7/16
250	25/32	3/4	11/16	5/8	5/8	9/16	1/2
300	13/16	13/16	3/4	3/4	11/16	5/8	9/16
350	13/16	7/8	13/16	13/16	3/4	11/16	5/8
400	1	15/16	7/8	13/16	13/16	3/4	11/16
500	1 1/8	1 1/16	1	15/16	7/8	7/8	3/4

MINIMUM THICKNESS OF TOP AND BOTTOM.

Gross Weight	Width of Case or Box.						
	Not over 8 ins.	Not over 9 ins.	Not over 10 ins.	Not over 11 ins.	Not over 12 ins.	Not over 14 ins.	Over 14 ins.
lb.	ins.	ins.	ins.	ins.	ins.	ins.	ins.
25	1/4	1/4	1/4	1/4	1/4	1/4	1/4
50	11/32	5/16	5/16	5/16	9/32	1/4	1/4
75	7/16	3/8	3/8	11/32	11/32	5/16	9/32
100	1/2	7/16	7/16	3/8	3/8	3/8	11/32
125	9/16	1/2	1/2	7/16	3/8	3/8	3/8
150	5/8	9/16	9/16	1/2	7/16	7/16	3/8
175	5/8	5/8	9/16	1/2	1/2	7/16	7/16
200	11/16	5/8	5/8	9/16	9/16	1/2	7/16
250	25/32	3/4	11/16	5/8	5/8	9/16	1/2
300	13/16	13/16	3/4	3/4	11/16	5/8	9/16
350	13/16	7/8	13/16	13/16	3/4	11/16	5/8
400	1	15/16	7/8	13/16	13/16	3/4	11/16
500	1 1/8	1 1/16	1	15/16	7/8	7/8	3/4

Sides, tops and bottoms to be nailed into cleats on cleated end type.

Nailing tops and bottoms to sides permitted.

Cases over 100 lb. gross weight are to be bound with hoop-iron or wire or battened.

(C) TESTING.—

All cases when packed should withstand a drop of one foot on to each corner, on solid concrete, without damage to contents, distortion to the case or loss of packing material.

SPECIFICATION No. 2A.

Returnable mild-steel drums for liquids.

Construction to be in accordance with British Standard Specification No. 1702-1950 in all details.

Drums must be either new or in a condition equal to new.

SPECIFICATION No. 2B.

Returnable mild-steel drums for liquids.

Construction to be in accordance with Australian Standard Specification, K.87, Type A or B, in all details.

Drums must be either new or in a condition equal to new.

MARKING.—

All drums shall be clearly marked as follows by embossing the bottom of the drum in type not less than 1 inch for 16 B.G. sheets and thicker, and not less than ½ inch for sheets thinner than 16 B.G.:—

(1) The manufacturer's name or mark	A.B. & Co.
(2) The gauge of the metal used (B.G.). The nominal capacity (Imperial gallons) and the year of manufacture .	14—44—47
(3) The number of the specification	A.S.S.—K.87, Type A

i.e., the above container is manufactured by A.B. & Co., having 14 B.G. body and ends, 44 Imperial gallons capacity, manufactured in 1947, complying with the requirements of A.S.S. No. K.87, Type A.

SPECIFICATION No. 2C.

Non-Returnable (single trip) 4 and 6-gallon black steel drums.

(A) MATERIAL—

Quality of material, method of manufacture and closure to comply with Australian Standard Specifications No. (E) K-501-1940 and (E) K.502-1940 (Emergency Series).

(B) THICKNESS OF MATERIAL—

The metal used in the drum shall be not less than 24 B.G. with a tolerance of 0.002 inch.

SPECIFICATION No. 2D.

Non-Returnable (single trip) mild-steel drums.

(A) MATERIAL—

As for Specification No. 2C.

(B) CLOSURE—

As for Specification No. 2C or with detachable head, the sealing of which must be positive and adequate to comply with tests prescribed for drums to Specification No. 2B.

(C) SEAMS—

Head and chime seam, double seamed or welded.

Body seam, welded or folded and grooved and leaded or soldered so as to comply with tests.

(D) DIMENSIONS AND PARTS—

Marked Capacity Not Over—	Minimum Thickness in the Black. Birmingham Gauge.		Rolling Hoops
	Body Sheet.	End Sheets.	
galls.			
6	24	24	None
10	22	22	None

(E) MARKING—

As for Specification No. 2B.

SPECIFICATION No. 2E.

Returnable mild-steel drums for liquids.

(A) MATERIAL AND CLOSURE—

As for Specification No. 2A.

(B) SEAMS—

All seams welded.

(C) CHIME REINFORCEMENT—

As for Specification No. 2A.

(D) DIMENSIONS AND PARTS—

Marked Capacity Not Over—	Minimum Thickness in the Black. Birmingham Gauge.		Rolling Hoops.
	Body Sheet.	End Sheets.	
galls.			
10	14	12	
20	12	10	Swedged or rolled in.
66	12	10	1½ ins. x 1 in. 1 bar 6 lb./yd.

(E) ROLLING BEADS—

Swedged or rolled into body of drum, shall not be less than ⅞ inch in depth.
Closures and drain plugs must not protrude beyond chime or rolling hoops.

(F) TESTING—

As for Specification No. 2B.

(G) MARKING—

As for Specification No. 2B.
Drums must be either new or in a condition equal to new.

SPECIFICATION No. 2F.

Mild-steel drums with lever lid or other suitable large aperture closure, for solids.

(A) MATERIAL—

As for Specification No. 2A.

(B) SEAMS—

Body seam welded or folded and grooved, ends double seamed and/or welded with or without end hoops.

(C) DIMENSIONS AND PARTS—

Diameter.	Height.	Non-Returnable.		Returnable.	
		Body, Birmingham, Gauge	Ends, Birmingham, Gauge	Body, Birmingham, Gauge	Ends, Birmingham, Gauge
Inches	Inches				
7	7 to 13	26	26	24	24
8	8 to 15	26	26	24	24
9	9 to 16	26	26	22	22
10	10 to 18	26	26	22	22
11	11 to 22	26	24	22	22
12	12 to 24	24	24	22	22
14	14 to 28	24	24	22	22
16	16 to 28	24	24	22	22
18	18 to 30	22	22	20	20
18½	18½ to 34	22	22	20	20
19½	19½ to 36	22	22	20	20
20	20 to 36	22	22	20	20
22	22 to 38	22	22	20	20
22½	22½ to 38	22	22	20	20
24	24 to 40	20	20	18	20

(D) TESTING—

All drums filled with finely powdered material to the equivalent of the weight to be transported shall withstand dropping a distance of 4 feet on to solid concrete so as to strike diagonally on chime or circumferential seam, or any part liable to show weakness, without rupturing or weakening.

Returnable drums must be either new or in a condition equal to new.

SPECIFICATION No. 2G.

Mild-steel drums with lever lid or other suitable large aperture closure, for solids.

(A) MATERIAL—

As for Specification No. 2A.

(B) SEAMS—

Body seam welded or folded and grooved, ends double seamed and/or welded with or without end hoops.

(C) DIMENSIONS AND PARTS—

Diameter.	Height.	Non-Returnable.		Returnable.	
		Body, Birmingham, Gauge	Ends, Birmingham, Gauge	Body, Birmingham, Gauge	Ends, Birmingham, Gauge
inches	inches				
10	10 to 18	22	20	20	20
11	9	24	24	–	—
12	12 to 24	22	20	20	20
14	14 to 28	20	20	20	20
16	16 to 28	20	20	20	20
18	18 to 30	20	20	18	18
20	20 to 36	20	20	18	18
22	22 to 38	20	20	18	18
24	24 to 40	20	18	18	16

(D) TESTING—

As for Specification No. 2F.

SPECIFICATION No. 2H.

Returnable lead-lined steel drums.

(A) MATERIAL AND CONSTRUCTION—

Must comply with Specification No. 2A.

(B) LINING—

Lead at least $\frac{3}{32}$ inch thick, completely bonded to the steel.

(C) CLOSURE—

Leak proof bung or plug screwed, not greater than $2\frac{1}{4}$ inches British Standard fine thread.

(D) TESTING—

As for Specification No. 2B.
Drums must be either new, or in a condition equal to new.

SPECIFICATION No. 2 "I."

Returnable aluminium-lined steel drums.

(A) MATERIAL AND CONSTRUCTION—

Must comply with Specification No. 2A.

(B) LINING—

Aluminium, 99 per cent. pure; thickness, 0.12 inch; all seams welded.
Lining must be good fit within outer container to prevent any undue movement.

(C) CLOSURE—

Leak proof screwed plug or cap, located for protection between rolling hoops. Not to be greater than $2\frac{1}{4}$ inches British Standard fine thread, fitted with an aluminium gasket not less than 0.10 inch thick.

(D) TESTING—

As for Specification No. 2B.
Drums must be either new, or in a condition equal to new.

SPECIFICATION No. 2J.

Returnable Aluminium Drums.

(A) MATERIAL—

Aluminium 99 per cent. pure or an aluminium base alloy of equivalent corrosion resisting and physical properties.

(B) SEAMS—

Welded, including attachments for closure, etc., circumferential seams at least 3 inches from chime. Chime seams not permitted.

(C) ROLLING HOOPS—

Must be firmly secured in place and not over 19 inches apart. Beading under hoops not allowed.

(D) CLOSURES—

Of screw thread type, not over 2¼ inches British Standard fine thread, with suitable gaskets.

Closing parts must not project beyond rolling hoops or chime.

(E) DIMENSIONS AND PARTS—

Capacity.	Minimum Thickness of Material.	Rolling Hoops "I" Section.
galls.	ins.	ins.
10	.110	1¼ x ¾
30	.154	1¼ x ¾
55	.187	1½ x 1
110	.230	1½ x 1

Drums must be either new, or in a condition equal to new.

SPECIFICATION No. 2K.

Returnable Stainless-steel Drums.

(A) MATERIAL—

All sheet metal, welding rod, closing devices and samples taken from the welded portion of the finished container must be nominal 18 chrome 8 nickel alloy with limits of 0.08 per cent. carbon maximum, 17-20 per cent. chromium, 7-11 per cent. nickel, or other approved types of stainless steel of equivalent corrosion resistance and physical properties. The steel must be capable of resisting the action of nitric acid as follows:—

The limit of inches per month penetration in accordance with corrosion test as used in American Society of Testing Materials Committee A.10, 1933 collaboration testing programme, shall be 0.0015 inch, this figure to be an average of five 48-hour tests.

(B) SEAMS—

All seams welded, including attachment for closure.

(C) ROLLING HOOPS—

Must be firmly secured in place, not over 19 inches apart, by shrinking or by a raised bead on either side of hoop. Beading under hoops not allowed. Rolling hoops to be fitted to all drums of 20 gallons capacity or over.

(D) DIMENSIONS AND PARTS—

Dimensions of materials and parts to be in accordance with Specification 2B.

(E) TESTING—

As for Specification No. 2B.

Drums must be either new, or in a condition equal to new.

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SPECIFICATION No. 3A.

Carboys in boxes.

(A) MANUFACTURE—

Glass Carboys.—To be thoroughly annealed, moulded with straight sides and upward curved bottom, and of sufficient strength to withstand all tests. The surface of the top must be smooth and even.

Earthenware, Clay or Stoneware Carboys.—To be made of acid proof material and of sufficient strength to withstand all tests. The surface of the lip must be smooth and even.

Closures.—Leak-proof stoppers tied down or screwed, with gaskets of ¼-inch asbestos rope or other resilient material equal in efficiency. Provided that where pressure is generated within the vessel, the closure shall be vented or sufficiently porous to vent off such pressure.

Outside Container.—

(a) Wooden box completely enclosing the body of the carboy, with four vertical corner posts, two cleats for shoes and two carrying cleats.

(b) Timber to be well seasoned, dry, and free from decay, loose knots or other defects which would lessen the strength of the case.

(c) One piece or equivalent construction for parts. Pieces linderman jointed and glued or tongued and grooved and glued. Bottom nailed to sides and ends and top by an efficient method. Efficient screwed construction acceptable.

(d) **Dimensions and Parts—**

Capacity Not Over—	Minimum Dimensions.		
	Thickness, Sides, Top, Bottom, Ends.	Vertical Corner Posts.	Carrying Cleats and Shoes.
galls.	ins.	ins.	ins.
Up to 6	$\frac{5}{8}$	$\frac{5}{8}$ x $2\frac{1}{2}$*	$\frac{5}{8}$ x $1\frac{7}{8}$*
Over 6 to 13 ..	$\frac{13}{16}$	$\frac{13}{16}$ x 3*	$\frac{13}{16}$ x $2\frac{3}{8}$*

* Other dimensions of equal cross sectional area acceptable.

(e) An efficient protective device or bridge must cover projecting neck of carboy, and be firmly secured to the top of the box, which shall be clearly labelled or marked: "THIS SIDE UP."

(B) PACKING MATERIAL—

Appropriate packing material, as indicated in table, must completely fill space between carboy and outer box, except where rubber cushioning pads are used.

(C) RE-USE OF PACKAGES—

Parts of outside container and cushioning must be replaced when broken, decayed, or inefficient in any way. Chipped or cracked lips on carboys not allowed.

(D) TESTING—

Each package, with carboy filled with water up to the lower edge of neck, shall withstand ten test blows on the side and five on the bottom when swung on a pendulum with a radius of 14 feet, a distance of 55 inches, against a solid concrete wall or block located at the lowest point of the swing.

Packages should be tested at regular intervals in order to check strength of the cases and efficiency of packing.

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SPECIFICATION No. 3B.

Metal-jacketed Lead Carboy.

(A) CAPACITY—

This specification refers to carboys with a maximum capacity of 15 gallons.

(B) CONSTRUCTION—

Inside Container—

Body.—Commercial sheet lead, weighing at least 8 lb. per square foot, rolled to shape, with edges lapped and burned inside and out to form side seam. Bottom and top edges must be flanged inwardly to form joints with bottom and top pieces.

Bottom.—Commercial sheet lead, weighing at least 8 lb. per square foot, joined to body by lapping and burning inside and out.

Top.—Commercial sheet lead, weighing at least 10 lb. per square foot, dished to shape, and joined to body by lapping and burning outside. Top must have circular hole in the centre for neck.

Neck.—Must be formed of commercial lead at least $\frac{3}{16}$ inch in thickness. Inside diameter 4 inches or less. Lower end must be flanged to fit top and joined thereto by burning outside. Neck must be of sufficient length to permit flanging over follower ring of closing device. If made of sheet lead rolled to shape, the side seam must be lapped and burned inside and out.

Closure.—To consist of a cast iron follower ring at least 3 inches larger in outside diameter than neck of carboy, bored to fit neck snugly and machined on top at right angles to neck hole. The top edge of the hole must have a $\frac{1}{4}$-inch radium approximately, and the bottom edge have the sharp corner removed.

The follower ring for carboys with up to a 2-inch diameter neck must have four $\frac{1}{2}$-inch studs, and over 2 inches diameter, six $\frac{1}{2}$-inch studs, arranged on a pitch circle $1\frac{1}{2}$ inches larger than the neck.

The neck must be flanged over the follower ring and be provided with clearance holes for the studs.

A gasket of commercial sheet lead weighing at least 8 lb. per square foot must be provided, and of a diameter equal to the outside diameter of the follower ring. Clearance holes to suit the stud arrangement are to be provided.

Closing cap of cast iron, of the same diameter as the follower ring, with both faces machined smooth and clearance holes for studs is to be provided. The thickness to be such that when the lid of the outer container, closing cap and gasket are studded together the locking or fastening device for the lid of the outer container is in its correct position.

Testing.—All lead carboys must be tested with 5 lb. per square inch internal pressure before they are consigned, without leakage.

Outside Container—

Body.—To be constructed of 14 Birmingham gauge mild steel sheet rolled to a cylindrical shape, body seam double seamed or welded or riveted.

Bottom.—To be 14 Birmingham gauge mild steel sheet. Double seamed, welded or riveted to the body.

Top.—To be 14 Birmingham gauge mild steel sheet, flanged to a depth of $1\frac{1}{4}$ inches, and of such diameter as to fit loosely into body, and held in place by six machined $\frac{3}{8}$-inch bolts, spaced equally around the circumference of the flange. An equal number of bayonet fastenings with efficient lock will be permitted.

Clearance holes for the studs of the closing device must also be provided.

Retaining Ring.—There must be a retaining ring of $1\frac{1}{4}$ in. x $\frac{3}{16}$ in. angle bolted to the inside of the container above the top edge of the body of the carboy to prevent up and down movement of the carboy.

SPECIFICATION No. 4A.

Earthenware Jars in Crates.

(A) CAPACITY—

Each jar not to exceed 3 gallons, and of each crate, two jars.

(B) MANUFACTURE—

To be made of acid-proof material. The surface of the lip must be smooth and even.

(C) CRATES—

(a) Timber shall be well seasoned, dry and free from knots detrimental to strength.

(b) Each side, end and bottom shall have at least three horizontal members and each corner a vertical member. The two-jar type shall have two compartments separated by a partition of similar dimensioned material.

(c) The nailed jointings shall be reinforced with hoop iron strapping. Efficient screwed construction allowed.

(d) **Dimensions and Parts—**

Minimum Dimensions of Parts.		
Horizontal End and Bottom Pieces.	Horizontal Side Pieces.	Vertical Pieces.
Inches. $2\frac{1}{2} \times \frac{5}{8}$	Inches. $2\frac{1}{2} \times \frac{1}{2}$	Inches. $2 \times \frac{3}{4}$*

* If two vertical pieces are used to form each corner, the dimensions may be 2 in. x $\frac{3}{8}$ in.

(e) Necks and stoppers must be protected by a closing batten and the top of the crate clearly labelled or marked "THIS SIDE UP."

(D) CLOSURES—

Leak-proof stoppers tied down or screwed, with gaskets of $\frac{1}{4}$-inch asbestos rope or other resilient material equal in efficiency. Provided that where pressure is generated within the vessel, the closure shall be vented or sufficiently porous to vent off such pressure.

SPECIFICATION No. 5A.

Wet Wooden Barrels.

(A) DEFINITION—

Wet Barrel.—Is a round, bulging, coopered wooden container made of staves, heads and hoops, and constructed to hold liquids without leaking.

(B) MATERIALS—

Staves and heads shall be constructed of the best quality barrel-making woods, well seasoned, free from knots, shakes, rotten sap, pitch pockets, seed, and worm holes in excess of fifteen in one container, or other defects.

Each piece of wood shall be quarter-sawn or cleft from straight-grained wood, so that no annular ring shall slope over half the thickness of the piece.

Hoops shall be of cooperage grade hoop steel or iron.

(C) CONSTRUCTION—

(a) **Staves.**—Shall be shaped evenly and circularly.

(b) **Heading.**—The heading shall be uniform in thickness, properly circled, planed or finely sawn on the outside, properly jointed and glued or dowelled and flagged. The diameter of the dowels shall not exceed five-twelfths of the thickness of the head.

(c) **Stave Joints.**—Shall be flush on the outside.

(d) **Dimensions and Parts—**

MINIMUM REQUIREMENTS.

Capacity	Diameter of Head	Diameter of Bilge	Height	Thickness of Wood for Heads and Staves	Steel Binding Hoops.		
					Number	Gauge	Width
Gallons	Inches	Inches	Inches	Inches			Inches
6	12	15¼	18	⅝	2 and 2	16 16	1½ 1¼
10	13½	16½	21½	⅝	4 and 2	16 16	1½ 1¼
12	14	18	23	⅝	4 and 2	16 16	1½ 1¼
20	17	22	26½	¾	4 and 2	16 16	1¾ 1½
29	18½	23	29½	¾	4 and 2	16 16	1¾ 1½
40	21½	25½	34	¾	4 and 2	16 16	1¾ 1½
117	24	36	57	1	6 and 4	16 16	1¾ 1½

(e) **Closure.**—Any type of closure that prevents leakage may be used. If bungs are used, bung holes in staves shall not exceed 2 inches in diameter.

(f) **Lining.**—The lining shall be appropriate for the contents.

(D) TESTING—

The sample container, at least two days old, shall not increase more than 10 per cent. on the diameter of the head when all hoops above the bilge are removed.

Each lined barrel shall withstand without leakage an internal pressure of 5 lb. per square inch air pressure. Leakers shall be repaired and retested in the same manner.

(E) RETURNABLE BARRELS—

This specification applies only to non-returnable or single trip containers.

Barrels of the returnable type are of special design and must be of dimensions and construction approved by the Railways Commissioners.

SPECIFICATION No. 5B.

Dry Wooden Casks.

(A) DEFINITION—

Dry Cask.—Is a round, bulging, coopered wooden container made of staves, heads, and hoops, and constructed to hold powders and commodities other than liquids.

(B) MATERIALS—

Staves and heads shall be constructed of the best quality cask making woods, well seasoned, free from knots, shakes, rotten sap, pitch pockets, seed, and worm holes in excess of fifteen in one container, or other defects.

Hoops shall be of cooperage grade hoop steel or iron.

(C) CONSTRUCTION—

(a) **Staves.**—Shall be shaped evenly and circularly.

(b) **Heading.**—The heading shall be uniform in thickness and properly circled.

(c) **Stave Joints.**—Shall be flush on the outside.

(d) **Dimensions and Parts—**

MINIMUM REQUIREMENTS.

Capacity	Diameter of Head	Diameter of Bilge	Height	Thickness of Wood for Head and Staves	Steel Binding Hoops.		
					Number	Gauge	Width
Cwt.	Inches	Inches	Inches	Inches			Inches
1	14	17	22	$\frac{7}{16}$	2 and 2	18 18	$1\frac{1}{4}$ 1
2	18	$21\frac{1}{2}$	28	$\frac{7}{16}$	4 and 2	18 18	$1\frac{1}{4}$ 1
3	21	25	32	$\frac{1}{2}$	2 and 4	18 18	$1\frac{1}{2}$ $1\frac{1}{4}$
4	22	26	36	$\frac{1}{2}$	2 and 4	16 16	$1\frac{3}{4}$ $1\frac{1}{2}$
5	22	27	42	$\frac{1}{2}$	2 and 4	16 16	$1\frac{3}{4}$ $1\frac{1}{2}$

(e) **Closure.**—Must comply with requirements of Specification No. 5A.

(f) **Lining.**—The lining shall be appropriate for the contents.

(D) RETURNABLE CASKS—

This specification applies only to non-returnable or single trip containers.

Casks of the returnable type must be of a design approved by the Railways Commissioners.

SPECIFICATION No. 6A.

Fibreboard Containers (Boxes).

1. **COMPLIANCE.**—Required in all details.

2. **DEFINITIONS—**

(a) Terms such as "200 pound test" mean minimum average bursting strength, Mullen or Cady test.

(b) "Joints" are edges of parts of boxes connected together in setting up boxes.

(c) "Seams" are visible edges of box, except joints, when box is closed.

INSIDE PACKING.

3. Liquids or solids in glass or earthenware must be packed with linings, partitions, wrappers, excelsior, straw or other material sufficient to afford adequate protection against breakage or damage. Partitions, if used, must be the full height of glass or earthenware containers.

Unauthorised packages are those with contents over 60 lb. weight, and those with any inside container over 5 pints capacity; for exception see para. 23.

4. Liquids in metal cans must be packed as necessary to provide adequate protection against breakage or damage.

Unauthorised packages are those with any inside container over one gallon capacity or with contents over 60 lb. weight; for exception see paragraph 23.

5. Solids in inside containers other than glass or earthenware must be packed as necessary to afford adequate protection against breakage or damage.

6. Mixed consignments (inside containers of different sizes, or solids and liquids in same outside container) must be packed as prescribed for articles in glass or earthenware (paragraph 3).

MATERIALS.

7. Classification of board.—Fibreboard is hereby classified by strength of completed board as in the first column of the following table, the strength specified being the minimum average bursting strength. The thickness specified is the nominal value.

Classified Strength of Completed Board	Solid Fibreboard Thickness (inches)	Corrugated Fibreboard—Thickness of Facings (inches).				
		Double-walled Single Corrugation		Triple-walled Double Corrugation		
		Outer Facing	Inner Facing	Outer Facing	Centre Facing	Inner Facing
175	0.080	0.016	0.014	—	—	—
200	0.080	0.016	0.016	0.016	0.014	0.016
275	0.095	—	—	0.016	0.014	0.016
350	0.115	—	—	—	—	—

8. Solid Fibreboard.—To be composed of three or more plies of basic fibreboard of good bending quality with all plies firmly glued together and the inner and outer facings water resistant to the degree specified in paragraph 12 (e). Each ply must be at least 0.014 inches nominal thickness.

9. Corrugated Fibreboard.—Both the inner and outer facings shall be water resistant to the degree specified in paragraph 12 (e); corrugating paper to be of minimum nominal substance 24 lb./1000 sq. ft., or nominal thickness 0.009 inches; all parts to be firmly glued together throughout all contact areas.

10. Stitching Staples of suitable flat wire at least $\frac{3}{32}$ inch x 0.019 inch or equal cross section, formed into staples about $\frac{7}{16}$ inch wide.

11. Tape—Cloth tape, of strength across the woof at least 70 units Elmendorf test, and coated with suitable adhesive. Sisal tape of one sheet of kraft paper and one sheet of crepe kraft paper, total weight 73 lb. per ream (480 sheets, 24 inches x 36 inches). These sheets are combined with bitumen (asphalt) and reinforced with unspun sisal fibres completely embedded in the bitumen (asphalt) and, extending across the tape the kraft paper side of the tape is coated with suitable adhesive.

TESTING OF FIBREBOARD.

12. (a) Tests to be made on samples taken from materials in good condition.

(b) All samples shall be fully conditioned and tested in an atmosphere maintained at 20° C. and 65 ± 2% relative humidity.

(c) The thickness of solid fibreboard and of the facings of corrugated fibreboard shall be determined by measuring the thickness of each of ten samples of the material and taking an average of the readings obtained. A dial type micrometer, having a circular foot between 0.56 inch and 0.65 inch in diameter, and a pressure of 7 to 9 lb. per sq. inch, shall be used for this purpose. A manufacturing tolerance on thickness of minus 5% shall be permitted.

(d) The bursting strength tests shall be carried out in the manner described in Australian Stndards Specification N.21. Six tests shall be made, three from each side of the board. If the average of these six tests falls below the minimum specified, twenty-four tests shall be made, twelve from each side of the board. If the average of these tests does not fall below the specified minimum strength, the board shall be considered to pass the test. All two-pop tests shall be considered void.

(e) The outer and inner facings of the container shall be water-resistant so that the water absorption (Cobb/30 min.), when determined by the method described in Australian Standard Specification N.21 shall not exceed 150 grammes per square metre.

CONSTRUCTION.

13. Types of boxes authorised.—To be of solid or corrugated fibreboard of the following types—slotted box, three-piece box without recessed ends, double-slide box, triple-slide box, telescope box, with sections of equal depth, or with covers, top or bottom or both, with 3 inches overlap.

14. Inside packing and weight limits—as prescribed in paragraphs 3 to 6 and 19.

15. Forming—Parts must be cut true to size and so creased and slotted as to fit closely into position without cracking, surface breaks, or separation of parts outside of crease.

16. Joints—as follows:—

(a) Stitched joint—For solid and corrugated fibreboard lapped 1½ inches, with maximum spacing between stitches of 2 inches, stitched within 1 inch of each end of joint.

(b) For corrugated fibreboard only the following joints are authorised:—

(i) Taped butt joint—2½ inch tape required for boxes over 30 lb. authorised gross weight and 2 inch tape for others.

(ii) Glued-laminated joint—constructed by the removal of corrugated sheet and centre facing, if one is used, and splicing inner and outer facings over the end of abutting corrugated board. The overlap must extend not less than 1 inch and both facings be securely glued.

(iii) Glued-lap joint—the sides of box forming joint must lap not less than 1¼ inches and be firmly glued together throughout entire area of contact.

(c) For double and triple-slide boxes—joints of all slides must be taped (paragraph 11) or stitched.

17. Flap closures—

(a) Flap pads between inner flaps are required when necessary to prevent an opening in the seam.

(b) If to be closed by adhesive, each inner flap must cover at least one-third of face; inner flaps must butt or flap pads must be used; outer flaps must butt or have full overlap.

(c) Single flap closures are authorised for boxes with one dimension not over 2 inches; each flap must be scored and form one of the small faces of the box and lap at least 5 inches on one of the largest faces.

(d) Seams which are to be stitched—Overlap, if any, required to be at least 1½ inches.

18. Linings (when prescribed in paragraph 19)—of two pieces to line completely the six sides of the interior of the box.

19. (a) Authorised gross weight of contents and specification for box and lining—as follows:—

Authorised Gross Weight of Contents (pounds)	Minimum Average Bursting Strength of Fibreboard			
	Solid Fibreboard Box	Double-walled Single Corrugation Box	Double-walled Single Corrugation Lining*	Triple-walled Double Corrugation Box
15	175	175		200
30	200	200		200
40	275	200	175	200
60	350	200	200	275

* As prescribed in paragraph 18. A complete box to the specification of the lining is acceptable in place of the lining.

(b) Triple-slide boxes authorised for gross weights as follows: of board at least 175 pound test for 40 pounds; of board at least 200 pound test for 60 pounds.

CLOSING FOR CONSIGNING.

20. (a) By coating with suitable adhesive the entire contact surfaces of closing flaps and flap pads or the slides of double or triple slide boxes, or the covers of telescope type boxes. For single flap closures, as authorised for boxes with one dimension not over 2 inches, the flaps must be fastened to the body with suitable adhesive.

(b) Or by stitching at 2½ inch intervals along all seams.

MARKING.

21. (a) On each container—"DANGER," "Name of contents," characteristic "INFLAMMABLE LIQUID," etc., and Specification No. 6A. Size of these markings at least ½ inch high.

(b) Name and address of plant making the container.

(c) Certificate showing the authorised gross weight of contents.

22. Special box—Safety Matches—authorised gross weight of contents 60 lbs. Solid or corrugated fibreboard box of strength not less than 200 lbs. test. Flap pads are not required.

23. Special box—authorised gross weight of contents 84 lbs.; authorised contents of inner containers not to exceed 1 gallon; must comply with this specification except as follows: Must be one-piece type, of triple-walled corrugated or solid fibreboard fitted with a lining to give a combined strength of not less than 400 pounds. Application must be made and written permission obtained for the use of this box.

SPECIFICATION No. 7A.

Wooden Casks—Glued Plywood.

(A) DEFINITION—

Plywood casks are cylindrical containers, the body is butt-jointed by means of a plywood strip on the inside, or a metal strip on the outside. The heads are made of single piece discs of plywood of three or more ply held in position by lining and closing hoops, and where necessary reinforced with crossbars.

(B) MATERIALS—

The plywood for the body shall be preferably three-ply, and for the heads, three or more ply.

The plywood and wood for the cross bars shall be best quality.

Knots in crossbars and bands shall not exceed one-third the width of the members at the point of occurrence nor shall they interfere with the nailing. Crossbars and bands shall not contain other defects which materially weaken them. Metal body joining strips shall be of terneplate, treated with rust preventative, or zinc.

(C) CONSTRUCTION—

(a) Plywood for the component parts shall be not less than the minimum thickness shown in the table for the weight of the contents packed.

(b) Crossbars shall not be less than 1⅞ inches wide and equal in thickness to the width of the closing hoop.

(c) **Body joining strip.**—Plywood not less than the thickness of the body and not less than 1¾ inches wide. It shall be fixed on the inside of the body and shall extend to the inner edge of each lining hoop.

OR

Metal not less than 30 B.G. in thickness and not less than 1¾ inches wide. The joining strip shall have beaded or folded edges and shall extend to within ¼ inch of the top and bottom edges of the body.

(d) Parts shall be efficiently fastened together with clout nails, wire staples, or bifurcated rivets in such a way that the finished cask is sift proof when filled with finely divided powder, and will remain so under all usual conditions of railway transportation. Casks may be rendered sift proof for fine powders by cotton packing fixed between the lining hoops and the heads at each end.

(D) DIMENSIONS AND PARTS—

Weight of Contents	Maximum Inside Diameter of Body	Minimum Thickness										Minimum Width of Hoops	
		Body	Heads	Body Bands				Hoops					
								Lining		Closing			
				No. of Bands	Plywood	Veneer	Width	Plywood	Veneer	Plywood	Veneer	Lining	Closing
cwt.	In.	In.	In.		In.	In.	In.	In.	In.	In.	In.	In.	In.
Up to 1	Up to 15	$\frac{1}{8}$	$\frac{3}{16}$	2	$\frac{3}{16}$	$\frac{3}{16}$	$1\frac{3}{4}$	$\frac{3}{16}$	$\frac{3}{16}$	$\frac{3}{16}$	$\frac{3}{16}$	$\frac{3}{4}$	$\frac{3}{4}$
Over 1 up to 2	Up to 18	$\frac{3}{16}$	$\frac{1}{4}$	2	$\frac{3}{16}$	$\frac{3}{16}$	2	$\frac{3}{16}$	$\frac{3}{16}$	$\frac{3}{16}$	$\frac{3}{16}$	$\frac{3}{4}$	$\frac{3}{4}$

SPECIFICATION No. 8A.

Rivetted or Lock-seamed Metal Cases, Fibre-board Lined.

In addition to the requirements of Packing Clause 109 (b), the cases shall conform to the following minimum requirements:—

Length of Film per Case	Minimum Gauge of Metal for Case	Number of Hinges	Number of Fastening Devices	Number of Carrying Handles
Not over 9,000 feet ..	20 B.G. steel	2	1	1
Not over 8,000 feet ..	22 B.G. steel	2	1	1
Not over 1 x 10in. reel	24 B.G. steel	1	1	1
Not over 500 feet ..	26 B.G. steel	1	1	1

SPECIFICATION No. 8B.

Welded or Rivetted Metal Cases, Fibreboard Lined.

In addition to the requirements of Packing Clause 109 (b), the cases shall conform to the following minimum requirements:—

Length of Film per Case	Minimum Gauge of Metal for Case	Number of Hinges	Number of Fastening Devices	Number of Carrying Handles
Not over 12,000 feet ..	18 B.G. steel or aluminium manganese alloy	2	2	2
Not over 9,000 feet ..	18 B.G. steel or aluminium manganese alloy	2	1	1
Not over 8,000 feet ..	20 B.G. steel or aluminium manganese alloy	2	1	1
Not over single reel ..	20 B.G. steel or aluminium manganese alloy	1	1	1

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Green Form with words "Owner's Risk" superimposed in red.

G.F.5.

VICTORIAN RAILWAYS.

CONSIGNMENT NOTE FOR EXPLOSIVES AND GOODS OF A DANGEROUS NATURE.

The Victorian Railways Commissioners hereby give notice that they are not common carriers of explosives or of goods of a dangerous nature, and that they do not undertake the carriage thereof except upon the terms of the following special contract:—

.....Station .....19...

To the Victorian Railways Commissioners

Please receive and forward the undermentioned goods to.....
(Block letters)

at.....Railway Station, subject to the Railways Acts and any Act
(Block letters)

relating to explosives or goods of a dangerous nature and the Regulations and By-Laws made under any of the foregoing Acts and upon the express conditions that the Commissioners shall be

(1) exempt from all liability of whatsoever nature howsoever arising in respect of loss of or damage to the explosives or goods of a dangerous nature and

(2) compensated by the sender for any loss or damage in any way caused by the said explosives or goods of a dangerous nature to the Commissioners' property and indemnified by the sender against any claim in respect of loss damage or injury in any way caused by the said explosives or goods of a dangerous nature to any person or the property of any person unless such loss damage or injury arises from wilful misconduct on the part of the Commissioners' servants.

I/We declare that all of the containers and packing of the goods described hereunder have been carefully examined and tested and certify them to be in accordance with the requirements of the foregoing Acts By-Laws and Regulaltions.

.....Signature of Consignor or Agent .....Address of Consignor or Agent

Truck No.	No. and type of packages, i.e., cases, drums, etc.	Description of Goods	Brands and Numbers	Weight				Freight Payable by—
				Tons	cwt.	qrs.	lbs.	

Loaded by { Consignor. / Commissioners. } How weight ascertained.....

.....
Signature of Railway Employe receiving Goods.

Time..... Date.....

By Authority—Victorian Railways Print.

VICTORIA

GOVERNMENT GAZETTE

Published by Authority.

[*Registered at the General Post Office, Melbourne, for transmission by post as a newspaper.*]

No. 928] WEDNESDAY, OCTOBER 6 [1954

VICTORIAN RAILWAYS

VICTORIAN RAILWAYS COMMISSIONERS

BY-LAW No. 375

GOODS RATES BOOK

Volume III

SHUNTING CHARGES

No. 928.—3266/54.—Price 4/9.

VICTORIAN RAILWAYS

GOODS RATES BOOK

VOLUME III

SHUNTING CHARGES

Applicable at the
Private Sidings, Stations and Sites
Specified.

To Take Effect On And From 1st December, 1954

The Commissioners hereby notify that the Shunting Charges shown herein are subject to alteration at any time.

To be read and construed with Goods Rates Book—
Volumes I and II.

All Shunting Charges issued prior to 1st December, 1954, are cancelled.

Published by the authority of the Government of the State of Victoria :
M. L. G. McKenzie
Manager
Victorian Railways Printing Works, North Melbourne

1954

VICTORIAN RAILWAYS

VICTORIAN RAILWAYS COMMISSIONERS

BY-LAW No. 375

The Victorian Railways Commissioners, in pursuance of the powers conferred in that behalf by the Railways Acts, do hereby make the following By-Law, and do hereby repeal all previous By-Laws, Conditions, Practices, Instructions, Classifications, Rates and Charges whatsoever as relate thereto or conflict therewith.

The several Rates and Charges which shall be imposed in respect of the carriage of Goods, Wool and Live Stock over the Victorian Railways, and the Classification and Conditions under which such Goods, Wool and Live Stock will be carried and the charges for other services shall be as set forth in Volumes I., II. and III. of this By-Law.

The provisions of this By-Law shall become effective as from 1st December, 1954.

In witness whereof the Common Seal of the Victorian Railways Commissioners was affixed hereto this 29th day of September in the year of our Lord One thousand nine hundred and fifty-four in the presence of—

(SEAL) R. G. Wishart, O. G. Meyer, E. H. Brownbill } Victorian Railways Commissioners

Confirmed by the Governor-in-Council

the fifth day of October, 1954.

A. Mahlstedt
Clerk of the Executive Council.

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Shunting and other charges applicable at the following private sidings, stations and sites.

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SHUNTING AND OTHER CHARGES APPLICABLE AT THE FOLLOWING PRIVATE SIDINGS, STATIONS, AND SITES.

The Sidings or Sites shown are open for consignments in truck loads (minimum 3 tons, empties, such as cases, crates drums, or tins 1½ tons), Out and In, a/c. Siding Holder, except where otherwise provided.

1. OUTWARDS.

The minimum tonnage (3 tons, empties 1½ tons), may be accepted for despatch outwards by one consignor when comprised of consignments of different consignees at the same or different destination stations.

Where the quantity loaded in a truck is less than 3 tons (empties 1½ tons), freight charges shall be computed at Tariff rates for the actual weight of the consignment concerned, plus the difference between the actual weight loaded and the 3 ton minimum (empties 1½ tons), at the lowest classification rate of any goods in the truck, as from the siding to the first station at which any part of the goods is or could be transferred.

A "paid" waybill for this difference shall be issued by the transfer station concerned, and the waybill accompanying the goods to destination endorsed with the date and waybill number of such "paid" waybill.

Sidings at Suburban Stations within 20 miles of Melbourne are not open for the despatch of consignments in less than full truckloads unless otherwise authorised by Chief Traffic Manager.

2. INWARDS.

Where consignments weighing less than 3 tons (empties 1½ tons), are received **inwards**, the freight charges shall be computed on the actual weight to the nearest station to which general goods can be forwarded, plus freight at the lowest classification rate of any goods in the truck for the distance from such nearest station to the destination siding, minimum 3 tons (empties 1½ tons).

3. Debit for Shunting Charges shall be raised by Supervising Station in the Miscellaneous Debit Book, and particulars included on Form T.R. 95. Haulage charges at a rate per ton must be shown on waybills as a separate entry to freight.

4. The conditions under which goods may be forwarded to or from private sidings at Commissioners' or at Owner's Risk, are shown in the General Conditions for the carriage of goods.

5. Where sidings are situate on Narrow-gauge lines, the prescribed charges for transfer (see Volume 1, page 176), shall be imposed.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
ALBION–BROADMEADOWS LINE— *Albistore (Dept. of Army Siding)* ...	‡	—	—	—	—	‡ 1. Freight charges shall be computed on the mileage to or from the siding situated 3½ miles from Albion and 6¼ miles from Broadmeadows.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—*continued.*

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
ALBION—BROADMEADOWS LINE—*continued.*						2. The charges shewn in footnote (a) shall apply, time charged for to be computed : (i) When placing is done by a passing goods train :—from when the engine arrives at the main line points giving access to the siding until its release therefrom ; (ii) When placing is done by a Pilot engine from Melbourne, Tottenham or Broadmeadows :—from when the engine leaves its originating place, Melbourne, Tottenham or Broadmeadows, until its return thereto ; (iii) If a special train with a minimum load of 15 four-wheeled trucks (or the equivalent thereof) is run from Melbourne or Tottenham to the siding, no shunting charges shall be imposed, but if the load is less than 15 four-wheeled trucks (or the equivalent thereof) the charges shewn in footnote (a) shall be imposed, time to be computed from when the engine leaves the Melbourne Yards or Tottenham, as the case may be, until its return thereto.
ALEXANDRA—						
Rubicon Tramway Co.	102	—	—	—	—	
Utah Construction Ltd's. Siding ...	102	—	—	—	—	In addition to the freight charges, the charge shewn in footnote (a) shall be imposed for each placing of trucks on the siding ; time to be computed from when the engine commences the service until its return to the originating point.
S. Howard's Storage Site	102	6 0	12 0	12 0	24 0	

ALLENDALE—						
Bluebonnett Straw Envelope Pty. Ltd's. Chaff Mill Site	90¼	6 0	12 0	12 0	24 0	
ALPHINGTON—						
Adams and Co's. Siding ...	6¾	6 0	12 0	12 0	24 0	
ANDERSON—						
G. Dance's Siding, a/c Kilcunda Coal Mining Co., or Victorian Coal Mining Co.	78¾	6 0	12 0	*24 0	*48 0	* To be apportioned *pro rata* between the companies.
ARARAT—						
Shell Co. of Australia's Siding ...	‡	—	—	—	—	‡ The mileage shall be computed to or from Ararat plus 1 mile. The charges shewn in footnote (a) shall be imposed for placing or removing trucks; time shall be computed from when the engine leaves Ararat Yard until its return thereto. The Siding shall be operated during the hours of daylight only, and at times during such hours as are suitable to the Commissioners.
Vacuum Oil Co's. Siding ...	131	—	—	—	—	See footnote (a)—time shall be computed from when the engine leaves Ararat yard until its return thereto.
Ararat Borough's Oil Discharging Site	131	24 0	—	—	—	Vehicles for discharge shall be placed at the site only on days and at times convenient to the Commissioners.
ARDEN-STREET—						
Australian Portland Cement Pty. Ltd. Shed a/c Brown and Roeger	1	6 0	12 0	12 0	—	Applicable only when trucks are at firm's request specially sorted and placed opposite the portion of shed leased by the Company or Brown and Roeger.
Brunton, T., and Co's. Siding ...	1	—	—	—	—	
Emoleum (Australia) Ltd. Site ...	1	6 0	12 0	12 0	24 0	
Flax Production Committee Shed Site ...	1	6 0	12 0	12 0	24 0	
McWilliams Wines Pty. Ltd., wine discharging site	1	6 0	12 0	12 0	24 0	
Pannifex and Co's. lease of Departmental Shed	1	6 0	12 0	12 0	24 0	
Poultrymen and Farmers Trading Co. ...	1	6 0	12 0	12 0	24 0	
Silk Bros. Pty. Ltd. Storage Shed ...	1	6 0	12 0	12 0	24 0	
Strahan and Davies, Site ...	1	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
AXEDALE—						
Ingham's Siding a/c H. W. Snell ...	86½	6 0	12 0	24 0	48 0	
E. F. McKenzie's Loading Ramp ...	89½	6 0	12 0	12 0	—	
BACCHUS MARSH—						
Maddingley Brown Coal Pty. Ltd's. Siding	31¾	6 0	12 0	12 0	—	
Defence Dept's. Shed Sites ...	31¾	6 0	12 0	12 0	24 0	
BAIRNSDALE—						
Dry Kiln Seasoning Co. Pty. Ltd. Wharf Line Site	171½	6 0	12 0	—	—	Trucks shall be placed or removed from the siding only at such times as the engine is required to work on the Wharf line. Only one trip daily will be made for placing or removing trucks at the shunting charges specified. If a special trip be necessary for the purpose of placing or removing trucks, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves Bairnsdale Yard until its return thereto.
Dry Kiln a/c Permewan Wright Ltd. ...	171½	6 0	12 0	—	—	
Shell Co. of Australia and Vacuum Oil Co's. Siding	170¾	6 0	12 0	12 0	24 0	If a special trip be necessary between Bairnsdale and the siding for the purpose of placing or removing trucks, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves Bairnsdale Yard until its return thereto. When trucks for both companies are placed at the one time, the charges shall be debited to each company proportionately.
Postal Department's Storage Site ...	171¼	6 0	12 0	12 0	24 0	
Dry Kiln Seasoning Co. Pty. Ltd. Storage Site in Station Yard	171¼	6 0	12 0	12 0	24 0	

BALLARAT—						
Victorian Inland Meat Authority ...	‡	—	—	—	—	‡ The freight charges on goods and live stock shall be computed on the mileage to or from Ballarat, plus 6 miles additional between the siding and Ballarat. In addition to the ordinary freight, the following haulage charges for goods traffic shall be imposed :— 1. When a special service is provided between Ballarat and the siding, a charge of 123s. 4d. per trip shall be made. 2. When traffic is worked in conjunction with the traffic to the Cattle Yards, a charge of 6s. 0d. per 4-wheeled truck and 12s. 0d. per bogie truck, shall be made for placing trucks in the Company's private siding, minimum charge as for three trucks, and maximum charge as for six trucks.
" " " " *a/c Egg and Egg Pulp Marketing Board*						
Ballarat Cattle Siding	‡	—	—	—	—	Open for Live Stock In and Out. ‡ Freight charges shall be computed on the mileage to or from Ballarat, plus 6 miles. If a special pilot service from Ballarat is required on days other than Mondays or Tuesdays, no charge shall be made if 6 or more trucks of stock are placed or cleared. If less than 6 trucks are placed or cleared, a charge of 123s. 4d. for the special pilot service shall be imposed in addition to freight.
Supply and Development Dept's. Siding (Flax Production Committee)	‡	6 0	12 0	17 11	35 10	1. ‡ Freight charges shall be computed on the mileage to or from Ballarat, plus 3¼ miles. 2. These shunting charges apply when trucks are placed on the siding in conjunction with traffic to or from the Ballarat Cattle Yards or the Freezing Works. 3. When a special service by a pilot engine from Ballarat is required to place trucks on, and/or, lift trucks from the Siding, a charge of £6 3s. 4d. per trip shall be made.
Redan	‡	—	—	—	—	1. ‡ Freight charges shall be computed on the mileage to or from Ballarat station, plus six (6) miles. 2. Redan shall be open for traffic in truck loads consigned to or by :— Any Commonwealth Government Department, any State Government Department, Imperial Chemical Industries of Australia and New Zealand Ltd., Ballarat Paper Mills Pty. Ltd., Simmonds Aerocessories Pty. Ltd.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
BALLARAT—*continued.* *Redan (contd.)*						3. In addition to the freight charges the following shunting charges shall be imposed :— (i) If placing be done by ordinary pilot, the charges shewn in footnote (a) shall be imposed for each placing of trucks on the siding ; time to be computed from when the engine enters the siding from the Cattle Yards line until its return to the same point. (ii) If placing or clearing be done by special pilot trip from Ballarat or North Ballarat, the charge shown in (i) shall be imposed for the time the engine is engaged on each trip to or from the siding ; time to be computed from when the engine commences the service until its return to the originating point. (iii) When trucks are placed or cleared at the one time for more than one siding user, the charges indicated in (i) or (ii) shall be borne proportionately by each in accordance with the number of trucks placed or cleared for each siding user during such shunting operations. (iv) Should the engine be detained at the siding to meet the special requirements of any siding user, the charge shown in (i) for the time the engine is detained shall be imposed against the siding user concerned, but such detention time shall not be included or apportioned to the other siding users served at the time.
Bunge (Aust.) Pty. Ltd. Siding ...	‡					1. The siding shall be operated at a time suitable to the Department.
" " " " " a/c *Martin's Stoneware Pipe Ltd.*	‡					2. ‡ Freight shall be computed on mileage to or from Ballarat, plus ¾ mile.

		6 0	12 0	24 0	48 0	3. Shunting charges :— (i) Applicable when the sidings are worked by the regular Doveton-street Wood Pilot : minimum and maximum charges shall be apportioned pro rata between the siding holders. (ii) When a special trip is run at the request of siding holder to place or clear trucks, the charge shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves the Ballarat Yard until its return thereto.
Shell Co. of Australia and Vacuum Oil Co's. Siding	73¾	6 0	12 0	12 0	—	When the companies desire the loaded trucks to be placed, or empty trucks to be cleared by special service, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves the Ballarat Yard until its return thereto.
Selkirk's Siding	75	—	—	—	—	A charge of 20s. 8d. shall be imposed for each trip made by an engine to place trucks, whether loaded or empty, on the siding.
Selkirk's Siding—						
a/c Ronaldson Bros. and Tippett ...	75	6 0	12 0	24 0	48 0	
a/c Bungaree Shire Council ...	75	6 0	12 0	24 0	48 0	
a/c Ballarat Agricultural Society ...	75	6 0	12 0	24 0	48 0	Open for general traffic a/c Society and show exhibits of goods and live stock.
Show Grounds Platform, a/c J. White and Co.	75½	—	—	—	—	An additional charge shall be made of 20s. 8d. for each trip made by an engine to place trucks, whether loaded or empty, on the siding. Inwards consignments to be waybilled to Ballarat, and trucks carded "Show Grounds Platform, Ballarat."
Commonwealth Oil Refineries Ltd. Site, Doveton-street	73¾	6 0	12 0	12 0	—	
Doveton-street Stock Yards	‡	6 0	—	—	—	‡ Mileage rate as to Ballarat shall be charged :— *Subject to a minimum of 24s. 9d. per trip, for placing by special pilot.*

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
BALLARAT—*continued.*	Miles.	s. d.	s. d.	s. d.	s. d.	
State Electricity Commission of Victoria ...	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Ballarat, plus 2¼ miles. In addition to the freight charges the charges shewn in footnote (a), shall be imposed, the time to be calculated as follows :— (a) in the case of traffic in full train loads taken direct to the Siding—for the time the engine is engaged in shunting at the Siding ; (b) in the case of traffic in less than full train loads operated by pilot from Ballarat—from when the engine leaves Ballarat until its return thereto ; (c) in the case of trucks placed by the engine of a passing train—from when the engine arrives at the main line points giving access to the siding until its journey is recommenced from that point.
BALLARAT EAST—						
J. Ryan's Loading Platform for Logs ...	73¼	6 0	12 0	—	—	
BALMORAL—						
State Rivers and Water Supply Commission's Oil Discharging Site	238½	6 0	12 0	—	—	
BALRANALD—						
Vacuum Oil Co's. Oil Discharging Site ...	272	6 0	12 0	12 0	24 0	
BARNAWARTHA—						
D. D. Anderson Siding	174	6 0	12 0	12 0	24 0	
BEALIBA—						
Departmental Ballast Siding, a/c J. Baruta, Bealiba Traders Ltd., Sutton Bros. and Forests Commission of Victoria	137¾	6 0	12 0	—	—	The shunting charges shall not be made in respect of :— (i) Empty vehicles placed for the loading of firewood : or

						(ii) Loaded vehicles placed on the siding and which are loaded Outwards with firewood within the prescribed time allowed for loading. *Where any loaded vehicles are placed on the siding, and* are not loaded Outwards with firewood within the above period, these shunting charges shall be imposed.
BEAUFORT—						
Carriage Dock	102½	6 0	12 0	12 0	—	These charges shall be imposed only when trucks are so placed at the request of consignors or consignees and it is convenient for the Department to do so. Shunting charges are not applicable to empty trucks placed at the Carriage Dock to be loaded with hay, straw, or chaff. The shunting charges shall not be imposed on trucks placed in the Carriage Dock to be loaded with gravel to be picked up by Down trains as no extra shunting is involved than placing the trucks in No. 5 Road, and this arrangement is for Departmental convenience.
Ripon Shire's Oil Pipe Site	102½	6 0	12 0	12 0	—	
A. Gist, Gravel Loading Site ...	102½	6 0	12 0	—	—	
H. H. George, Timber Site	102½	6 0	12 0	12 0	—	
BEECH FOREST—						
Calco Sawmilling Co. Pty. Ltd., Sawmilling Site	124¾	6 0	12 0	12 0	24 0	
BEECHWORTH—						
Carriage Dock	171¼	6 0	12 0	12 0	—	To be imposed only when trucks are so placed at the request of consignors or consignees and it is convenient for the Department to do so.
Zwar Bros. Oil Discharging Site ...	171¼	6 0	12 0	12 0	—	
Reidy Creek Alluvial Co's. Site ...	171¼	6 0	12 0	12 0	—	The Company has been granted permission to have trucks of fuel oil placed at a discharging point in the station yard opposite Zwar Bros. discharging pipe.
BELL—						
Clifton Brick Co's. Siding	8¼	—	—	—	—	
,, ,, ,, ,, *a/c Wm. Cook Pty. Ltd.*	8¼	6 0	12 0	12 0	—	
Departmental Stock Yards	7½	—	—	—	—	Live stock will be placed or cleared by ordinary train. If a special *service from Melbourne is required*, a charge of £7. 14s 7d., in addition to freight charges, shall be imposed.
Electric Crane Site	7½	6 0	12 0	12 0	—	For sorting out and placing trucks at the Electric Crane Site.

(a) *In addition to the freight* a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
BENALLA—						
Vacuum Oil Co's., Commonwealth Oil Refineries, and Shell Co. of Australia Sidings	‡	—	—	—	—	‡ Freight charges shall be computed on mileage to or from Benalla, plus 1 mile. In addition, the charges shewn in footnote (a) shall be imposed for the time that the engine is engaged in placing or removing trucks ; time to be computed from when the engine leaves Benalla until its return thereto. When trucks are placed for more than one Company at the one time, the charges shall be debited to each company proportionately.
Shell Co. of Australia Ltd. and Commonwealth Oil Refineries Ltd. a/c H. C. Sleigh Ltd.	‡	—	—	—	—	‡ Freight charges shall be computed on mileage to or from Benalla, plus 1 mile. In addition, the charges shewn in footnote (a) shall be imposed for the time that the engine is engaged in placing or removing trucks ; time to be computed from when the engine leaves Benalla until its return thereto. When trucks are placed for more than one Company at the one time, the charges shall be debited to each Company proportionately.
Cattle Yards Siding	121‡	—	—	—	—	
Benalla Concrete Works Siding ...	‡	...	—	—	—	‡ Freight charges shall be computed on mileage to or from Benalla, plus 1 mile. In addition to the freight charges, the charges shewn in footnote (a) shall be imposed for the time that the engine is engaged on each trip to and from the siding ; time to be computed from when the engine leaves Benalla until its return thereto. When trucks are placed on the Benalla Concrete Works Siding at the same time as trucks are placed at any of the Oil Company's Sidings, the charges shall be borne proportionately by each in accordance with the number of trucks placed for each of them.

BENDIGO—						
Dazzel Paint Co's. Loading Platform ...	100¾	6 0	12 0	12 0	24 0	The charge shall be imposed when it is necessary to specially place trucks opposite the site leased.
Bendigo Gas Co's. Siding	102¾	—	—	—	—	
Bendigo Preserving Co's. Siding ...	100¾	6 0	12 0	—	—	For small consignments sent to the Siding. No *shunting charge shall* be made for haulage of goods in truck loads from Bendigo to the siding.
Sandhurst (Ministry of Munitions' Siding)	103½	6 0	12 0	24 0	48 0	Open for consignments in truck loads account Ministry of Munitions or contractors for the erection of buildings for the Ministry of Munitions. Applicable for each placing by ordinary trains. If a special trip from Bendigo be necessary to work the siding the charge shewn *in footnote (a) shall be imposed ; the time* to be computed from when the engine leaves Bendigo until its return thereto.
Old Cattle-yards Platform, a/c Hume and Iser	101¾	6 0	12 0	12 0	24 0	
Tomlins, Simmie and Co. Siding ...	101½	6 0	12 0	24 0	48 0	
Victorian Inland Meat Authority's Siding " " " " " (a) *Fortuna Food Processing Pty. Ltd.* (b) *Intercontinental Packers Pty. Ltd.* (c) *Mayfair Distributors Pty. Ltd.* ...	99½‡	6 0	12 0	12 0	—	1. To be applied to trucks placed on the siding by a through Goods train. 2. When a special trip is necessary between Bendigo and the siding, a charge of 61s. 8d. for each trip shall be imposed, and any detention at the *works for which the Company is liable* shall be charged for at the detention charges shewn in footnote (b). 3. If the placing for any one Company be done in conjunction with the *placing for another Company (or Companies)* the charge of 61s. 8d. shall be borne proportionately, i.e., according to the number of trucks placed for each Company. 4. *When engine is detained at the siding for any* departmental reason, such as to discharge loaded trucks to secure the empty wagons for other traffic, the Company will not be debited with detention charges. 5. ‡*This siding junctions with the main line,* Wallan to Bendigo, at a point 99 miles from Melbourne. Freight chall be computed for the actual mileage to or from the *terminus of the siding,* 99½ *miles from* Melbourne.

(a) *In addition to the freight a charge shall* be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

(b) Detention charges shall be imposed at the rates shewn in (a).

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
BENDIGO—*continued.*						
Bendigo Cattle Siding	100¾	—	—	—	—	Eight shillings (8s. 0d.) per truck for consignments of stock charged at full truck rate, and four shillings (4s. 0d.) per truck for consignments charge at less than full truck rate, shall be added to the mileage rates to or from Bendigo for haulage to or from the Stock Yards Siding. No additional charge shall, however, be made for Pigs which are discharged at the Bendigo Station.
Rangelea (Department of Supply and Development Sidings) on Wallan-Bendigo line	99½‡	—	—	—	—	1. ‡The mileage of the terminus of the sidings is 99½ miles from Melbourne, the junction point on the line from Wallan to Bendigo is situated at a point 98½ miles from Melbourne. Freight charges shall be computed at ordinary tariff rates for the actual mileage to or from the siding.
		6 0	12 0	24 0	48 0	2. Applicable when placing is done by a passing train. 3. (i) When the siding is worked by Pilot Engine from Bendigo or North Bendigo, the charges shewn in footnote (a) shall be imposed for each placing of trucks at the siding, time to be computed from when the engine leaves Bendigo or North Bendigo, as the case may be, until its return thereto. (ii) A similar charge as provided in (i) shall be imposed for special trips to clear trucks from the siding, but if both placing and clearing be done with one service one charge only shall be imposed.
North Bendigo–Victorian Wheatgrowers Corporation Ltd. Site	101½	6 0	12 0	12 0	24 0	
Caltex Oil (Aust.) Pty. Ltd. Siding ...	101¾	6 0	12 0	—	—	
,, ,, a/c Postmaster General's Dept.	101¾	6 0	12 0	—	24 0	

North Bendigo	101½	6 0	12 0	12 0	—	To be imposed for each placing at North Bendigo for consignors or consignees not mentioned above.
BET BET—						
Consols Sdg., a/c Akers, Trickey and any other consignor	121	6 0	12 0	—	—	To be imposed on each occasion trucks are placed on or cleared from the siding.
BEULAH—						
W. C. Thomas and Sons Siding ...	239	6 0	12 0	12 0	—	
Vacuum Oil Coy. Pty. Ltd. Oil Discharging Site	238¾	6 0	12 0	—	—	
BIRCHIP—						
Hogan and McLoughlin's Shed Site ...	214¾	6 0	12 0	12 0	24 0	
Vacuum Oil Co's. Pipe Site	214¾	6 0	12 0	12 0	24 0	
BLACKBURN—						
Blackburn Cool Stores Pty. Ltd. Siding ...	‡	6 0	12 0	12 0	—	‡ Freight to be computed on the mileage to or from Blackburn, plus ½ of a mile.
Blue Moon Fruit Co-operative Ltd. Siding ...	‡	6 0	12 0	12 0	—	
BOOLARRA—						
E. Carstein's Timber Mill Site ...	102	6 0	12 0	12 0	24 0	Applicable if engine power be used to sort out or place trucks opposite the site.
BOORCAN—						
Trufood of Australia Ltd'.s Siding ...	131½	6 0	12 0	24 0	48 0	
BOX HILL—						
W. T. Murphy Pty. Ltd.	10½	6 0	12 0	12 0	—	
Standard Brick Co's. Siding, a/c Trufood Co. (Aust). Pty. Ltd.	10½	6 0	12 0	—	—	
State Electricity Commission's Site ...	10½	6 0	12 0	—	—	
BOLTON—						
Australian Plaster Industries Pty. Ltd. ...	265½	—	—	—	—	
BRIDGEWATER—						
M. Coghlan's Siding	126½	6 0	12 0	12 0	24 0	
Water Wheel Flour-mills Siding ...	126½	—	—	—	—	
F. G. Lyndon Firewood Site	126½	6 0	12 0	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
BRIGHT— *State Electricity Commission of Vic. Site* ...	196	6 0	12 0	—	—	
BROADMEADOWS— *"Broadstore" (Department of Army Siding)*	‡	—	—	—	—	‡ 1. Freight charges shall be computed on the mileage as to or from Broadmeadows, plus 1½ miles.

2. When placing is done at the siding:—	The charges shall be imposed as shown in Footnote	Time to be computed from when the engine leaves the undermentioned station until its return thereto.
(i) by an engine of a local or passing goods train.	(d)	Broadmeadows.
(ii) by extension from Essendon of the local Essendon pilot or ordinary goods train to Essendon.	(c)	Essendon.

3. If a special train (with a minimum load of 15 four-wheeled trucks) is run from Melbourne and/or Tottenham to the Siding, no special shunting charges shall be imposed, but if a special train from the

						abovementioned places is run with less than 15 four-wheeled trucks, the charges shown in footnote (c) shall be imposed, the engine time to be computed from when the engine leaves Melbourne Yard or Tottenham, as the case may be, until its return thereto.
BUNGAREE—						
Brophy, Foley, and Co's. Site ...	64	6 0	12 0	12 0	24 0	
Tinney Bros. and Hanrahan Pty. Ltd.'s Shed Sites	64	6 0	12 0	12 0	24 0	
Tinney Bros. and Hanrahan Pty. Ltd. ...	64	6 0	12 0	12 0	24 0	
BURNLEY—						
Burnley Flour Mills Pty. Ltd. Siding ...	3½	6 0	12 0	—	—	
State Electricity Commission's Storage Site	3½	6 0	12 0	—	24 0	
CALIFORNIA GULLY—						
Atlantic Union Oil Co. Ltd. Siding ...	104½	6 0	12 0	12 0	—	
H. C. Sleigh Ltd.	104½	6 0	12 0	12 0	—	
CAMBERWELL—						
State Electricity Commission's Storage Site	6½	6 0	12 0	—	—	
CAMPERDOWN—						
Vacuum Oil Co's. and Shell Co. of Aust. Ltd. Siding	123¼	—	—	—	—	The siding will be worked by an engine from Camperdown ; the charges shewn in footnote (a) shall be imposed for the time that the engine is engaged in placing trucks on the siding ; time shall be computed from when the engine leaves Camperdown until its return thereto. If the trucks are placed for both companies by the one trip, the engine charges shall be apportioned *pro rata* between the two companies according to the number of trucks placed for each.
Cattle Siding	123¾	—	—	—	—	This mileage shall be charged for freight on Live Stock in and out.

(c) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes for the time that the engine is engaged on each trip to and from the siding.

(d) The charge shewn in Footnote (c) shall be imposed except that the minimum charge shall be 61s. 8d. for each trip.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
CARISBROOK—						
Caristore (Dept. of Air Siding) ...	‡	6 0	12 0	24 0	—	‡ 1. Freight charges shall be computed on the mileage to or from Maryborough, plus 2¾ miles. 2. Applicable if the shunting is performed by a passing goods train. 3. If the siding be worked by a pilot engine from Maryborough, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged ; time to be computed from when the engine leaves Maryborough until its return thereto.
CARRUM—						
Australian Glass Manufacturers Co. Ltd. Siding	22¾	6 0	12 0	24 0	48 0	
Pullen, Taylor and Turrey Firewood Cutting Site	22¾	6 0	12 0	12 0	—	
CASTERTON—						
Shire of Glenelg Storage Site ...	245¼	6 0	12 0	12 0	24 0	
Elder Smith and Co. Shed Site ...	245¼	6 0	12 0	12 0	24 0	
J. Handbury and Son's Shed Site ...	245¼	6 0	12 0	—	—	
Fred Osborne and Son's Shed Site ...	245¼	6 0	12 0	—	—	
CASTLEMAINE—						
Thomspons (Castlemaine) Ltd.'s Siding ...	78¾	—	—	—	—	
Thompson (Castlemaine) Ltd.'s Oil Discharging Site	78¾	6 0	12 0	12 0	—	
CHARLTON—						
Charlton Cattle Siding	173¾	6 0	12 0	12 0	—	Live stock is exempt from shunting charges.
Noske Flour Mills Pty. Ltd.	173¾	6 0	12 0	24 0	48 0	

Shell Co. of Aust. Ltd. and Atlantic Union Oil Coy. Ltd.	173¾	6 0	12 0	24 0	48 0	If trucks for Atlantic Union Oil Coy. Ltd. and Shell Coy. of Australia Ltd. are placed on the siding during the one operation, the total charge shall be apportioned according to the number of trucks placed for each Company. On any occasion on which an engine engaged in the placing of trucks is delayed by the Company's failure to arrange for the prompt opening of the gates for the passage of railway vehicles, a detention charge of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes for the time the engine is delayed, shall be imposed. In the event of trucks for Atlantic Union Oil Coy. Ltd. and Shell Coy. of Australia Ltd. being placed at the one time, the detention charge shall be apportioned according to the number of trucks placed for each Company.
Ampol (Alba) Petroleum Pty. Ltd.'s Oil Valve Site	173¾	6 0	12 0	12 0	—	
Charlton Electric Light and Power Co's. Oil Tank Site	173¾	6 0	12 0	12 0	—	
CHILTERN—						
Departmental Siding, a/c State Rivers and Water Supply Commission	‡	—	—	—	—	‡ The siding is situated 2¾ miles on the Up side of Chiltern. Freight charges shall be computed on the mileage to or from Chiltern, plus 2¾ miles. A charge of 61s. 8d. shall be imposed on each occasion on which trucks are placed on or removed from the siding.
COBDEN—						
Standard Quarries Pty. Ltd. ...	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Curdie, plus 4 miles, provided two or more trucks are placed at the one time. If only one truck is placed at a time, a placing charge of 12s. 0d. per 4-wheeled truck, shall be imposed, in addition to the freight charges mentioned in preceding paragraph.
Victorian Agricultural Lime Ltd's. Siding ...	‡	6 0	12 0	12 0	—	‡ Freight charges shall be computed on the mileage to or from Curdie, plus ½ mile.
COBRAM—						
Cobram Fruit Packing Co. Pty. Ltd. Site ...	155¼	6 0	12 0	—	—	The shunting charges are to be imposed only in respect of inwards loaded trucks. Shunting charges are not applicable to empty trucks placed at the site for loading fruit Outwards.
N.S.W. Water Conservation and Irrigation Commission's Site	155¼	6 0	12 0	12 0	—	
Kimpton, W. S. & Sons Shed Site ...	155¼	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (mimimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
COBURG—						
Lincoln Knitting Mills (Australia) Ltd. Siding	7	—	—	—	—	The charges shewn in footnote (a) shall be imposed for the time the engine is employed in working the siding ; the time to be computed from when the engine leaves Coburg until its return thereto.
G. Cornell Firewood Cutting and Stacking Site	6¼	6 0	12 0	12 0	24 0	
COHUNA—						
Gunbower Co-op. Butter Factory and Trading Co. Ltd.	185½	6 0	12 0	—	—	
State Rivers and Water Supply Commission's Storage Site	185½	6 0	12 0	—	—	
COLAC—						
Caltex Oil (Aust.) Pty. Ltd. Oil Discharging Site	95¼	6 0	12 0	24 0	—	
Shell Co. of Australia Siding ...	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Colac, plus 1½ miles. The charges shewn in footnote (a) shall be imposed for the time that the engine is engaged in placing or removing trucks ; time shall be computed from when the engine leaves Colac until its return thereto.
Colac Farmers' Co-op. Shed, a/c Saltau and Sons	95¼	10 3	20 6	—	—	
Atlantic Union Oil Co's. Siding; *Commonwealth Oil Refineries Siding and Vacuum Oil Co's. Siding*	‡	—	—	—	—	‡ Mileage shall be computed on the mileage to or from Colac, plus ¼ of a mile. The charges shown in footnote (a) shall be imposed for the time that the engine is engaged in placing or removing trucks ; time to be computed from when the engine leaves Colac until its return thereto. When trucks are placed for more than one Company at one time, the charges shall be debited to each company proportionately.
COLBINABBIN—						
Colbinabbin Grain Storage Co's. Shed Site	117½	—	—	—	—	

COLERAINE—						
G. Riordan Loading Site	221¼	6 0	12 0	12 0	24 0	
CORIO—						
Geelong Grammar School Siding ...	39½	6 0	12 0	—	—	Inwards traffic only.
Distillers Corporation Pty. Ltd. Siding ...	40½	6 0	12 0	12 0	24 0	See footnote (g) When a special trip is run from North Geelong to place trucks at, or *clear trucks from the siding, the charges shewn in footnote (a) shall* be imposed ; time to be computed from when the engine leaves North Geelong until its return thereto.
Shell Refinery (Aust.) Pty. Ltd.	‡	—	—	—	—	‡ The freight charges shall be computed on the actual mileage to or from Corio (39¾) plus ¾ mile. See footnote (g). In addition to the freight charges a shunting charge of 30/10d. for the first 15 minutes and 9/4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30/10d.) shall be imposed for the time the engine is engaged in placing trucks on or removing trucks from the siding, time to be computed as follows :— (i) When the siding is worked by a through train—from when the engine arrives at the main line points giving access to the Siding until its journey is recommenced from that point. (ii) When the siding is worked by pilot trip from North Geelong—from when the engine leaves North Geelong until its return thereto. (In the event of the pilot engine operating at the siding of the Distillers Corporation Pty. Ltd. before returning to North Geelong the charge shall be apportioned according to the number of trucks placed and/or cleared for each siding holder.)
COWWARR—						
Thomson Valley Farmers Ltd's. Terminal Box and Oil Pipe Line Site	114¾	6 0	12 0	12 0	24 0	
CRANBOURNE—						
Cranbourne Sand Pty. Ltd.	‡30½	—	—	—	—	‡ On the Down journey the mileage shall be computed *via* the Junction, which is situated on the Main Line at a point 28 miles from Melbourne (Spencer-street). Ordinary mileage rates shall be charged from the terminus of the siding, subject to the following additional charges :— (i) Provided not less than 15 fully loaded trucks for stations on the Up or Down side of Cranbourne are available for clearance by engine placing empties, and that they are all properly marshalled in destination order and coupled up by the company, ordinary mileage rates only shall be imposed.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

(g) Freight on consignments forwarded between Melbourne and suburban stations and the siding shall be computed in accordance with the District *Rates between Melbourne and Geelong and North Geelong.*

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
CRANBOURNE—*continued*—						(ii) When the number of trucks ready to be cleared is less than 15, as set out in preceding clause, a shunting charge of 6s. 0d. for each 4-wheeled truck less than 15 shall be imposed, but such charge must not exceed 72s. 1d. (iii) When a proportion of the 15 trucks are loaded for stations on the Down side of Cranbourne they must be loaded in destination order for a straight kick-off at Cranbourne station. If the siding be cleared by the Dandenong engine, on a day other than Saturday, the number of trucks specified shall be reduced to 10 vehicles.
CRIB POINT— *Naval Base Siding*	46½	—	—	—	—	The charges shewn in footnote (a) shall be imposed (except that the minimum charge shall be 61s. 8d. for each trip), the time to be computed from the departure of the engine from Crib Point until its return thereto. If the work cannot be performed by the ordinary engine and an engine has to be specially obtained for the service, the question of the special charge to be imposed shall be referred to the Chief Commercial Manager. If shunting is performed for both Naval Base and Works and Railways Departments, the charges shall be apportioned between the two Departments.
Naval Base Siding, used by Contractors to Works and Railways Department	46½	—	—	—	—	
CROWES— *Pile Siding a/c Keith King Pty. Ltd.* ...	130¼	6 0	12 0	12 0	24 0	Open for Inwards and Outwards goods, account Messrs. Keith King Pty. Ltd.
CROYDON— *Croydon Orchardists' Co-operative Association's Siding*	19½	6 0	12 0	—	—	Applicable to full truck loads only of chaff, grain, or other traffic not incidental to the fruit growing industry, when consigned to the siding which may, if it is convenient for the Department to do so,

						be placed in the siding. When it is inconvenient for the Department to place such truck loads in the siding, the Stationmaster shall so advise the Manager of the Association and delivery of the goods must be taken at the public siding.
DANDENONG—						
State Electricity Commission of Victoria ...	19½	6 0	12 0	24 0	48 0	
Gippsland Co-op. Bacon Curing Coy. Ltd.'s Siding	19¼	6 0	12 0	—	—	Outwards traffic may be sent in less than truck loads.
Gippsland Co-op. Bacon Curing Coy. Ltd.'s Siding, a/c Dandenong Bluestone Quarries Pty. Ltd.	19¼	6 0	12 0	24 0	48 0	Cyclone gates have been erected across the line at the Company's Siding and on any occasion on which an engine engaged in the placing of trucks beyond the gates is delayed by the failure of the Company to open the gates promptly, a detention charge of 30/10d. for the first 15 minutes or part thereof and 9/4d. for each 5 minutes or part thereof beyond the first 15 minutes shall be imposed for the time the engine is delayed.
Gippsland Co-op. Bacon Curing Coy. Ltd.'s Siding, a/c Vacuum Oil Co.	19¼	6 0	12 0	—	—	
Shell Co. of Australia	‡	—	—	—	—	‡ Freight shall be computed on the mileage to or from Dandenong plus ¾ of a mile. The charges shewn in footnote (a) shall be imposed for the time that the engine is engaged in placing or removing trucks on or from the the siding. The time shall be computed as follows :— (i) For placing or removing trucks—15 minutes. (ii) For placing and removing trucks—20 minutes.
International Harvester Coy. of Australia Pty. Ltd. Siding	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Dandenong plus 2¼ miles. The siding will be worked by pilot from Dandenong and in addition to the freight charges the shunting charges shown in footnote (a) shall be imposed for the time the engine is engaged on each trip to and from the siding. The time to be computed from when the engine leaves Dandenong until its return thereto.
Swallow and Ariel (Dehydrated Products) Pty. Ltd. *Swallow and Ariel (Dehydrated Products) Pty. Ltd., a/c Dandenong Bluestone Quarries Pty. Ltd.*	19½	6 0	12 0	24 0	48 0	
DARNUM—						
A. J. Peterson Storage Site	66½	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
DARTMOOR—						
Dartmoor Bark Mill Site	261¼	6 0	12 0	—	—	To be imposed when an engine is used to place trucks at or move trucks from the site.
DEAN MARSH—						
Anderson, Mackie and Co.	90¾	6 0	12 0	24 0	—	Applicable only when trucks are placed for this Company on the Siding near crane when no other trucks are being placed on the crane siding.
DEER PARK—						
Ravenhall (Department of Interior Siding)	‡	—	—	—	—	‡1. Freight charges shall be computed on the mileage to or from Deer Park, plus 1½ miles. 2. (i) When the siding is worked by the local pilot or by a passing goods train the charges shewn in footnote (a) shall be imposed, time to be computed from when the engine leaves Deer Park until its return thereto. (ii) If a special trip from Tottenham be necessary to work the siding the charges shown in footnote (a) shall be imposed, time to be computed from when the engine leaves Tottenham until its return thereto.
DENILIQUIN—						
Shell Co. of Australia	190¼	24 0	—	—	—	If the placing of trucks for the Werai Sawmilling Co. and the Shell Co. of Australia Ltd. be performed at the same time, the placing charge shall be borne proportionately.
Werai Sawmilling Co.	190¼	24 0	—	—	—	
Deniliquin Municipal Council Oil Pipe Site	190¼	6 0	12 0	12 0	—	
N.S.W. Water Conservation and Irrigation Commissions' Oil Pipe Site	190¼	6 0	12 0	12 0	—	

Postal Department's Pole Site ...	190¼	6 0	12 0	24 0	48 0	
DENNINGTON—						
Nestle's Food Specialities (Australia) Ltd. Siding	169	6 0	12 0	—	—	When an engine is ordered from Warrnambool for the special purpose of placing or clearing trucks between the station yard and the Company's siding, or to place trucks which may be specially required at the siding by the Company after the ordinary daily trips, a charge for the use of the engine at the rate of 81s. 1d. per hour, or part thereof, shall be imposed, the time to be computed from when the engine leaves Warrnambool until its return thereto.
Shell Co. of Australia's Siding ...	169	6 0	12 0	24 0	—	To be imposed for placing trucks whether loaded or empty in the siding by an ordinary goods train or by an engine performing ordinary daily trip from Warrnambool. When a special trip is run at the request of the siding holder to place or clear trucks the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves Warrnambool Yard until its return thereto.
DIMBOOLA—						
Dimboola Flour Mill Co's. Siding ...	224¾	6 0	12 0	—	—	
Dimboola Shire's Discharging Site for Oil in Bulk	224¾	6 0	—	—	—	
Vacuum Oil Co's. Siding	224¾	6 0	12 0	12 0	24 0	
DONALD—						
Victorian Inland Meat Authority ...	181¾	6 0	12 0	24 0	48 0	When it is necessary to make a special trip to the siding from Donald to place or clear trucks, a charge of 20s. 8d. shall be imposed for each 15 minutes or part thereof that an engine is so engaged. The time to be computed from when the engine leaves Donald Station Yard until its return thereto.
J. S. Bartlett's Siding	182½	6 0	12 0	12 0	24 0	
Vacuum Oil Coy's. and Commonwealth Oil Refineries Siding	182½	—	—	—	—	The charges shewn in footnote (a) shall be imposed for the time that the engine is engaged in placing trucks, time to be computed from when the engine leaves Donald Yard until its return thereto. In the event of trucks being placed on the siding for both companies, only one charge shall be imposed, the charges being divided *pro rata* between the companies.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
DONALD—*continued.*						
James Malcolm and Co Pty.. Ltd. Oat Storage Site	182½	6 0	12 0	12 0	24 0	
Grain Shed Sites	182½	6 0	12 0	12 0	24 0	
Shire of Donald's Oil Discharging Site ...	182½	6 0	12 0	12 0	24 0	
DOOKIE—						
Shepparton and Rodney Shires Siding ...	130	—	—	—	—	
DUNNSTOWN—						
Warrenheip Distillery (Federal Distilleries Ltd.) Siding	67½	—	—	—	—	
DUNOLLY—						
Australian Wheat Board's Wheat Storage Bin Sites	125¾	10 2	20 4	—	—	To be imposed for placing inwards loaded trucks at the site or removing loaded trucks from the site and shall be included on waybills.
DYSART—						
Dysart Military Siding	‡	—	—	—	—	‡ 1. Freight charges shall be computed on the mileage to the junction points on the Main line—59¼ miles from Melbourne—plus ½ mile, with the exception that the charges on consignments in less than train loads of 15 four-wheeled trucks—forwarded on Down journey to the siding or on Up journey from the siding shall be computed on the mileage to Seymour—61¼ miles from Melbourne—plus 2½ miles. 2. In addition to the freight charges the following shunting charges shall be imposed :— (i) When trucks are detached from roadside trains a charge of 24s. 0d. for each placing of trucks on the siding.

EAGLEHAWK— *Keast and Co's. Chaff Mill Site* ...	105‡	6 0	12 0	—	12 0	
ECHUCA— *Echuca Wharf*	‡	—	—	—	—	

(ii) When a pilot engine is utilised for placing trucks on the siding, the charges shewn in footnote (a) shall be imposed ; the time to be computed from when the engine leaves Seymour until its return thereto.

‡ 1. All goods river borne to or from Echuca Wharf and despatched by rail between the wharf and stations other than Echuca, and Live Stock forwarded between the Echuca Wharf and stations other than Echuca, shall be charged for 1 mile in addition to the mileage to or from Echuca.

2. Except as prescribed in clause 6 hereof, the stevedoring charge at the wharf shall be 3s. 4d. per ton on all classes of traffic.

3. No charge shall be made for placing or removing loaded or empty trucks.

4. Grain, from the wharf to the station or to the local mills, shall be charged 2s. 11d. per ton, in truck loads, subject to a minimum of full carrying capacity of trucks, less 5 per cent.

Sleepers transferred by the Department, direct from river boats into rail trucks in one operation, shall be subject to a stevedoring charge of 1s. 10d. per ton, which shall be in addition to the usual loading charge of 4s. 0d. per ton.

6. The undermentioned commodities river borne to or from Echuca, Wharf, but not carried by rail, shall be subject to the following charges for stevedoring :—

Commodity.	*Rate per Ton.* s. d.
Timber (Murray Pine)	7 11
" (Hardwood)	4 0
Bricks	7 11
Furniture	7 11
Firewood	4 0
Red Gum Paving Blocks	6 0
Telegraph Poles	4 0

7. Goods in truck loads consigned to Echuca Station and which, after arrival at Echuca, are reconsigned to the wharf, shall be charged 10s. 3d. per 4-wheeled truck, and 20s. 6d. per bogie truck.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
ECHUCA—*continued.*	Miles.	s. d.	s. d.	s. d.	s. d.	
Echuca Wharf Forests Commission of Victoria Loading and Sawmilling Site	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Echuca, plus 1 mile. The charges shewn in footnote (a) shall be imposed for placing trucks at the site or removing trucks therefrom ; time to be computed from when the engine leaves the Echuca Station Yard until its return thereto.
McCulloch Carrying Co's. Siding ...	145	6 0	12 0	12 0	24 0	The minimum charge for each placing shall not be enforced when two trucks are ordered and only one supplied, or when two trucks are supplied one of which is used for loading account the Department.
Caltex Oil (Aust.) Pty. Ltd. ...	145	6 0	12 0	24 0	—	
R. J. Evans' Sawmill Site	146	6 0	12 0	12 0	—	These charges apply when Echuca Wharf is open for general traffic. The minimum charge shall not be enforced when two trucks are ordered and only one supplied or when two trucks are supplied, one of which is used for loading account the Department. When Echuca Wharf is closed for general traffic and this site is worked in conjunction with local traffic, these shunting charges shall also apply except when it is necessary to make a special trip to place empties or clear loaded trucks at this site, when the charges shewn in footnote (a) shall be made ; the time to be computed from when the engine leaves Echuca Yard until its return thereto.
Echuca Flour Mills Pty. Ltd. Siding ...	145¼	—	—	—	—	
Echuca Rice Milling Pty. Ltd. Siding ...	145	6 0	12 0	24 0	48 0	
Echuca Motor Garage's Oil Discharging Site	146	6 0	—	—	—	When it is necessary to run a special trip from Echuca to place trucks at the site, the charges shewn in footnote (a) shall be imposed for the time that an engine is engaged ; time to be computed from when engine leaves Echuca until its return thereto.

Shell Co. of Australia, and Vacuum Oil Co's. Siding, and Commonwealth Oil Refinery's Siding	‡	6 0	12 0	24 0	—	‡ Mileage shall be computed as to or from Echuca, plus ½ mile. For any delay at the gate at the entrance of the C.O.R. Siding to any engine engaged in the operation of traffic to or from the Siding, a detention charge of 30s. 10d. for the first 15 minutes or part thereof, and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes, shall be made.
Well's Shed Lease	145	6 0	12 0	12 0	24 0	
ELMORE—						
Carriage Dock Siding	128½	6 0	12 0	12 0	—	These chages shall be imposed only when trucks are placed at the request of consignors or consignees and it is convenient for the Department to do so.
Vacuum Oil Co's. Oil Discharging Site ...	128½	6 0	12 0	12 0	—	
EMERALD—						
C. A. Nobelius and Sons Pty. Ltd. Siding	33½	—	—	—	—	Any regular train may stop at the siding :— (i) To pick up or discharge goods in quantities of 5 cwt. or over *without extra charge.* (ii) In lesser quantities, on payment by Mr. Nobelius of an additional charge of 4s. 0d. (iii) With (i) and (ii), passengers holding tickets for the next station (longer mileage) may join or alight. (iv) For passengers when there are no goods, as above, on payment by Mr. Nobelius of 4s. 0d. on each occasion. Debit for the extra charge shall be raised by the Stationmaster, Emerald, on a paid waybill issued to Upper Ferntree Gully.
Emerald and District Co-op. Society's Lease of Department's Goods Shed	33	6 0	12 0	—	—	To be imposed for each truck specially placed at the Goods Shed by engine power.
EPSOM—						
Henry Williams and Sons Pty. Ltd. Loading Platform and Tramway	105½	6 0	12 0	—	—	
Bendigo Pottery Co. (G. D. Guthrie and Co. Pty. Ltd.)	105½	—	—	—	—	
ERWEN—						
D. J. Hitchcock's Firewood Cutting and Storage Site	109	6 0	12 0	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
EUREKA	74½	—	—	—	—	Actual mileage rates shall be charged. One trip daily (if required), (Sundays and holidays excepted), will be run from Ballarat to place and clear trucks. Should any consignee or consignor require a special trip to place or clear trucks, in addition to the regular service, the haulage charge for such special service shall be 20s. 8d. per return trip. In the event of the engine of this special trip being required by the hirers to place or clear trucks for more than one consignee or consignor, only one charge of 20s. 8d. shall be imposed (in addition to freight) for such trip, and the amount debited in equal parts to each firm concerned.
H. C. Sleigh Ltd.	74½	6 0	12 0	12 0	—	When a special engine is engaged, the charges shewn in footnote (a) shall be imposed; time to be computed from when the engine leaves the Ballarat Yard until its return thereto.
Atlantic Union Oil Co. Ltd. Siding ...	74½	6 0	12 0	12 0	—	When a special engine is engaged, the charges shewn in footnote (a) shall be imposed; time to be computed from when the engine leaves Ballarat Yard until its return thereto.
Eureka Terra Cotta and Tile Co's. Siding	74½	6 0	12 0	—	24 0	Consignments of tiles, minimum 1 ton, may be accepted for despatch, provided trucks are carded at the siding for Ballarat Goods Shed for further suitable loading.
Caltex Oil (Aust.) Pty. Ltd. Siding ...	74½	6 0	12 0	12 0	24 0	When a special engine is engaged, the charges shewn in footnote (a) shall be imposed; time to be computed from when the engine leaves Ballarat Yard until its return thereto.
EVERTON—						
I. G. Gillies' Firewood Cutting Site ...	161	6 0	12 0	12 0	24 0	

FAIRFIELD—						
Australian Paper Manufacturers Ltd. Siding	6¾	6 0	12 0	—	—	Minimum charge 60s. 5d. for each special service. A tractor is used by this Company to haul trucks between their installation and the point where the trucks are placed and cleared by this Department— (1) The tractor shall only be used during clear daylight. (2) The tractor must not be used and must be placed clear of the siding lines when a locomotive is due to leave Fairfield for the Company's sidings, or at any time a locomotive has left Fairfield for the siding, and the tractor must be kept clear of the siding lines until the locomotive has completed its work at the siding and has departed on the return for Fairfield. The responsibility for ascertaining that a locomotive has entered on to the siding lines at Fairfield devolves upon the Company. (3) Should the Department's operations be held up on account o the movements of the tractor, a charge of 9s. 4d. for each 5 minutes or portion thereof that a locomotive is detained thereby, shall be debited to the Company.
FISH CREEK—						
Meyer Bros. Shed Site	100½	6 0	12 0	—	—	
FITZROY—						
State Electricity Commission's Storage Site	6	6 0	12 0	—	—	
Boyd's Timber Discharging Site ...	6	6 0	12 0	—	—	
Firewood Siding Frontages	6	6 0	12 0	—	—	This charge shall be made for placing all trucks (either for loading or unloading) for all lessees opposite their sites on the siding frontages in the Goods Yard.
C. B. Jardine's Straw Envelope Factory Site	6	6 0	12 0	—	—	
FOOTSCRAY—						
Angliss' Siding	3	—	—	—	—	(1) *Inwards Traffic.*—Goods or live stock forwarded to the siding shall be subject to a special charge of 20s. 8d. per truck (minimum £4 2s. 1d., for each special trip), in addition to the tariff rates from the forwarding station.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
FOOTSCRAY—*Angliss Siding continued.*						(2) *Outwards Traffic.*—Tallow forwarded from the siding to Port Melbourne Pier or Williamstown Pier shall be charged the ordinary rates, subject to a minimum of four (4) trucks per special trip between Melbourne and the siding. If tallow or skins be forwarded by a special trip run for meat, the minimum of four trucks will not be enforced. Blood manure from the siding to any station by a special trip run for meat, tallow, or skins, and in those conditions only, shall be charged the ordinary rates. Special rates are provided for the carriage of Meat, Skins, Hares and Rabbits.
Co-operative Box Co. of Victoria Electric Crane Site (Maribyrnong River Loop)	4	6 0	12 0	—	48 0	
State Electricity Commission's Siding (Maribyrnong River Loop)	4	6 0	12 0	12 0	—	If engines are delayed owing to the gates at the siding not being unlocked, the chargs shewn in footnote (a) shall be imposed as a detention charge for the time so involved.
Co-operative Box Co. of Victoria Ltd. (Maribyrnong River Loop)	4	10 3	20 6	—	—	All Outward traffic from the siding for the Northern, North-Western, Western, and South-Western lines shall be charged, in addition to the freight, for the actual mileage from the siding, a haulage charge of 1s. 11d. per ton.
Commonwealth Fertilizers and Chemicals Ltd. Siding (Maribyrnong River Loop)	4	10 3	20 6	—	—	All Outwards traffic from the private siding or forwarded by Commonwealth Fertilizers and Chemicals Ltd. from the public sidings for the Northern, North-Western, Western, and South-Western lines shall be charged, in addition to the freight, a haulage charge of 1s. 11d. per ton. Empty manure bags in any quantity may be accepted for despatch direct to the Company's siding.

Colonial Gas Association's Siding ...	4	6 0	12 0	12 0	24 0	
FORREST—						
J. Sharp and Sons Pty. Ltd. Timber Mill ...	103¼	6 0	12 0	12 0	—	
Otway Sawmills Pty. Ltd. Mill Site ...	103¼	6 0	12 0	12 0	—	
FRANKSTON—						
Australian Glass Manufacturers Co. Pty. Ltd. Site	27½	6 0	12 0	12 0	24 0	
C. A. Jackson's Firewood Storage Site ...	27½	6 0	12 0	12 0	24 0	
FYANSFORD—(see page 70)						
GEELONG—						
Co-Dair Products (Geelong) Ltd. ...	45	—	—	—	—	In addition to the freight charges the charges shewn in footnote (a) shall be imposed for the time the engine is engaged in placing trucks on the siding. Time to be computed from when the engine leaves Geelong until its return thereto.
Dalgety and Co's. Siding	‡	6 0	12 0	—	—	‡ Freight shall be computed on the mileage to or from Geelong plus ¾ mile.
Dennys, Lascelles Ltd. Siding ...	‡	6 0	12 0	—	—	‡ Freight shall be computed on the mileage to or from Geelong plus 1 mile.
Geelong Terminal Elevator Siding ...	‡	—	—	—	—	Open for the receipt of bulk wheat. ‡ Freight on such inwards traffic shall be computed on the mileage from the forwarding station to Geelong Pier.
Huddart Parker Ltd. Siding	45½	—	—	—	—	
Racecourse Platform	47¾	6 0	12 0	—	—	Open for race and show traffic.
Phosphate Co-op. Co. of Australia Siding ...	‡	—	—	—	—	‡1. Freight charges shall be computed on the actual mileage to or from North Shore, plus 1 mile (see Footnote (g)). 2. When not less than 20 trucks, whether loaded or empty, are placed at any one time a shunting charge of 61s. 8d. shall be imposed.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

(g) Freight on consignments forwarded between Melbourne and suburban stations, and the siding, shall be computed in accordance with the District rates between Melbourne and Geelong and North Geelong.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
GEELONG—*continued.* *Phosphate Co-op. Co. of Australia Siding —continued.*						3. When less than 20 trucks, whether loaded or empty, are placed at any one time, a charge of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes, computed from the time the engine leaves North Geelong until its return thereto, shall be imposed, minimum charge 61s. 8d. 4. Delays *en route* between North Geelong and turn-out to Company's Siding, at North Shore, to be eliminated from charges. 5. Empty return bags consigned to the Phosphate Co. of Aust. (Pivot Company) are to be waybilled to Geelong and charged the prescribed rate thereto, plus an assembling charge of 15s. 5d. per ton, which shall be dealt with as under : (i) Consignments on which the rail freight is prepaid. The assembling charge of 15s. 5d. per ton shall be collected from the consignor by the forwarding station in addition to the freight and shown separately on waybills. (ii) Consignments on which the rail freight is "to pay." The ordinary freight charges as to Geelong shall be shown on waybills and S.M., Geelong, shall issue "paid" waybill for the assembling charges of 15s. 5d. per ton and render an account to the Company. In addition to the assembling charge, an unloading charge at Geelong of 4s. 0d. per ton is to be imposed.
Australian Wheat Board Storage Site ...	‡43½	—	—	—	—	‡Freight on inwards wheat to be computed on mileage as to Geelong Pier, and on cornsacks for the mileage to the site.
Australian Wheat Board Storage Site, a/c Australian Barley Board	‡43½	—	—	—	—	‡Freight charges shall be computed as follows : (i) for inward traffic—on the mileage as from the forwarding station to Geelong Pier ;

						(ii) for outward traffic— (a) from the siding to Geelong Pier—at 2s. 11d. per ton, subject to the tonnage minimum per truck prescribed for barley ; (b) from the siding to other destinations—on the mileage as from North Geelong to destinations concerned.
International Harvester Coy. of Australia Limited	‡	—	—	—	—	‡Freight shall be computed on the mileage to or from North Shore, plus (1) mile (see footnote (g)). When not less than 20 trucks, whether loaded or empty, are placed at the siding at any one time, the shunting charge of 61s. 8d. shall be imposed. When less than 20 trucks, whether loaded or empty, are placed at any one time, the charge of 30s. 10d. for the first 15 minutes and 9s. 4d. for each five minutes or part thereof beyond the first 15 minutes (minimum charge, 61s. 8d.) shall be imposed, computed from the time the engine leaves North Geelong until its return thereto. Delays *en route* between North Geelong and turn-out to the company's siding at North Shore to be eliminated from charges.
Geelong Pier	—	—	—	—	—	See Volume 1, page 177
Brougham St. Crossing	—	—	—	—	—	See Volume 1, page 177
GLENHUNTLY— *Caulfield City Council's Siding* ...	8½	6 0	12 0	—	—	
GLENROWAN— *Glenrowan Quarrying Co's. Siding* ...	136¼	6 0	12 0	—	—	
GOLDEN SQUARE— *N. Cumming (Blair's)*	‡	6 0	12 0	12 0	24 0	‡ Freight shall be computed in accordance with footnote (e). If the placing be done in conjunction with the placing for the Commonwealth Oil Refineries, the shunting charges shall be borne proportionately by each, according to the number of trucks placed for for each siding holder.

(g) Freight on consignments forwarded between Melbourne and suburban stations, and the siding, shall be computed in accordance with the District rates between Melbourne and Geelong and North Geelong.

(e) Freight shall be computed as follows :—

Outwards :— From the siding for the actual mileage as from Golden Square.
Inwards :— (i) From stations on "Down" side for the mileage to Golden Square.
(ii) " " " "Up" " " " " " " " plus 3 miles.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
GOLDEN SQUARE—*continued.*						
Stock Yards	99¼	—	—	—	—	In cases where live stock is delivered at or forwarded from Golden Square by special service, the charges shewn in footnote (a) shall be imposed ; time to be computed from when engine leaves Bendigo until its return thereto.
Shell Co. of Aust. Ltd.	‡	6 0	12 0	12 0	—	‡ Freight shall be computed in accordance with footnote (e).
Vacuum Oil Co's. Shed Site	99¼	6 0	12 0	12 0	24 0	
Commonwealth Oil Refineries Siding ...	‡	6 0	12 0	12 0	—	‡ Freight charges shall be computed in accordance with footnote (e).
GREAT WESTERN—						
Seppelt and Son's Siding	141¼	6 0	12 0	24 0	48 0	
HAMILTON—						
Gas Co. Siding	197½	16 11	—	33 10	—	
Saltau and Son Pty. Ltd. and Caltex Oil (Aust.) Pty. Ltd. Siding	197½	6 0	12 0	—	—	
Shell Co. of Aust. Ltd. Siding ...	197½	6 0	12 0	24 0	48 0	
Commonwealth Oil Refineries, Atlantic Oil Co., and Vacuum Oil Co's. Siding	197½	—	—	—	—	In addition to the freight, the charges shown in footnote (a) shall be imposed for the time the engine is engaged for each placing of trucks on the siding; time to be computed from when the engine commences the work necessary for such placing until its return to the Hamilton yard. In the event of trucks for more than one Company being placed at the one time the charges shall be apportioned according to the number of trucks placed for each Company.
Postal Department's Storage Site ...	197½	16 11	33 10	33 10	—	
Patterson's Mill Site	197½	6 0	12 0	12 0	24 0	
Hamilton Bark Mills and Trading Co. Pty. Ltd. Site	197½	6 0	12 0	—	—	
Noske Flour Mills Pty. Ltd. Shed Site ...	197½	6 0	12 0	12 0	—	

HASTINGS—						
Peninsula Fruit Growers' Association Shed Site	40½	6 0	12 0	12 0	24 0	1. Where trucks are both placed and removed by engine at the one shunt, the charges shall be imposed for placing only. 2. Where trucks are placed by hand and removed by locomotive or placed by locomotive and removed by hand, the prescribed shunting charges shall be imposed for the placing or removal by locomotive, as the case may be. 3. Where trucks are hand shunted to and from the sites by the bodies concerned, no shunting charge shall be imposed.
Hastings and District Co-op. Cool Stores Ltd. Site	40½	6 0	12 0	12 0	24 0	
HAWTHORN—						
Associated Fuel Sales Firewood Stacking Site	4½	6 0	12 0	—	—	
A. L. Eltis Firewood Storage Site ...	4½	6 0	12 0	—	—	
HEALESVILLE—						
Kestle Bros. Platform Site	40	6 0	12 0	—	—	
HEYFIELD—						
Alstergren Pty. Ltd. Timber Mill Site ...	121½	6 0	12 0	—	24 0	
G. Stuckey & Co. Pty. Ltd. Firewood Site ...	121½	6 0	12 0	—	—	
HEYWOOD—						
Bond's Shed Site	235¼	6 0	12 0	—	—	
HEYWOOD–MT. GAMBIER LINE—						
South Australian Worsted Mill Co's. Siding at Mount Gambier	‡	—	—	—	—	Freight shall be computed on the mileage to Mt. Gambier. Trucks may be placed at these Sidings at a time convenient to the Department. When this service is performed, the charges shewn in Footnote (a) *shall be imposed for the time engaged by the engine in placing trucks.*
Mount Gambier Corporation ...	‡	—	—	—	—	
HOPETOUN—						
Vacuum Oil Co's. Oil Discharging Site ...	254½	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

(e) Freight shall be computed as follows :—

Outwards :—From the siding for the actual mileage as from Golden Square.
Inwards :— (i) From stations on "Down" side for the mileage to Golden Square.
(ii) „ „ „ "Up" side for the mileage to Golden Square, plus 3 miles.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
HORSHAM—						
Noske Flour Mills Pty. Ltd. Siding ...	‡	—	—	—	—	‡ Freight charges on traffic to or from the Siding shall be computed on the mileage to Horsham Station, plus ½ of a mile.
Ampol (Alba) Petroleum Pty. Ltd. Oil Discharging Site	203¼	6 0	12 0	—	—	
Caltex Oil (Aust.) Pty. Ltd's. Oil Discharging Site	203¼	6 0	12 0	—	—	
Shell Co. of Australia Site	203¼	6 0	12 0	—	—	
Commonwealth Oil Refineries Site ...	203¼	6 0	12 0	—	—	
Vacuum Oil Co. Pty. Ltd. Site ...	203¼	6 0	12 0	—	—	
Atlantic Union Oil Co. Ltd. Site ...	203¼	6 0	12 0	—	—	
Cortesi's Firewood Cutting Site ...	203¼	6 0	12 0	—	—	
IRREWARRA—						
Country Roads Board Siding ...	93½	—	—	—	—	Mileage rates shall be charged as to or from the Siding and in addition, the following charges shall be imposed :— (i) Where seventeen fully loaded trucks all of which are consigned in one direction from the Siding, are available for clearance therefrom by the engine employed to place the trucks thereat, no charge will be made for the placing of trucks by that engine ; (ii) Where seventeen fully loaded trucks all of which are consigned in one direction from the Siding, are not available for clearance therefrom by the engine employed to place the trucks thereat, a charge of 6s. 0d. for each 4-wheeled vehicle less than seventeen shall be made ; provided that the maximum charge in any such case shall be 48s. 0d. A special trip will not be made to the Siding for the purpose of placing trucks thereat unless at least nine fully loaded trucks, all

						consigned in one direction from the Siding, are available for clearance therefrom.; (iii) Where trucks are placed at the Siding by the engine of a passing train, a charge shall be made of 6s. 0d. for each 4-wheeled vehicle, and 12s. 0d. for each bogie vehicle, whether loaded or empty, placed on the Siding, with a minimum charge of 24s. 0d., and a maximum charge of 48s. 0d., for each occasion when such a service is performed.
IRYMPLE—						
Co-operated Dried Fruit Sales Pty. Ltd's. Siding	347¼	—	—	—	—	
Irymple Packing Co's. Siding ...	347¼	—	—	—	—	
Mildura Co-op. Fruit Co. Ltd. Siding ...	347¼	—	—	—	—	
Sleigh, H. C. Ltd. Oil Depot Site ...	347¼	6 0	12 0	12 0	—	
JEPARIT—						
H. Block's Oil Pipe Site	247¾	6 0	12 0	12 0	24 0	
Vacuum Oil Coy. Pty. Ltd.	247¾	6 0	12 0	—	—	
JEWELL—						
L. Partenio Sand Site	4	6 0	12 0	—	—	
Cornwall's Siding	4¼	—	—	—	—	
Hoffman and Co's. Siding	4¼	—	—	—	—	
Broons and Sons' Storage Site ...	4¼	6 0	12 0	—	—	
JUNG—						
State Rivers and Water Supply Commission's Storage Site	192	6 0	12 0	12 0	—	
KANIVA—						
Lawloit Shire's Oil Pipe Site ...	272½	6 0	12 0	12 0	—	When it is necessary for trucks to be specially placed by an engine at the site of the surface valve— The Council shall accept delivery of "Inwards" trucks and give delivery of "Outwards" trucks at a point in the Station Yard fixed by the Officer-in-Charge of the Station, and will be required to convey trucks to and from the site in question by means of horse or hand power, and then only with the permission and under the personal supervision of the Officer-in-Charge of the Station, whose instructions must be implicitly obeyed by the Council's employees performing the work. Trucks to be discharged at the site of the surface valve shall be placed only at such times as are suitable to the Commissioners, and, notwithstanding the fact that a truck has not been completely discharged, if Officer-in-Charge requires such truck to be removed from the said site, the Council will be bound to move the truck to such a position as the Officer-in-Charge may direct.
Shell Co. of Australia Ltd's. Oil Storage Site	272½	6 0	12 0	24 0	—	

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
KENSINGTON—						
Kimpton, W. S. & Son's Siding ...	‡2¼	—	—	—	—	‡ The same rates as to Melbourne shall be charged for *Inward Consignments* and unless otherwise specified, actual mileage rates from the Siding for Outward Consignments.
Younghusband Ltd. Siding	‡2¼	—	—	—	—	
KERANG—						
Kerang Flour Milling Co. Pty. Ltd. Siding ...	‡	6 0	12 0	12 0	24 0	‡ Freight charges shall be imposed on the mileage as to or from Kerang, plus ¼-mile.
(i) *Vacuum Oil Co. Pty. Ltd. Siding* ...	‡	—	—	—	—	‡ Freight charges shall be computed as follows :— (i) On the mileage to Kerang, plus ¼-mile. (ii) On the mileage to Kerang, plus ½-mile. (iii) On the mileage to Kerang, plus ¾-mile. (iv) On the mileage to Kerang, plus ¾-mile. (v) On the mileage to Kerang, plus ¾-mile. The charges shewn in Footnote (a) shall be imposed for each placing of trucks on the Siding, the time to be computed from when the engine leaves Kerang until its return thereto. If trucks for more than one siding holder are placed during the one operation, the shunting charge shall be borne proportionally by each Company, in accordance with the number of trucks placed for each Company.
(ii) *Shell Co. of Australia Ltd. Siding* ...						
(iii) *Commonwealth Oil Refineries Ltd. Siding*						
(iv) *Webb Industries Ltd. Siding* ...						
(v) *H. C. Sleigh Ltd. Siding* ...						
KEW—						
Firewood Siding Frontages	5¾	6 0	12 0	—	—	This charge shall be made for placing all trucks (either for loading or unloading), opposite sites on the siding frontages in the Goods Yard.
Department of Army Site	5¾	6 0	12 0	—	—	
KINGSTON—						
J. and R. Tyner's Siding	92¼	6 0	12 0	12 0	24 0	These shunting charges shall be imposed when trucks are placed by engine— If two trucks are ordered and only one is supplied, the minimum charge shall not be applied. If trucks hand-shunted from the public siding to the private siding by siding holder are subsequently removed by an engine, these

						shunting charges shall be imposed, but if the trucks that are hand-shunted into the Siding are removed therefrom with other trucks which were previously placed in the Siding by an engine, the charges imposed shall be subject so far as minimum and maximum charges are concerned to the total number of trucks so removed. For example :—If three trucks are placed by an engine and one hand-shunted by the siding holder into the Siding and the four trucks removed at the same time by engine, the minimum charge of 12s. 0d. will not apply in respect of the truck which was hand-shunted into the Siding : for this truck, the rate of 6s. 0d. only shall be imposed. Trucks hand-shunted from public siding to private siding and subsequently hand-shunted back to public siding shall not be subject to shunting charges.
KNOWSLEY—						
Bywater's Siding ...	83	—	—	—	—	
KOONDROOK—						
Arbuthnot Saw Mills Siding	193¾	6 0	12 0	12 0	—	
Boundy & Hickey's Firewood Site ...	193½	6 0	12 0	12 0	—	
KOO-WEE-RUP—						
Plowright Albion Koo-wee-rup Washed Sand and Gravel Co. Pty. Ltd. ...	42	6 0	12 0	12 0	24 0	These shunting charges shall be applied as under :— (i) No shunting charge shall be imposed for removing trucks from Siding by engine for trucks previously placed by engine and on which a shunting charge was imposed. (ii) Trucks hand-shunted to the Siding, and subsequently removed by engine shall be charged for. (iii) For trucks placed and removed by engine power, siding holders must accept and give delivery with last truck in and, first truck out, between Notice Board and Scotch Block. Outwards trucks to be coupled together, doors closed and secured, and as far as practicable, loaded in station order ready for a straight pick-up. (iv) Trucks hand-shunted from the public siding to the private siding, and subsequently hand-shunted back to public siding shall not be subject to shunting charges, but no trucks shall be moved from or to the private siding by hand until authority is obtained from the Stationmaster, Koo-wee-rup. (v) The prescribed shunting charges shall not be imposed as long as the Company accepts delivery of empties and gives delivery of loaded trucks in No. 4 Road.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
KOO-WEE-RUP—*continued.*						
Plowright Albion Koo-wee-rup Washed Sand and Gravel Company Pty. Ltd. Sand Loading Site	42	6 0	12 0	12 0	24 0	For each placing of trucks at, or removing of trucks from, the site by engine power. No shunting charge shall be imposed for removing trucks from the site by engine power if such trucks were previously placed by engine power and a placing charge was imposed for the service.
Permewan Wright and Co's. Shed ...	42	6 0	12 0	12 0	—	
Commonwealth Fertilizers and Chemicals Ltd. Shed Site	42	6 0	12 0	12 0	—	
Supply & Development Dept's. Site (Flax Production Committee)	42	6 0	12 0	12 0	—	
Barker, Green & Parke Ltd's. Shed Site ...	42	6 0	12 0	12 0	24 0	
KORONG VALE—						
Australian Wheat Board's Wheat Stacking Sites	‡	10 2	20 4	—	—	‡ 1. Freight charges shall be computed on the mileage to or from Korong Vale. 2. These shunting charges shall be imposed for placing Inwards loaded trucks at the site or for removing Outwards loaded trucks from the site and such charges shall be included on all waybills. 3. When an engine is utilised to perform shunting within the storage area, after having placed trucks at the usual receiving point, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine commences such shunting work until it is completed.

KORUMBURRA—						
Sunbeam Colliers Pty. Ltd. Siding ...	70¾	—	—	—	—	Traffic shall be charged as follows :— Inwards—Tariff rates to the *Siding, plus shunting charges.* Outwards—Tariff rates from the Siding, plus 2s. 1d. per ton haulage charge, coal excepted, which shall be charged freight on the mileage from the Siding. Traffic between the Siding and Korumburra shall be charged 1—10 mile rate, plus shunting charges for Inwards traffic or 2s. 1d. per ton haulage for Outwards traffic.
Dead-End Siding	71¾	6 0	12 0	—	—	
Rowe's Shed Site	69¾	6 0	12 0	12 0	—	
KURTING—						
W. Fry's Firewood Cutting Site ...	135	6 0	12 0	—	—	
KYABRAM—						
Kyabram Co-op. Fruit Preserving Co's. Siding	124	6 0	12 0	—	24 0	
Southern Can Co. (Aust.) Pty. Ltd's. Siding	124	6 0	12 0	12 0	24 0	
Curnow and Son's Shed Site	124	6 0	12 0	12 0	24 0	
KYNETON—						
Shell Co. of Aust. Ltd., and Vacuum Oil Co. Pty. Ltd.	57	6 0	12 0	12 0	24 0	When trucks for both Companies are placed at the one time, the charges shall be debited to each Company proportionately.
R. M. Watson's Siding	57	6 0	12 0	24 0	48 0	
Willis Bros. Flour Mill Siding ...	57	6 0	12 0	24 0	48 0	
LA LA—						
R. Inverarity's Shed Site	49	6 0	12 0	12 0	24 0	
Melbourne and Metropolitan Board of Works Siding	49	6 0	12 0	12 0	24 0	
W. P. Wilson and Sons Storage Site ...	49	6 0	12 0	12 0	24 0	
LAKE BOGA—						
Lake Boga Grain Association's Shed ...	205	6 0	12 0	12 0	24 0	
LALBERT—						
Vacuum Oil Co. Pty. Ltd's. Oil Discharging Site	205¾	6 0	12 0	—	—	
LAL LAL—						
R. Fowler Ltd's. Loading Site	78½	6 0	12 0	12 0	—	
H. A. Green's Loading Site	78½	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
LANG LANG—						
Australian Glass Manufacturers Ltd's. Siding	‡	—	—	—	—	1. ‡ This Siding is situated between Lang Lang and Nyora and the freight charges shall be computed on the mileage from Melbourne, 53¾ miles, plus ¼-mile. 2. The Siding will be worked by Up trains only and, in addition to the freight charges, the charge shewn in footnote (a) shall be imposed for the time the engine is engaged for each placing of trucks on, or clearance of trucks from the Siding ; time to be computed from when the engine arrives at the points giving access to the Siding until its return to the train on completion of shunting operations.
LANGWARRIN—						
Langwarrin Poultry Supply Co's. Shed Site	30¾	—	—	—	—	Trucks will not be specially placed at the shed site.
LAVERTON—						
Defence Department's Aircraft Siding ...	‡	6 0	12 0	12 0	24 0	‡ Freight charges shall be computed on the mileage to or from Laverton plus 1¼ miles.
LEONGATHA—						
Knox Siding	80½	—	—	—	—	
Placing trucks on any particular portion of the Back Road a/c Various Persons ...	79	6 0	12 0	12 0	24 0	
Commonwealth Oil Refineries Ltd's. Siding	79	6 0	12 0	24 0	48 0	If the placing of trucks at both Sidings be done at the same time, placing charges shall be borne proportionately by each, in accordance with the number of trucks placed at each Siding.
Shell Co. of Australia Ltd. Siding ...	79	6 0	12 0	24 0	48 0	
Vacuum Oil Co. Pty. Ltd. Site ...	79	6 0	12 0	24 0	48 0	
J. A. McIndoe, L. M. Timmins, and R. Cross' Storage Site	79	6 0	12 0	12 0	24 0	
LETHBRIDGE—						
C. Nash and Son Pty. Ltd's. Siding ...	62¾	—	—	—	—	

LILLIMUR—						
Shell Co. of Australia Ltd. Oil Discharging Site	278¾	6 0	12 0	24 0	—	
LILYDALE—						
Forests Commission of Victoria Firewood Cutting and Storage Site	24⅛	6 0	12 0	—	48 0	
Cave Hill (D. Mitchell's Estate) Siding ...	24⅛	—	—	—	—	
Postal Dept's. Pole Storage Site ...	24⅛	6 0	12 0	—	—	
LINGA—						
Imperial Chemical Industries of Aust. & New Zealand Ltd. Loading Ramp Site	326¼	6 0	12 0	12 0	24 0	
LINTON—						
E. B. Cochran and G. W. Walker's Firewood Cutting Site	98¾	6 0	12 0	—	—	To be imposed when trucks are placed at the site by engine, or shunting is involved in having trucks in position on the goods loop siding so that they can be placed by horse.
LINTON JUNCTION—						
Australian Wheat Board's Wheat Stacking Site	76⅛	6 0	12 0	—	—	1. These shunting charges shall be imposed for placing Inwards loaded trucks at the site or removing Outwards loaded trucks from the site and shall be included on all waybills. 2. When an engine is utilised to perform shunting within the storage area, after having placed trucks at the usual place, or when a Special Pilot engine is run from Ballarat to place or remove trucks or to perform shunting movements within the area, the charges shown in footnote (a) shall be imposed for the time that the engine is engaged ; the time to be computed from when the engine leaves Ballarat until its return thereto.
LITCHFIELD—						
McBean's Departmental Shed	192	6 0	12 0	—	—	
MACAULAY—						
Victorian Producers' Co-op. Co's. Siding	‡2	6 0	12 0	—	—	1. ‡ Freight shall be computed on the mileage to or from Macaulay. *Exception.*—Wool forwarded to the Siding shall be charged the same rate as from the forwarding station to the Melbourne Goods Sheds. 2. **Outwards.**—Consignments weighing less than 3 tons shall be accepted for despatch from the Siding on any day when an engine is working there. In addition to the freight and siding charges, a special charge of 6s. 0d. per ton, minimum 12s. 0d. per truck, shall

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (mimimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
MACAULAY—*continued.*						be imposed for the unloading and distribution at the Melbourne Goods Sheds. Exception.—This special charge shall not be imposed on any consignment weighing in the aggregate 3 tons or over forwarded from the Siding in the same truck to different consignees at different destination stations. 3. **Inwards.**—Hides and Skins for the Company shall be waybilled to Melbourne. Wool (in any quantity) and grain, including wheat (in truck loads), may be accepted for the Siding and waybilled to Macaulay, see footnote (f).
Elder Smith & Co. Ltd.	‡2	6 0	12 0	—	—	1. ‡ Freight shall be computed on the mileage to Macaulay. Exception.—Wool forwarded to the Siding shall be charged the same rate as from the forwarding station to the Melbourne Goods Sheds. 2. **Inwards.**—Goods consigned and addressed to the Company at Macaulay shall be waybilled thereto, see footnote (f).
Goldsborough Mort & Co. Ltd's. Siding	2	6 0	12 0	—	—	Freight shall be computed on the mileage to Macaulay. Open only for hides, sheep skins and tallow which are to be waybilled to Macaulay see footnote (f). Furred skins must be waybilled to Melbourne.
Goldsborough Mort & Co. Ltd's. Siding a/c Vict. Wheatgrowers' Corporation Ltd.	—	—	—	—	—	The Victorian Wheatgrowers' Corporation Ltd. has been granted permission to use this Siding for two trucks per day during the months April to November, and five trucks per day December to March. The same conditions as those applicable to Goldsborough Mort & Co. shall apply for traffic placed on this Siding account the Victorian Wheatgrowers' Corporation.

Younghusband's Ltd. Siding	‡2	6 0	12 0	12 0	—	‡ 1. Freight shall be computed on the mileage to or from Macaulay. Exception : Wool forwarded to the Siding shall be charged the same rate as from the forwarding station to the Melbourne Goods Sheds. 2. Goods consigned and addressed to the Company at Macaulay shall be waybilled thereto (see footnote (f)).
MACLEOD—						
Mont Park	12¼	6 0	12 0	24 0	—	Open for goods in any quantity for Public Works Department and Hospital for Insane only.
MAFFRA—						
Public Works Dept. Dehydration Factory	132¼	6 0	12 0	12 0	48 0	
Maffra Co-op. Milk Products Co's. Siding	132¼	6 0	12 0	—	—	
Cereal Processors (Victoria) Pty. Ltd's. Siding	132¼	6 0	12 0	24 0	48 0	
Commonwealth Milk Products Pty. Ltd. Oil Discharging Site	132¼	6 0	12 0	12 0	24 0	
Nestle's Food Specialities (Australia) Ltd.	132¼	6 0	12 0	—	—	
Nestle's Food Specialities (Australia) Ltd. for Boiler House	132¼	6 0	12 0	—	—	
MALVERN—						
State Electricity Commission's Siding ...	6½	6 0	12 0	—	—	
MANANGATANG—						
Vacuum Oil Coy's. Discharging Site ...	259¾	6 0	12 0	—	—	
MANGALORE—						
I. Brombal and P. Munari Firewood Cutting Site	68	6 0	12 0	—	—	
Department of Army Siding	‡	—	—	—	—	‡ 1. Freight charges shall be computed on the mileage as to or from Mangalore, plus ½ mile. In addition to the freight charges, the shunting charges shewn in footnote (a) shall be imposed, time to be computed :— (i) When done by a passing train—From when the engine leaves Mangalore until its return thereto. (ii) When done by a Pilot engine from Seymour—From when the engine leaves Seymour, until its return thereto.

(f) Unless otherwise specified, full truck loads are to be forwarded direct to Macaulay but consignments in less than full truck loads are to be loaded with other non-perishable goods into trucks carded "Melbourne and Transfers." In addition to the freight and siding shunting charges, a special charge of 6s. 0d. per ton, minimum 12s. 0d. per truck, shall be imposed for assembling and reloading small consignments into truck loads at Melbourne. Goods Superintendent shall raise debit for the assembling and reloading and render account for these services to the Company.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charge, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage rom Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
MANSFIELD—						
Phosphate Co-op. Co. of Australia Siding	129½	6 0	12 0	—	—	Provided that not less than ten loaded trucks are made available for clearing at any one time, a Special train will be employed for placing empties and clearing loaded trucks, for which service the shunting charges specified will not be imposed ; but in the event of a less number than ten vehicles, the shunting charges specified shall apply.
Phosphate Co-op. Co. of Australia Siding a/c Delatite Sawmills Ltd.	129½	6 0	12 0	—	—	
MARMALAKE—						
Australian Wheat Board's Bulk Wheat Storage Sites	184¾	10 2	20 4	—	—	To be imposed for placing Inwards loaded trucks at the site or removing Outwards loaded trucks from the site ; such shunting charges to be included on waybills.
MARYBOROUGH—						
Firewood Cutting Sites— ... ... *a/c J. C. Tranter* *a/c S. B. Parsons* *a/c Maffescioni Bros.*	‡	6 0	12 0	12 0	24 0	‡ Freight charges shall be computed on the mileage as to or from Maryborough, plus ¾ mile, for J. C. Tranter's Site; and plus ½ mile for S. B. Parsons' site and Maffescioni Bros. site. In addition to the freight charges, the following shunting charges shall be imposed :—
		—	—	—	—	1. These shunting charges shall be imposed when the work is performed by a Pilot engine engaged in other work at Loco. Depot or Storage Siding for each placing of trucks at the site. 2. When a special trip by an engine from Maryborough Yard is necessary to place at or remove trucks from the site, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves the Maryborough Yard until its return thereto.
Maryborough District Co-op. Butter Factory Co. Ltd. Siding	112	6 0	12 0	—	24 0	

Maryborough Gas Works	112¼	—	—	—	—	
Maryborough Flour Mills Pty. Ltd. Siding	112	6 0	12 0	12 0	24 0	
Shell Co. of Aust's. Site	112	6 0	12 0	24 0	—	
Vacuum Oil Co. Pty. Ltd.	112	6 0	12 0	12 0	24 0	
MATHOURA—						
Douglas Bros.' Siding	169¼	6 0	12 0	—	—	
MELBOURNE—						
Victoria Dock	...	...	...	...	...	See Volume 1, page 184.
Piggott Street	...	...	...	...	...	
Straw Siding and Cowper Street ...	...	...	...	...	...	
Harbour Trust Crane Siding	...	...	...	...	...	
Montague Shipping Shed	...	...	...	...	...	
MELTON—						
H. S. K. Ward Pty. Ltd. Siding ...	23¼	—	—	—	—	
Wright Stephenson & Co. (Aust.) Pty. Ltd. Siding	23¼	6 0	12 0	12 0	24 0	To be imposed whenever it is necessary for trucks to be specially sorted out for the Company.
MENTONE—						
State Electricity Commission's Briquette Storage Site	15¼	6 0	12 0	12 0	24 0	
MERBEIN—						
Mildura Co-op. Fruit Co. Ltd. Siding ...	358¼	—	—	—	—	
Aurora Packing Co. Pty. Ltd. Siding ...	358¼	—	—	—	—	
Irymple Packing Co. Siding	358¼	—	—	—	—	
MERRIGUM—						
Carnation Co. Pty. Ltd. Siding ...	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Merrigum, plus ¼ mile. The charges shewn in footnote (a) shall be imposed for the time the engine is engaged in placing trucks on the Siding, the time to be computed from when the engine commences the service until its return to the originating point. No charge is to be made for clearing the trucks from the Siding.
MIDDLE BRIGHTON—						
State Electricity Commission's Siding ...	9¼	6 0	12 0	12 0	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
MILDURA—						
R. M. Anderson Sawmills Pty. Ltd. ...	351¼	—	—	—	—	In addition to the freight the charges shewn in footnote (a), shall be imposed for the time the engine is engaged in placing trucks on the Siding; time to be computed as follows :— (a) when placed by engine of a passing train—from when the engine arrives at the points on the Main line giving access to the Siding until its return thereto; (b) when placed by Pilot engine from Mildura—from when the engine leaves Mildura Yard until its return thereto.
Postal Department's Siding	351¾	6 0	12 0	12 0	—	
Shell Co. of Aust. Ltd., Vacuum Oil Co. Pty. Ltd. and Atlantic Union Oil Coy. Siding	352	—	—	—	—	In addition to the freight the charges shewn in footnote (a), shall be imposed for the time the engine is engaged; time to be computed from when engine departs from Mildura Yard until its return thereto. If trucks for Shell Co. of Aust. Ltd., Vacuum Oil Co. Pty. Ltd. and Atlantic Union Oil Coy. Ltd. are placed on the Siding during the one operation, the total charge shall be apportioned according to the number of trucks placed for each Company.
Silk Bros. Shed Site	351¼	6 0	12 0	—	—	
Mildura Co-op. Fruit Co's. Ltd. Siding ...	350½	6 0	12 0	12 0	24 0	
Mildura Co-op. Fruit Co's. Ltd. Siding a/c Caltex Oil (Aust.) Pty. Ltd.	350½	6 0	12 0	12 0	24 0	
Commonwealth Oil Refineries Siding ...	351¼	6 0	12 0	24 0	48 0	
Mildura Shire Council's Siding ...	351½	6 0	12 0	12 0	24 0	
Mildura City Council's Siding ...	351½	6 0	12 0	12 0	24 0	
Mildura City Council (Elecrtic Supply Dept's Siding)	351¼	6 0	12 0	24 0	48 0	
Mildura City Council's Pole Storage Site	351¼	6 0	12 0	24 0	—	
Mildura Box Factory Casemaking Site ...	351¼	6 0	12 0	—	24 0	

Construction Branch Siding	351¾	6 0	12 0	48 0	—	No charge shall be imposed for placing trucks for the loading of wool from River Steamers. The Siding is ordinarily open for consignments forwarded to or from Construction Branch only.
River Frontage Sidings	351¼	6 0	12 0	12 0	24 0	
Clayton's Shed Site	351¼	6 0	12 0	—	—	
First Mildura Irrigation Trust Oil Discharging Site	351¼	*	*	*	—	* Minimun 24s. 0d. for the placing of each truck at the site.
MINYIP—						
Dunmunkle Shire's Siding	198¼	6 0	12 0	12 0	24 0	To be imposed for trucks, whether loaded or empty, placed clear of points leading to Mill Siding.
Loading Sites on Dunmunkle Shire's Siding	198¼	6 0	12 0	—	—	
Vacuum Oil Co. Pty. Ltd. Oil Depot Site ...	198¾	6 0	12 0	—	—	
MITCHAM—						
Builders' Roofing and Trading Co's. Siding	14¼	6 0	12 0	—	—	
Australian Tesselated Tile Co. Pty. Ltd. Siding	14¼	—	—	—	—	
MOAMA—						
N. Rogan & Son Grain Store	146½	6 0	12 0	—	—	
MOE—						
Christensen and Saxton's Siding ...	81¼	—	—	—	—	
J. L. Higgins' Firewood Cutting Site ...	81¼	6 0	12 0	—	—	
MOOROODUC—						
T. E. Butler's Siding	35¾	6 0	12 0	12 0	24 0	To be imposed for trucks placed at "Up" end of the Public Loop Siding.
MOOROOPNA—						
McLennan and Co.	110	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
MOOROOPNA—*continued.*						
Ardmona Fruit Products Co-op. Co's. Siding (The Blue Moon Fruit Co-operative Ltd. and the British Australasian Tobacco Co. Pty. Ltd. have permission to use this siding subject to these siding charges and conditions.)	110	6 0	12 0	12 0	24 0	These shunting charges shall be imposed :— (1) For placing trucks by engine power in the Company's No. 3 dead-end Siding. (2) For removing trucks by engine power from the Company's No. 3 dead-end Siding in instances where the trucks were placed by the Company by horse power. As Inwards loading for the Company is usually placed on Public Siding No. 5 from where the Company horse-shunt trucks into their Private Siding. Inwards consignments, which comprise principally full truck loads of empty tins and small lots of canned products, returned for retreatment, shall not be subject to the minimum of three tons.
Northern Pear Growers Ltd. Siding ...	‡	—	—	—	—	‡ Freight charges shall be computed at tariff rates, on the mileage to or from Mooroopna, plus ¾ mile. The Siding will be worked by passing train or by Pilot trip from Mooroopna and, in addition to the freight the charges shown in footnote (a) shall be imposed for the time the engine is engaged in placing trucks on the Siding, in the case of a passing train, or in placing trucks on and/or clearing trucks from the Siding in the case of a Pilot engine from Mooroopna ; time to be computed as follows :— (i) In the case of a passing train, when placing trucks on the Siding—from when the engine arrives at the points giving access to the Siding until its return to the Main line, and (ii) In the case of a Pilot engine from Mooroopna, when placing trucks on or clearing trucks from the Siding—from when the engine leaves Mooroopna station until its return thereto.

MORDIALLOC—						
Mordialloc Timber Co. Pty. Ltd. ...	17½	6 0	12 0	12 0	24 0	
MORELAND—						
Brunswick Plaster Mills Pty. Ltd. ...	5½	6 0	12 0	—	—	For placing trucks at the "Up" end of "A" Siding clear of points to Moreland Timber Co's. Siding. Trucks will be sorted out, when necessary, by Moreland Timber Co.
Moreland Timber Co.	5½	6 0	12 0	—	—	
Moreland Grain and Free Stores ...	5½	—	—	—	—	
State Electricity Commission	5½	—	—	—	—	
MORNINGTON—						
Vacuum Oil Co. Pty. Ltd. Siding ...	‡	—	—	—	—	‡ Freight shall be computed on the mileage to or from Mornington, plus ¾ of a mile. The Siding shall be worked by engine from Mornington on days and at times suitable to the Department. In addition to freight, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged working the Siding; time to be computed from when the engine leaves Mornington until its return thereto.
Defence Department's Siding	40¼	6 0	12 0	—	24 0	
Fibre Processors (Aust.) Pty. Ltd. Storage Sites at Loco Sheds and Goods Sheds.	40¼	6 0	12 0	12 0	24 0	
MORTAT—						
F. Sheehan's Firewood Cutting Site ...	251¼	6 0	12 0	—	—	
MORWELL—						
State Electricity Commission's Storage Site	90	6 0	12 0	12 0	24 0	
Australia Paper Manufacturers Ltd's. Sdg. (Maryvale)	92¼	6 0	12 0	12 0	24 0	To be imposed when empty and/or loaded trucks are placed at Maryvale by an ordinary Roadside Goods train. No charge will be made for trucks cleared from the Siding by an ordinary Goods train. When a Special trip is run from Morwell or Traralgon, either to place empty or loaded trucks or to clear such trucks from Maryvale, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged; time shall be computed from when the engine leaves Morwell or Traralgon until its return thereto.
MOULAMEIN—						
Mrs. C. Geron's Firewood Cutting Site ...	231½	6 0	12 0	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
MULWALA—						
Mulyarra—Ministry of Munitions Siding	‡	—	—	—	—	‡ 1. Freight shall be computed on the mileage to or from the Main line points leading to the Siding, 163 miles from Melbourne, plus 1¼ miles, subject to the following conditions :— (i) Between Mulyarra–Melbourne, the Melbourne–Mulwala rates shall apply as a maximum. (ii) Between Mulyarra and Suburban stations at rates shown in subclause (i) subject to District Rates conditions. (iii) Between Mulyarra and other Victorian stations at the mileage to and from the Siding. 2. In addition to the freight charges, the following charges shall be imposed :— (i) When the Siding is worked by a passing train from Oaklands, the charges shewn in footnote (a) shall be made ; time to be computed from when the engine arrives at the Main line points giving access to the Siding until its release therefrom ; (ii) When the Siding is worked by a Pilot Engine from Yarrawonga, the charges shewn in footnote (a) shall be imposed,; time to be computed from when the engine leaves Yarrawonga until its return thereto.
MURCHISON EAST—						
Defence Department's Siding	91½	6 0	12 0	12 0	24 0	
Hammond and Son's Siding	96¼	—	—	—	—	
MURRABIT—						
Murrabit Packing Co. Pty. Ltd's. Shed Site	194½	6 0	12 0	—	—	
MURRAYVILLE—						
Manure Sheds Sites	356¾	6 0	12 0	12 0	—	

MURRUMBEENA—						
Plowright Albion Koo-wee-rup Washed Sand & Gravel Co. Pty. Ltd.	9¼	6 0	12 0	—	—	
MURTOA—						
Melbourne Machinery Co.	185¼	—	—	—	—	The charges shewn in footnote (a) shall be imposed for each placing of the trucks on the Siding ; time to be computed from when the engine leaves Murtoa until its return thereto.
Thomas, W. C., and Sons Pty. Ltd. Siding	185¼	—	—	—	—	
Dunmunkle Shire's Oil Discharging Site ...	185¼	—	—	—	—	
NAGAMBIE—						
Caelli, L., Site	78¼	—	—	—	—	
McLeod's Site	78¼	—	—	—	—	
PeHifer's Site	78¼	—	—	—	—	
NARRE WARREN—						
G. R. Bailey's Storage Site	25½	6 0	12 0	—	—	To be imposed when necessary to specially sort out and place trucks at the site.
NATHALIA—						
W. T. Ross	147¾	6 0	12 0	12 0	24 0	
Shed Sites	147¾	6 0	12 0	—	12 0	
L. C. Ferguson's Firewood Cutting Site ...	147¾	6 0	12 0	—	—	
J. A. Darlow Firewood Site	147¾	6 0	12 0	—	—	
NATIMUK—						
Natimuk Electric Supply Oil Discharging Site	218¾	6 0	12 0	24 0	—	
Vacuum Oil Co. Pty. Ltd. Oil Discharging Site	218¾	6 0	12 0	24 0	—	
NAYOOK—						
Limberlost Lumber Mills Pty. Ltd. Siding	83¾	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
NEWLYN—						
Tinney Bros & Hanrahan Pty. Ltd's. Shed Site	90¾	6 0	12 0	12 0	24 0	
Tinney Bros. & Hanrahan Pty. Ltd's. Siding	90¾	6 0	12 0	12 0	24 0	
Stewart and Co's. Shed Site	90¾	6 0	12 0	12 0	24 0	
Boustead and Mitchell's Site	90¾	6 0	12 0	12 0	24 0	
Placing Trucks on Back Road, a/c Various Persons	90¾	6 0	12 0	12 0	—	
NEWMARKET—						
Dalgety and Co.	‡2¾	—	—	—	—	‡ The same rates as to Melbourne shall be charged for Inwards consignments and, unless otherwise specified, actual mileage rates for Outwards consignments.
Royal Agricultural Show Grounds Siding	3¾	—	—	—	—	Open for Show Exhibits during the Royal Agricultural Show only; at other times, Inwards and Outwards consignments for this Siding must not be accepted until the authority of the Chief Traffic Manager, and direction re Special placing charges to be enforced, have been obtained.
Flemington Racecourse	3¾	—	—	—	—	Inwards and Outwards consignments for the Racecourse must not be accepted until the authority of the Chief Traffic Manager, and direction re Special placing charges to be enforced, have been obtained.
W. J. Stacey's Sand Storage Site ...	2¾	6 0	12 0	12 0	24 0	
Tancred Bros. Pty. Ltd.	2¾	6 0	12 0	12 0	24 0	

NEWPORT—						
South Brooklyn (Borthwick Freezing Co's. Siding)	8¾	—	—	—	—	1. Freight charges to or from these Sidings shall be computed on the shortest mileage via Newport or via Sunshine, see page 114. This provision applies to live stock only for Austral, see clause 4.
Little Brooklyn	9	—	—	—	—	
North Brooklyn (N. S. Smorgan and Sons' Siding)	9¼	—	—	—	—	2. The following special charges shall be imposed :— (i) Trips run by Ordinary trains on week days—nil. (ii) Special trips from Sunshine or Newport, as the case may be, run for ten or more trucks of stock on week days or Sundays, or Special Holidays—nil.
Prossor (Western and Murray Co-op. Bacon Co.)	9¼	—	—	—	—	(iii) Special trips from Sunshine or Newport, as the case may be, run for less than ten trucks of stock on week days—114s. 5d.
Austral (Austral Meat Works) Sims Cooper Pty. Ltd.	7½	—	—	—	—	(iv) Special trips from Sunshine or Newport, as the case may be, run for less than ten trucks of stock on Sundays or Special Holidays—170s. 10d.
						3. Special rates apply for the carriage of fresh or frozen meat, see Volume 1, page 156.
						4. Traffic forwarded to Austral shall be charged on the mileage computed via Footscray or Laverton except that live stock, when cheaper, shall be charged via the Newport–Sunshine Loop.
						Gates have been erected across the line at the entrance to Messrs. Borthwick Freezing Co's. Siding and on any occasion on which an engine engaged in placing of trucks beyond the gates is delayed by the gates not being opened promptly, a detention charge as shewn in footnote (a) shall be imposed for the time that the engine is delayed.
						On any occasion on which an engine engaged in the operation of traffic to or from the Austral Meat Works is delayed as a result of the gate at Challis Street crossing being closed across the railway track, a detention charge of 30/10 for the first 15 minutes and 9/4 for each 5 minutes or part thereof beyond the first 15 minutes shall be imposed for the time the engine is delayed.
Ready Mixed Concrete Ltd.	‡9¼	6 0	12 0	12 0	24 0	‡ Freight shall be computed on the shortest mileage to or from the Siding. See page 114.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
NEWPORT—*continued.*						
Forest Commission Siding	6¾	—	—	—	—	
Wright Stephenson & Co. (Aust.) Pty. Ltd.	‡8	6 0	12 0	12 0	24 0	‡ Freight shall be computed on the shortest mileage to or from the Siding. See page 114. The shunting charges shewn shall be imposed only if placing be done by ordinary Pilot between Newport and Sunshine. If placing be done by Special Pilot trip, the charge shewn in footnote (a) shall be imposed for the time the engine is engaged for each placing of trucks on the Siding, time to be computed from when the engine commences the Special service until its return to the same point.
Departmental Sub-station Siding a/c Forests Commission	6¾	6 0	12 0	12 0	24 0	
Commonwealth Oil Refineries Siding, Kororoit Creek see clause 4 re Rowden and Son	9½	—	—	—	—	The freight shall be computed on the actual mileage to or from the Siding, subject to the following conditions :— 1. *Traffic in less than Full Train Loads Operated by Ordinary Goods Trains*—The charges shewn in footnote (a) shall be imposed for the time during which the engine of any such train is detained at the Siding for the purpose of handling such traffic. 2. *Traffic in less than Full Train Loads Operated by a Special Engine from Newport*—The charges shewn in footnote (a) shall be imposed for the time occupied by the engine handling such traffic in proceeding to the Siding, working thereat, and returning therefrom ; time to be computed from when the engine leaves Newport until its return thereto. 3. *Traffic in Full Train Loads*—The charges shewn in footnote (a) shall be imposed for the time during which the engine handling such traffic is detained at the Siding.

..						4. The same charges as those provided for the Commonwealth Oil Refineries Siding shall apply to traffic placed on the Siding for Rowden & Son. Where trucks for the Commonwealth Oil Refineries and Rowden & Son are placed at the one time, the charges for placing shall be borne proportionately by each, in accordance with the number of trucks placed.
..						
State Electricity Commission Siding, Kororait Creek	9	—	—	—	—	1. *Traffic in less than Full Train Loads Operated by Pilot Trip from Newport*—The charges shewn in footnote (a) shall be imposed for the time the engine is engaged ; time to be computed from when the engine leaves Newport until its return thereto. 2. *Traffic in Full Train Loads with a Minimum Load of 33 Four-wheeled Trucks or equivalent thereof*—The charges shewn in footnote (a) for the time the engine is engaged shunting at the Siding. When the Siding is operated by Pilot trip from Newport and the engine also works at Commonwealth Oil Refineries' Siding, the charges shall be borne proportionately by each siding holder or user, in accordance with the number of trucks placed or cleared for each party concerned.
Departmental Old Metal Siding, a/c Forest Commission	6¾	6 0	12 0	12 0	24 0	
Caltex Oil (Aust.) Pty. Ltd. Siding ...	‡7	6 0	12 0	—	—	‡ See page 114.
Thomas, W. C., and Sons Pty. Ltd. Siding	‡7	—	—	—	—	‡ See page 114.
McKenzie and Holland (Aust.) Pty. Ltd. ...	‡7½	—	—	—	—	‡ See page 114.
Newport–Sunshine Loop Sidings ...	—	—	—	—	—	For mileages to Sidings from Newport, Sunshine and Broadmeadows, see page 114.
NHILL—						
Vacuum Oil Co. Ltd. Oil Discharging Site	248¼	12 0	—	12 0	—	
Noske Flour Mills Pty. Siding	248¼	6 0	12 0	—	12 0	
,, ,, ,, ,, ,, *a/c Co-op. Wheat Group*	248¼	6 0	12 0	12 0	12 0	
Lowan Shire's Siding	248¼	6 0	12 0	24 0	48 0	
Lowan Shire's Oil Discharging Site off Shed Road	248¼	6 0	12 0	12 0	—	
NOOJEE—						
Noojee Extension	90¾	10 3	20 6	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
NOOJEE—*continued.*						
Kauri Timber Coy. Ltd. Siding ...	89¾	6 0	12 0	12 0	—	These shunting charges shall be imposed when less than 6 trucks are placed. No shunting charge shall be imposed when 6 or more trucks are placed on the Siding at the one time.
Smith Bros. Pty. Ltd.	89¾	6 0	12 0	12 0	—	
Goodwood Siding	89¾	6 0	12 0	12 0	—	
NORTH CARLTON—						
Crane Site	4¼	6 0	12 0	12 0	—	To be imposed for sorting out and placing trucks at the Crane site.
Brown and Dureau's Site	4¼	6 0	12 0	12 0	—	
A. J. Gibson and Son Pty. Ltd's. Timber Storage Site	4¼	6 0	12 0	12 0	—	
NORTH FITZROY—						
A. C. Carpenter Timber Storage Site ...	5	10 3	20 6	—	—	
E. R. McKellar Fuel Storage Site ...	5	6 0	12 0	12 0	24 0	
B. Rosenbaum's Site	5	6 0	12 0	—	—	
NORTHCOTE—						
Northcote Brick Co's. Siding ...	5¾	—	—	—	—	
NORTH GEELONG—						
Vacuum Oil Co's. Siding	43½	6 0	12 0	24 0	48 0	See footnote (g).
Shell Co. of Aust. Ltd's. Siding ...	‡	6 0	12 0	24 0	48 0	‡ Mileage as to or from North Geelong, plus 1 mile, shall be charged. See footnote (g).
Commonwealth Oil Refineries Siding ...	43½	6 0	12 0	24 0	48 0	See footnote (g).

Corio Quay—						
Geelong Harbour Trust's Siding No. 1	‡	6 0	12 0	18 0	—	‡ Freight charges shall be computed on the mileage to or from North Geelong, plus one mile. See footnote (g).
,, ,, ,, ,, *No. 2*	‡	6 0	12 0	18 0	—	
,, ,, ,, ,, *No. 3*	‡	6 0	12 0	18 0	—	
,, ,, ,, ,, *a/c Sims, Cooper Pty. Ltd. Freezing Works*	‡	6 0	12 0	18 0	—	‡ 1. Freight charges shall be computed on the mileage to or from North Geelong, plus one mile. See footnote (g). 2. These shunting charges are to be imposed on all traffic other than live stock. 3. The following shunting charges shall apply to :— (i) Special trips with live stock from North Geelong to the Siding, which shall be subject to a minimum number of fifteen trucks per Special trip, and for every truck short of fifteen per trip, an additional charge of 2s. 1d. per truck shall be made. (ii) Special trips with empties, or to remove empties, engine running light one way, 30s. 10d. per trip to be charged. (iii) Not more than 30 minutes' free time for the engine shall be allowed at the works for traffic mentioned in clauses (i) and (ii), and any additional time incurred shall be charged for at 26s. 3d. per half-hour or part thereof, and if one trip be required to place the trucks at the Siding, and another trip to remove them, both trips shall be paid for. Twenty minutes shall be allowed for discharging each truck of live stock. 4. If the empty trucks be required so urgently that the time specified cannot be given, and it is necessary for a Special trip to be run to the Freezing Works for empty trucks before sufficient time has been allowed to discharge the full number on hand there, the trip so run shall not be charged for unless all the trucks that were loaded in are removed. This means that, providing the trucks are discharged within the time allowed, only one trip for removing empties shall be charged for. Further, the Special charge for removing empties shall not be imposed if, within 24 hours after the loaded trucks be placed, another trip is run to place more loaded trucks.

(g) Freight on consignments forwarded between Melbourne and suburban stations, and the siding, shall be computed in accordance with the District rates between Melbourne and Geelong and North Geelong.

Shunting Charges, etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
NORTH GEELONG—*continued.*						
Corio Quay—continued.						
Cresco Fertilizer's Ltd. Siding ...	‡	6 0	12 0	18 0	—	‡ Freight charges shall be computed on the mileage to or from North Geelong, plus one mile. See footnote (g).
H. C. Sleigh Ltd.	43½	6 0	12 0	24 0	48 0	See footnote (g).
Ford Motor Co. of Aust. Pty. Ltd. ...	‡	—	—	—	—	‡ Freight shall be computed on the mileage to or from North Shore, plus ½ mile. See footnote (g). A charge of 61s. 8d. shall be imposed for each Special trip run from North Geelong either to place trucks at or to remove trucks from the Siding.
Australian Portland Cement Pty. Ltd. (Fyansford)	‡	—	—	—	—	‡ For freight charges see Volume 1, page 152. In the event of the Company desiring to have trucks specially placed or removed from one Road to another, or from one Road to the Public Siding at Fyansford, a shunting charge of 6s. 0d. for each 4-wheeled truck and 12s. 0d. for each bogie truck, with a minimum charge of 12s. 0d., shall be imposed in respect of each occasion upon which such a service is performed.
Oriental Timber Co's. Siding, a/c Sims, Cooper	‡	6 0	12 0	20 8	—	1. ‡ Mileage as to or from North Geelong, plus 1 mile, shall be charged. See footnote (g). 2. The shunting charges are to be imposed in addition to freight in respect of each trip the engine makes into the Siding, whether to place loaded or clear empty trucks or *vice versa*. 3. If the engine is required to work in the Siding sorting or marshalling trucks to be placed in different positions there, the charges shewn in footnote (a), for the time that the engine it so employed, shall be made.

Geelong Plaster Mills Pty. Ltd. Siding (Fyansford)	‡	6 0	12 0	12 0	24 0	‡ Freight charges shall be computed at tariff rates on the mileage to or from North Geelong, plus 1 mile. These shunting charges shall be imposed if worked by a passing Pilot engine in conjunction with Fyansford Siding. If it be necessary to work Siding by a Special Pilot trip from North Geelong, charges shewn in footnote (a) shall be imposed for the time the engine is engaged for each placing of trucks on the Siding ; time to be computed from when the engine leaves North Geelong until its return thereto.
East Siding	43½	6 0	12 0	24 0	48 0	
Geelong Gas Co's. Siding	43½	6 0	12 0	24 0	48 0	For placing loaded trucks on the Siding by engine.
NORTH WILLIAMSTOWN—						
Associated Fuel Sales Firewood Storage Site	7½	6 0	12 0	12 0	24 0	
Lander & O'Brien Firewood Site ...	7½	6 0	12 0	12 0	24 0	
NUMURKAH—						
Holdensen & Neilson's Oil Pipe Site ...	133¾	6 0	12 0	12 0	24 0	
Numurkah Farmers' Produce and Store Co. Ltd. Shed	133¾	—	—	—	—	
OAKLANDS—						
Department of Army Siding	199¾	6 0	12 0	24 0	48 0	For each placing by a Victorian engine.
Department of Munitions Sidings ...	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Oaklands, plus 8s. 1d. per ton, unless cheaper to charge the District rates and conditions applicable to Oaklands shewn in Volume 1, Page 163, plus 8s. 1d. per ton. The charges shewn in footnote (a) shall be imposed for the time a Victorian engine is engaged ; time to be computed from when the engine leaves the Oaklands Station Yard until its return thereto.
OAKLEIGH—						
J. F. Ezard's Timber Site	10½	6 0	12 0	24 0	48 0	
State Electricity Commission's Site ...	10½	6 0	12 0	—	—	
Oakleigh Council Siding	10½	6 0	12 0	12 0	24 0	
Oakleigh Council Siding a/c Reinforced Concrete and Monier Pipe Construction Coy. Pty. Ltd.	10½	6 0	12 0	12 0	24 0	
Oakleigh Council Siding a/c J. Hodgkinson and Son	10½	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (mimimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

(g) Freight on consignments forwarded between Melbourne and suburban stations, and the siding, shall be computed in accordance with the District rates between Melbourne and Geelong and North Geelong.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
OAKLEIGH—*continued.*						
Oakleigh Council Siding a/c Clive King ...	10½	6 0	12 0	12 0	24 0	
Clive King	10½	6 0	12 0	12 0	24 0	
Australian Plaster Industries Pty. Ltd. ...	10½	6 0	12 0	12 0	24 0	
J. McBeth	10½	6 0	12 0	12 0	24 0	
C. H. Tutton Pty. Ltd.	10½	6 0	12 0	24 0	48 0	
C. H. Tutton Pty. Ltd. Eastoakleigh Siding	11¼	6 0	12 0	24 0	—	
OUYEN—						
Angliss and Co's. Site	288¾	6 0	12 0	—	—	
Walpeup Shire's Oil Discharging Site ...	288¾	6 0	12 0	12 0	24 0	
PAKENHAM—						
T. E. Butler's Shed Site	36½	6 0	12 0	12 0	—	
Pakenham Cool Stores Platform Site ...	36½	6 0	12 0	12 0	—	
PATCHEWOLLOCK—						
Vacuum Oil Coy's. Oil Discharging Site ...	281½	—	12 0	—	—	Only tank trucks shall be placed at the site, which shall not be available for this traffic during the months of December and January each year.
PIANGIL—						
J. C. Branson's Firewood Cutting Site ...	242	6 0	12 0	—	—	
PORT FAIRY—						
Wharf Siding	‡	—	—	—	—	‡ All goods, except fish and rabbits, between country stations and the wharf shall, unless otherwise provided, be charged 1s. 6d. per ton in addition to the rate to or from Port Fairy Station.
Glaxo Laboratories (A'asia) Pty. Ltd. ...	185¾	—	—	—	—	A special charge of 30s. 10d. shall be imposed for each placing of trucks in the Siding.
Western District Cement Co. Ltd. ...	186½	6 0	12 0	12 0	24 0	

PORTLAND—						
Portland Pier (See Volume 1, page 177)						
PORTLAND NORTH—						
Shell Co. of Aust. Ltd. Siding	250½	—	—	—	—	A charge of 61s. 8d. shall be imposed in respect of each placing of trucks irrespective of the number of trucks.
Vacuum Oil Co. Pty. Ltd. Siding ...	250½	—	—	—	—	
Portland Freezing Works (See Volume 1, page 179)						
Wool Appraisement Centre (See Volume 1. page 178)						
PORT MELBOURNE—						
Commonwealth Oil Refineries Ltd. Siding	3¾	6 0	12 0	—	—	These charges apply when trucks are placed on the Siding by Ordinary trains carrying out operations at the New Pier.
and Anglo Iranian Oil Co. Ltd's. Siding	3¾	20 8	41 4	103 4	144 10	These charges apply when it is necessary to make special arrangements for the placing of trucks, whether loaded or empty, on the Siding.
Swallow and Ariell's Factory	3¼	—	—	—	—	The charges shewn in footnote (a) shall be imposed for the time the engine is engaged in placing trucks on the site, time to be computed from when the engine leaves Port Melbourne Yard until its return thereto.
Marine Engine Annexe	3¼	—	—	—	—	
Portstore	‡3¾	—	—	—	—	Open for Inwards and Outwards traffic in truck loads a/c Commonwealth Government Departments and Robert Hutchinson Ltd. ‡ Freight charges shall be imposed on the mileage to or from the Siding, with the exception shown in (i) :— In addition to the freight charges shewn in footnote (a) shall be imposed for the time an engine is engaged in placing trucks on the Siding, time to be computed from when the engine leaves the Port Melbourne Yard until its return thereto. When trucks are placed at the same time for more than one user of the Siding, the charges shall be borne proportionately by each, in accordance with the number of trucks placed for each of them during such shunting operation. (i) Traffic between the Siding and Port Melbourne Pier— Charge Per 4-wheeled truck ... ... £3 1s 8d. Per bogie truck ... ... £6 3s 4d.
Pier (see Volume 1, page 181)	—	—	—	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
PYRAMID—						
Gordon Shire's Oil Pipe Site	155	6 0	12 0	12 0	—	When it is necessary for trucks to be specially placed by an engine at the site of the surface valve :— The Council shall accept delivery of Inwards trucks and give delivery of Outwards trucks at a point in the Station Yard at Pyramid to be fixed by the Officer-in-Charge of the Station, and will be required to convey trucks to and from the site by means of horse or hand power, and then only with the permission and under the supervision of the Officer-in-Charge of the Station, whose instructions must be implicitly obeyed by the Council employees performing the work. Trucks to be discharged at the site of the surface valve shall be placed only at such times as are suitable to the Commissioners, and notwithstanding the fact that a truck has not been completely discharged, if the Officer-in-Charge requires such truck to be removed from the said site, the Council will be bound to move the truck to such a position as the Officer-in-Charge may direct.
H. W. Snell's Loading Site	155	6 0	12 0	12 0	24 0	
QUAMBATOOK—						
Vacuum Oil Co. Site	191	6 0	12 0	12 0	24 0	
QUEENSCLIFF—						
Fishermen's Pier	67½	6 0 6 0	— —	— —	— —	Fish traffic only. Minimum charge, 20s. 8d. per special trip for other traffic.
Department of Navy—Overhead Travelling Crane Site	67½	6 0	12 0	12 0	—	
Defence Department's Gantry Crane Site	67½	6 0	12 0	12 0	—	For each truck placed for loading or unloading at the Gantry Crane.

RAINBOW—						
W. C. Thomas and Sons' Siding ...	266¼	6 0	12 0	—	12 0	
Vacuum Oil Co. Oil Depot	266¼	6 0	12 0	12 0	—	
REDCLIFFS—						
State Rivers and Water Supply Commission's Siding	342½	6 0	12 0	12 0	24 0	
Redcliffs Co-operative Co's. Ltd. Site ...	342½	6 0	12 0	24 0	48 0	
Aurora Packing Co's. Site	342½	6 0	12 0	24 0	48 0	
Leviathan Plaster Co's. Site	342½	6 0	12 0	24 0	48 0	
R. D. Wurfel's Firewood Stacking Site ...	342½	6 0	12 0	12 0	24 0	
P. Rogan's Pole Storage Site	342½	6 0	12 0	12 0	24 0	
REDESDALE—						
A. E. Turner's Firewood Cutting Site ...	76¼	6 0	12 0	12 0	24 0	If the placing of trucks at both sites be done at the same time, placing charges shall be borne proportionately by each, in accordance with the number of trucks placed at each site.
D. W. Ferguson's Firewood Cutting Site ...	76¼	6 0	12 0	12 0	24 0	
RESERVOIR—						
P. Cairn's Firewood Site	9½	6 0	12 0	—	—	
R. Fowler Ltd's. Siding	11½	6 0	12 0	24 0	48 0	
RINGWOOD—						
Ringwood Co-op. Cool Stores Society Ltd. Siding	16¼	—	—	—	—	
Ringwood Co-op. Cool Stores Society Ltd. a/c E. Russell Pty. Ltd.	16¼	6 0	12 0	12 0	—	
ROBINVALE—						
Robinvale Producers Co-operative Co. Ltd's. Siding	‡	—	—	—	—	‡ Mileage shall be computed on the mileage to or from Robinvale, plus ¼ mile. In addition to the freight charges, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged for each placing of trucks at the Siding ; time to be computed from when the engine leaves the points giving access to the Siding until its return thereto.
ROCHESTER—						
Permewan, Wright Ltd. Shed on Public Siding	138¾	6 0	12 0	—	—	
Aggregate Contracting Coy. Pty. Ltd. Loading Ramp	138¾	6 0	12 0	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
ROCKBANK—						
H. S. K. Ward Pty. Ltd's. Siding ...	18½	—	—	—	—	
RUPANYUP—						
T. Brunton & Co's. Flour Mill Siding ...	184¾	—	—	—	—	
Vacuum Oil Co. Pty. Ltd. Site ...	184¾	6 0	12 0	12 0	24 0	
RUSHWORTH—						
Waranga Shire's Storage Site	104¾	6 0	12 0	12 0	24 0	
RUTHERGLEN—						
Carriage Dock	169¼	6 0	12 0	12 0	—	To be imposed only when trucks are placed at request of consignors or consignees, and it is convenient for the Department to do so.
C. Merrifield Firewood Site	169¼	6 0	12 0	12 0	—	
W. P. Gilfui's Shed Site	169¼	6 0	12 0	12 0	—	
D. Baker and F. Cuthbert Firewood Site ...	169¼	6 0	12 0	12 0	—	
SALE—						
Sale Gas Works (on Wharf Line) ...	128¾	6 0	12 0	—	—	Trucks of coal may be placed on the wharf road at the convenience of the Commissioners.
H. J. Shopland's Firewood Cutting Site (on Wharf Line)	129½	6 0	12 0	12 0	24 0	For each placing at such times as the engine is working between the Sale and sidings adjacent to the wharf. If a Special trip be necessary between the Sale Yard and the site, a fixed charge of 30s. 10d. for each return trip shall be imposed.
Daylesford Woollen and Worsted Mills Pty. Ltd. (Gippsland Section) Siding	‡	—	—	—	—	‡ 1. Freight charges shall be computed on the mileage to or from Sale, plus 1¼ miles. 2. In addition to freight, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged in placing the trucks on the Siding ; time be to computed from when the engine leaves Sale until its return thereto.

Silver Lake Flour Milling Co's. Siding ...	129¾	6 0	12 0	—	—	
Silver Lake Flour Milling Co's. Siding a/c Caltex Oil (Aust.) Pty. Ltd.	129¾	6 0	12 0	12 0	24 0	To be imposed when trucks are placed by an engine engaged in working between Sale and Sidings adjacent to the wharf. When trucks are placed by an engine assigned to make a Special trip to the Siding, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged in placing the trucks on the Siding ; time to be computed from when the engine leaves Sale until its return thereto.
Shell Co. of Australia's Siding ...	129½	6 0	12 0	12 0	24 0	To be imposed for each placing when performed by an engine engaged in working between Sale and Sidings adjacent to the wharf. When a Special trip is necessary between Sale and the Siding, a fixed charge of 30s. 10d. for each return trip shall be imposed. In the event of the Company desiring the removal of an empty truck by Special engine, the charges shewn in footnote (a) shall be imposed for the time that the engine is engaged.
Sale District Co-op. Butter and Cool Storage Co's. Siding	129¾	6 0	12 0	—	—	Trucks will only be placed and cleared from the Siding at such times as the engine is working between Sale and Sidings adjacent to the wharf.
Vacuum Oil Co's. Discharging Site ...	128¾	6 0	12 0	12 0	24 0	When a Special trip is run by an engine, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves Sale until its return thereto. If a Special trip is run to place trucks for the Vacuum Oil Co. and the Commonwealth Oil Refineries, only one charge will be imposed, but each Company shall be debited with its proportion of such charge.
Commonwealth Oil Refineries Ltd. Site ...	128¾	6 0	12 0	12 0	24 0	
Atlantic Union Oil Co's. Siding ...	128¾	6 0	12 0	12 0	24 0	When a Special trip is run by an engine, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine leaves Sale until its return thereto.
SEA LAKE—						
Shell Co. of Australia and Vacuum Oil Co's. Siding	238	6 0	12 0	12 0	—	
Imperial Chemical Industries of Australia & New Zealand Ltd. Bulk Salt Storage Site	238	6 0	12 0	12 0	24 0	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (mimimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
SEYMOUR—						
Defence Department's Mobilization Store Siding	‡	—	—	—	—	‡ 1. Mileage to the Siding shall be computed on the mileage to or from Seymour plus 1 mile. 2. The charges shewn in footnote (a) shall be imposed for the time that the engine is employed on the Siding ; time to be computed from when the engine leaves Seymour until its return thereto.
Colonial Gas Co's. Siding	61¼	6 0	12 0	12 0	24 0	
Seymour Council's Oil Site on Defence Department's Siding	61¼	24 0	—	—	—	Trucks will be placed only at the convenience of the Department and only during daylight. Trucks must be discharged and ready for clearance within one hour of being placed. The Department will not be responsible for any delay in placing trucks at the valve site, and if delay to trucks is caused by the use of the Siding by the Commonwealth, demurrage charges shall be enforced and paid by the Shire.
Vacuum Oil Co. Pty. Ltd's. Site ...	61¼	6 0	12 0	24 0	48 0	Trucks shall be placed only at a time convenient to the Department, and the Company must immediately discharge trucks on being placed, and in every case, before 5 p.m. on the day of placing.
Commonwealth Oil Refineries Ltd. Siding	61¼	6 0	12 0	24 0	48 0	
Shell Co. of Australia Ltd's. Oil Discharging Site	61¼	6 0	12 0	24 0	48 0	Trucks shall be placed only at a time convenient to the Department, and the Company must immediately discharge trucks on being placed, and in every case, before 5 p.m. on the day of placing.
Aggregate Contracting Co. Pty. Ltd. ...	61¼	6 0	12 0	24 0	48 0	

SHEPPARTON—						
Permewan Wright Ltd. Site	113	6 0	12 0	12 0	—	
Permewan Wright Ltd. Site a/c Ansell and Williams	113	6 0	12 0	12 0	—	
Shepparton Pear Packing Coy. Ltd. ...	112¼	6 0	12 0	30 10	61 8	
Shepparton Fruit Preserving Co's. Siding	113½	6 0	12 0	30 10	61 8	
Shepparton Fruit Preserving Co's. Siding, a/c S. J. Perry and Co.	113½	6 0	12 0			
Shepparton Borough Council's Metal Sdg.	113	6 0	12 0	24 0	48 0	
Lamrock (Deane and Runge) Siding ...	124½	—	—	—	—	
Shell Co. of Australia, Commonwealth Oil Refineries, Vacuum Oil Co's. and Caltex Oil (Aust.) Pty. Ltd. Siding	113	—	—	—	—	The charges shewn in footnote (a) shall be made for the time that an engine is employed on the Siding ; time to be computed from when the engine leaves Shepparton Yard until its return thereto. Where trucks are placed by an engine for more than one of the Companies concerned, the charges shall be debited to each Company proportionately.
Gaylard's Site	113	6 0	12 0	—	—	
State Electricity Commission Site ...	113	6 0	12 0	12 0	—	
Ampol (Alba) Petroleum Pty. Ltd. ...	113	—	—	—	—	The charges shewn in footnote (a) shall be imposed for the time that an engine is engaged in placing trucks on the Siding ; time to be computed from when the engine leaves Shepparton Yard until its return thereto. If the placing is done in conjunction with the placing for another consignee, the charge shall be borne proportionately by each.
G. Rogerson's Shed Site	113	6 0	12 0	12 0	—	
Shepparton Co-operative Society Ltd. Shed Site	113	6 0	12 0	12 0	—	
Postal Department's Storage Site ...	113	6 0	12 0	12 0	24 0	
Atlantic Union Oil Co. Ltd's. Siding ...	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from Shepparton, plus ½ mile. The Siding shall be worked by engine from Shepparton at a time suitable to the Department, and in addition to freight, the charges shewn in footnote (a) shall be made for the time the engine is engaged working the Siding ; time to be computed from when the engine leaves the Shepparton Yard until its return thereto.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
SHEPPARTON—*continued.*						
Shepparton Cattle Siding	114½	—	—	—	—	The freight charges on Live Stock forwarded to or from the Shepparton Cattle Siding shall be computed on the mileage shown, *viz.*, 114½ miles.
SMYTHESDALE—						
Albion Quarrying Co. Pty. Ltd. Siding ...	87	6 0	12 0	12 0	24 0	
SOUTH GEELONG—						
Atlantic Union Oil Co's. Site	46¼	6 0	12 0	12 0	—	Tariff rates as to or from South Geelong shall be charged.
Cheetham Salt Co's. Siding	48½	—	—	—	—	For charges on consignments to Melbourne and Suburban stations, see Volume 1, page 159.
State Electricity Commission	46¼	6 0	12 0	24 0	—	
SOUTH KENSINGTON—						
City Abbatoirs' Siding	2¼	20 8	41 4	—	—	Minimum £4 2s. 6d. per Special trip for Inwards traffic. (See Special Rates, Volume 1, page 156.) In addition, the charge shewn in footnote (b) shall be imposed for the time the engine is detained waiting for the gates to be opened.
Minifie and Co.	‡2¼	—	—	—	—	‡ The same rates as to Melbourne shall be charged for Inwards consignments and unless otherwise specified, actual mileage rates for Outwards consignments.
James Minifie & Coy. Pty. Ltd. a/c O-So-Lite Products Pty. Ltd.	‡2¼	—	—	—	—	
New Zealand Loan and Mercantile Co. ...	‡2¼	—	—	—	—	
Australian Mercantile Land & Finance Co.	‡2¼	—	—	—	—	

Kenstore (Defence Dept's. Ordnance Sdg.) and Dept. of Navy's Stores	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from South Kensington, plus ½ mile. In addition to the freight charges, the following charges shall be made:— 1. For placing trucks, whether loaded or empty, on the Siding, £4 2s. 6d. for each placing. 2. For detention of an engine at any gate or gates erected across the Sidings, the charges shewn in footnote (b) shall be imposed for the time the engine is detained.
Commonwealth Food Control and Munitions Departments Shed Sites at Dynon	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to or from North Melbourne, plus 1¼ miles. In addition to the freight, the charges shewn in footnote (a) shall be imposed ; the time to be computed as follows :— (i) When placing is done by a Special pilot—from the time the engine leaves *Dudley Street until its return thereto.* (ii) When placing is done by the Ordinary Pilot—from the time the engine leaves Weighbridge Junction until its return thereto. If the placing be done for both authorities at the one time, the charge *shall be borne proportionately* by each, in accordance with the number of trucks placed at each shed site.
SPOTSWOOD— *Department of Munitions Siding* ...	7	6 0	12 0	12 0	—	To be imposed when the Siding is worked in conjunction with traffic to or from the Vacuum Oil Co's. Siding If a Special trip be arranged to place trucks on or clear trucks from the Siding, the charges shewn in footnote (a) shall be imposed for the time the engine is engaged, the time to be computed from when the engine commences the Special service until its return to the same point.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

(b) In addition to the charges shewn a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each subsequent 5 minutes or part thereof that the engine is detained waiting for the gates to be opened.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
SPOTSWOOD—*continued.*	Miles.	s. d.	s. d.	s. d.	s. d.	
Shell Co. of Australia's Sidings—						
Works Siding	6¾	6 0	12 0	‡	—	‡ 1. Minimum shunting charges as for four trucks.
Wharf Siding ... ... *also Traffic a/c Neptune Oil Co. Ltd.*	7	6 0	12 0			2. Loaded trucks hauled between Spotswood Wharf and the Shell Co's. Works Siding shall be subject to a haulage charge of 40s. 2d. per 4-wheeled truck and 80s. 4d. per bogie truck. 3. *Special Train Service.*—When Special train services are arranged at the request of the Shell Company to place or remove trucks, loaded or empty, the following conditions for the Special service shall apply:— (i) A charge of £8 0s. 8d. shall be made for each Special trip run to or from the Company's Works Siding, or to and from the Wharf Siding for the Company, provided that such Special trip does not occupy more than two (2) hours from the starting to the returning point. (ii) The provisions and charges in respect of all Special trips that are estimated to occupy over two (2) hours shall be subject to special agreement with the Chief Commercial Manager and the Chief Traffic Manager. 4. *Detention to Engine.*—If Outwards loading is waited for, a charge of 20s. 8d. for every quarter of an hour or part thereof that the engine is detained over the first quarter of an hour, shall be imposed. The time is to be computed from when the engine passes through the boundary gate into the Company's works, or, if the detention occurs at the Wharf, from the time the engine arrives at the Wharf. 5. Inwards and Outwards traffic *a/c* the Neptune Oil Coy. Ltd. may be accepted to or from these Shell Coy's. Sidings under the same conditions as appply to the Shell Co. of Australia. 6. Empty drums and cases for the Shell Co. must be addressed to this Siding and waybilled thereto.

Sharp and Taylor's Siding, see * ...	5¾	—	—	—	—	* Open for traffic *a/c* John Sharp & Sons, Victorian State Wool Committee and the International Harvester Co. of Australia Pty. Ltd.
Wright, Stephenson and Co, (Aust.) Pty. Ltd.	5¾	—	—	—	—	The Company is to accept and give delivery of trucks with the last truck in and first truck out just inside the Engine Notice Board.
Australian Glass Manufacturers Co. Ltd. Siding	5¾	—	—	—	—	
State Electricity Commission's Power House Sidings	7	—	—	—	—	Inwards consignments are to be waybilled to the State Electricity Commission, Spotswood. The State Electricity Commission has also taken over the Departmental Newport Power House Sidings, in respect of which, freight charges shall be computed on the mileage to or from the Sidings which are 7 miles from Melbourne. When trucks are, used for the clearance of ashes or for other local services within the Sidings area, a truck hire charge of 68s. 2d. per day for each 4-wheeled, and 136s. 4d. for each bogie truck, shall be made. These charges shall be imposed for each day until the trucks are released for ordinary service. In instances where the Arrival Roads in the Power Station area are not available for the reception of trucks and the trucks require to be placed at some other point and be subsequently placed by Special service in such Arrival Roads, the charges shewn in footnote (a) shall be imposed ; the time to be computed from when the engine leaves its Depot until its return thereto.
Ampol (Alba) Petroleum Pty. Ltd. ...	6¾	6 0	12 0	24 0	—	When a Special trip is arranged to work the Siding, the charges shewn in footnote (a) for the time the engine is engaged shall be imposed ; the time to be computed from when the engine commences the special service until its return to its originating point. On each occasion on which it is necessary to obtain the keys of the gates from the Company and return them thereto in connection with the operation of traffic beyond the gates, a detention charge of 30s. 10d. for the first fifteen minutes and 9s. 4d. for each subsequent 5 minutes, shall be imposed.
H. C. Sleigh Ltd.	6½	6 0	12 0	24 0	—	To be imposed when the Siding is worked in conjunction with other Sidings. When a Special trip is arranged to work the Siding, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine commences the special service until its return to its originating point.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
SPOTSWOOD—*continued.*						
Melbourne and Metropolitan Board of Works Siding	5¾	—	—	—	—	
Robinson and Co's. Siding	5½	—	—	—	—	
Victoria Ammonia Co's. Siding ...	5¾	—	—	—	—	
Vacuum Oil Co's. Siding	8	6 0	12 0	48 0	72 1	‖ Special trips are arranged, the charges shewn in footnote (a) for the time the engine is engaged shall be imposed ; the time to be computed from when the engine commences the Special service until its return to its originating place. Empty oil drums, cases, and cases containing tins must be addressed to this Siding and waybilled as follows :— Truck Loads, minimum 30 cwt.—to Spotswood ; less than Truck Loads—to Grain Shed, Melbourne, freight, if any, to be computed as to the Siding at Spotswood.
Atlantic Union Oil Co's. Siding ...	5¾	—	—	—	—	Empty returns shall be consigned to the Company's Siding and dealt with as follows :—Truck Loads, minimum 30 cwt.—Waybilled and sent direct, and charges, if any, are due computed to Spotswood. Less than Truck Loads—Waybilled and sent to Grain Shed, Melbourne and charges, if any are due, computed to Spotswood.
SPRINGVALE—						
Kelly and Lewis Pty. Ltd. Siding ...	15½	6 0	12 0	24 0	48 0	Open for traffic, a/c Messrs. Kelly and Lewis, Spring Vale Foundry Co., and K. L. Engines and Tractors Pty. Ltd. Only one minimum or maximum charge shall be imposed for each placing.
Rocla Pipes Ltd.	‡	6 0	12 0	24 0	48 0	‡ The freight shall be computed on the mileage to or from Spring Vale, plus a ¼ of a mile.

ST. ALBANS—						
John McGrath Industries Ltd.	11¼	6 0	12 0	24 0	48 0	To be imposed when placing is done by Ordinary Pilot engine between Sunshine and St. Albans. If placing be done by a Special Pilot trip, the charges shewn in footnote (a) shall be imposed ; time to be computed from when the engine commences the Special service until its return to its originating place.
ST. ARNAUD—						
Malcolm and Co's. Siding	158½	—	—	—	—	
Postal Department's Storage Site ...	158½	6 0	12 0	—	—	
Shell Co. of Australia's Siding ...	158½	6 0	12 0	24 0	—	
St. Arnaud Council's Oil Pipe Site ...	158½	6 0	12 0	12 0	24 0	
Cresco Fertilizers' Shed Site	158½	6 0	12 0	12 0	24 0	
Commonwealth Fertilizers and Chemicals Ltd. Shed Sites	158½	6 0	12 0	12 0	24 0	
Caltex Oil (Aust.) Pty. Ltd. Oil Discharging Site	158½	6 0	12 0	12 0	24 0	
Vacuum Oil Co. Oil Discharging Site ...	158½	6 0	12 0	24 0	—	
STATE MINE—						
Siding to Siding	—	6 0	12 0	—	—	To be imposed for each truck transferred from one Siding to another.
Dudley, Eastern, Kirrak or Station Area Sidings	—	6 0	12 0	—	—	To be imposed for each truck placed in the Sidings mentioned.
STAWELL—						
Grampians Line	see ‡ (iv)	—	—	—	—	1. *Traffic to or from any loading place between a point 8 miles from Stawell and the termination of the Grampians line* :— (i) Subject to Departmental convenience, and not less than three days' notice, a Special Goods train may be arranged by Stationmaster, Stawell. (ii) The traffic offering shall consist of not less than seven trucks loaded to the minimum weights prescribed for the description of traffic specified, except that in the case of stone traffic, the minimum shall be five 16-ton trucks loaded to the prescribed minimum weights. (iii) The amount of £13 12s. 11d. representing the charge for the Special service, shall be collected by the applicant and must be paid to the Stationmaster, Stawell, before the Special train is arranged. This charge is to be taken to debit in the Miscellaneous Debit Book and reported on Form T.R. 95.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		Remarks.
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
STAWELL— *Grampians Line—continued.*						‡ (iv) The ordinary through tariff rates for the mileage to or from the loading place on the Grampians line and the final destination station shall be imposed in addition to the Special charge. (v) The charges shewn in footnote (a) shall be imposed for any detention arising owing to Outwards trucks not being loaded within four hours after being placed in position for loading. This charge shall be imposed notwithstanding any delay that may take place in unloading Inwards traffic. (vi) Inwards traffic must be unloaded immediately the trucks are placed. (vii) Small consignments from Stawell for places where trucks are arranged to be loaded or unloaded may be accepted at Owner's Risk and subject to immediate delivery on payment of a surcharge of 3s. 6d. per ton, minimum 3s. 6d. per consignment, in addition to the ordinary tariff rates for the distance carried. The surcharge and its minimum is to be entered separately on Goods waybills. (viii) Freight charges on small consignments and truck loads of Inwards goods to be "Prepaid" and on Outwards goods "to pay." (ix) A charge of 4s. 0d. per ton shall be collected, and debit raised by Stawell on waybill and subsequently refunded to the person by whom the goods are unloaded. (In order that disputes may be avoided, the waybill must be signed by the person by whom the goods are unloaded and his signature certified to by the Guard of the train.) (x) All traffic shall be carried at "Owner's Risk."

						(xi) Duplicate Guards' Truck Sheets must be prepared, showing particulars of trucks placed or cleared at the various loading points (mileage to be shown), time of departure from Stawell, time of arrival and departure from each stopping place, time trucks placed, and time loading is finished at Fyan's Creek, and any remarks necessary in regard to detention. These sheets are to be numbered consecutively, and kept for future reference. 2. Traffic in full truck loads *to or from any loading place on the Grampians line (Stawell Brick Co's. Siding excepted), less than 8 miles from Stawell* :— (i) Subject to Departmental convenience, a Special train may be arranged by the Stationmaster, Stawell, to place and clear trucks on payment of 23s. 9d. per mile or part thereof for each placing, in addition to through tariff rates between the loading place and destination station. (ii) Trucks must be loaded immediately after being placed in position. (iii) All traffic shall be carried at "Owner's Risk." (iv) Duplicate Guards' Truck Sheets, showing times of arrival and departure, the mileage of each stopping place, and other particulars, and numbered consecutively, to be filed by Stationmaster, Stawell, for reference. (v) Particulars of Special charges to be reported on Form T.R. 95.
Stawell Brick Co's. Pty. Ltd. Siding ...	150	6 0	12 0	—	—	Open for Outwards traffic in truck loads.
Wimmera Flour Milling Co. Pty. Ltd. Sdg.	‡	6 0	12 0	—	—	‡ Freight charges shall be computed on the mileage to or from Stawell, plus one quarter of a mile. In instances where loaded trucks are placed for the Flour Mills, discharged, and hand-shunted from the Flour Mill to the Bark Mills for Outwards *loading or vice versa, no shunting charge* shall be imposed for the removal of such trucks, for the reason that the prescribed shunting charge has been levied for the placing of the same trucks.
Wimmera Flour Milling Co. Pty. Ltd. Sdg., a/c Carl Wehl & Son						
Borough of Stawell Oil Discharging Site ...	150	6 0	12 0	—	—	
R. C. Neal's Site	150	6 0	12 0	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
STAWELL—*continued.*						
Oates Bros.' Site	150	6 0	12 0	—	—	
Forests Commission of Victoria ...	150	6 0	12 0	12 0	24 0	
L. A. Sanderson's Siding	‡	6 0	12 0	12 0	—	‡ Freight to be computed on the mileage to or from Stawell, plus ½ mile.
Australian Barley Board's Stacking Site on Grampians Line	‡	10 2	20 4	—	—	‡ 1. Freight shall be computed on the mileage to or from Stawell, plus 2 miles. 2. The shunting charges shall be imposed for placing Inwards loaded truck at the site or removing Outwards loaded trucks from the site. These shunting charges are to be included on waybills. The charges shewn in footnote (a) shall be imposed when an engine has to run to the site either to place or remove trucks ; time to be computed from when the engine leaves its originating point at Stawell or Ararat, as the case may be, until its return thereto.
SUNSHINE—						
Ardeer Siding	10	6 0	12 0	—	—	To be imposed when it is necessary for the engine of the Ordinary Goods train to place trucks on the Siding outside railway boundary. For each Special trip run at the request of the Company between Sunshine and the Siding for explosives, a charge of 80s. 4d. shall be made, and for each Special trip for other goods the charge shall be 40s. 2d., and a further charge of 20s. 8d. shall be imposed for each period of 15 minutes (or part thereof) engine is detained at the Siding after the first such period.
Imperial Chemical Industries of Aust. and New Zealand Ltd.	‡	6 0	12 0	24 0	48 0	‡ The freight charges shall be computed on mileage to or from Deer Park, plus one quarter mile.
Australian Reinforced Concrete Engineering Co Pty. Ltd.	8¾	6 0	12 0	12 0	—	

Melbourne Iron and Steel Mills Pty. Ltd. ...	‡9	6 0	12 0	12 0	24 0	‡ See page 114. When trucks are sorted out and placed at *particular locations* at the request of the Company, the charges shewn in footnote (a) for the time that the engine is so employed shall be imposed.
Darling and Sons	7¾	6 0	12 0	—	—	
McKay, Massey Harris, Pty. Ltd. ...	7¾	—	—	—	—	
Albion Quarrying Co. Pty. Ltd. ...	8¾	—	—	—	—	
Jas. Hardie and Co.	‡8¾	6 0	12 0	12 0	24 0	‡ See page 114.
Commonwealth Quarry	‡8½	—	—	—	—	‡ See page 114.
Highfield's Quarry	‡9¼	—	—	—	—	‡ See page 114.
S. J. Willis Pty. Ltd.	‡9¼	6 0	12 0	36 0	—	‡ See page 114.
Brooklyn Quarries Pty. Ltd. a/c Sulphates Pty. Ltd.	‡8	6 0	12 0	12 0	24 0	‡ See page 114. *These charges are to be imposed when placing is done by ordinary pilot engine between Newport and Sunshine.* When placing is done by special pilot trip the charge shown in footnote (a) shall be imposed—time to be computed from when the engine commences the special service until it returns to its originating point.
Armbrook a/c F. Watkins Pty. Ltd. ...	‡9¼	—	—	—	—	‡ See page 114. The Special charges shewn on page 65 under Newport (South Brooklyn, North Brooklyn, Little Brooklyn, Prossor or Austral Sidings, clauses 2 and 3 inclusive), shall also apply to traffic a/c F. Watkins Pty. Ltd. at Armbrook Siding.
Armbrook a/c State Electricity Commission of Victoria *Armbrook a/c Myer Emporium Ltd.* ...	‡	6 0	12 0	24 0	48 0	‡ Freight to be imposed for the shortest mileage to or from the Siding, which is 3¼ miles from Newport and 1½ miles from Sunshine. These charges are to be imposed when placing is done by ordinary pilot engine between Newport and Sunshine. When placing is done by special pilot trip the charges shown in footnote (a) shall be imposed—time to be computed from when the engine commences the special service *until it returns to its* originating point. In the event of trucks for more than one user of the Siding being placed at the one time *the charges shall be apportioned according* to the number of trucks for each Company.
Brookwood (Forest Commission Firewood Storage Site)	‡8	6 0	12 0	12 0	24 0	‡ See page 114. For sorting out and placing trucks at *particular* locations at the request of the Commission, the charges shewn in footnote (a) shall be imposed for the time the engine is so engaged.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
SUNSHINE—*continued.*						
Melbourne Machinery Co. Pty. Ltd. ...	‡	6 0	12 0	24 0	48 0	‡ See page 114. These charges are to be imposed when placing is done by ordinary pilot engine between Newport and Sunshine. For placing by Special Pilot trip the charges shewn in footnote (*a*) shall be imposed; time to be computed from when the engine commences the Special service until it returns to the originating point. In the event of trucks for more than one user of the siding being placed at the one time the charges shall be apportioned according to the number of trucks placed for each Company.
Melbourne Machinery Co. Pty. Ltd. a/c H. M. Butterfield Pty. Ltd.	‡	6 0	12 0	24 0	48 0	
Monsanto (Aust.) Pty. Ltd.	‡9¼	6 0	12 0	12 0	24 0	‡ See page 114.
Western Metropolitan Market Trust's Sdg. (Melbourne Pig & Calf Market), Brooklyn	‡9¼	—	—	—	—	1. This Siding is open only for consignments of pigs and calves sent to or from the Market a/c the following selling agents :— McPhail, Anderson & Coy. T. Shaw Logan & Coy. Gippsland and Northern Co-op. Coy. Ltd. Gordon C. Ellis & Co. Pty. Ltd. ‡ 2. In addition to the freight charges to or from this Siding, the mileage to which shall be computed on the shortest mileage *via* Newport or Sunshine (see page 114), as the case may be, the following Special charges shall be imposed :— 3. Trips run by Ordinary trains on week days—nil. 4. Special trips from Sunshine or Newport, as the case may be, run for ten or more trucks of stock on week days or Sundays, or public holidays—nil.

						5. Special trips from Sunshine or Newport, as the case may be, run for less than ten trucks of stock on week days—114s. 5d. 6. Special trips from Sunshine or Newport, as the case may be, run for less than ten trucks of stock on Sundays or public holidays—170s. 10d. 7. *Detention Charges. The charges for detention of engine at Siding shall be :—* When loaded and/or empty trucks are placed and/or removed in straight movements entailing no delay, no detention charges shall be made, but if the engine is delayed for any reason whatsoever not related to safe-working and/or train control, engine detention time shall be charged against the Trust at the charges shewn in footnote (*a*), the time to commence immediately after the first placing of Inwards trucks and to continue until the train or engine is ready for departure ; in the event of there being no Inwards traffic, time will commence immediately the engine is ready to pick up the Outwards trucks, and continue until the train is ready for departure.
Braybrook Quarries Pty. Ltd.	‡9¼	—	—	—	—	‡ See page 114.
Stanley Quarries Pty. Ltd.	‡9¼	—	—	—	—	‡ See page 114.
Stanley Quarries, a/c Consolidated Concrete Co. Pty. Ltd.	‡9¼	6 0	12 0	24 0	—	‡ See page 114.
Sunshine–Newport Loop Sidings ...	—	—	—	—	—	For mileages to Sidings on this Loop from Newport, Sunshine and Broadmeadows (see page 114).
SWAN HILL— *Wharf*	214¾	—	—	—	—	When Inwards Goods which arrive too late for a boat are unloaded into Goods Shed and subsequently reloaded into a truck and placed at the wharf, a charge of 4s. 0d. per ton, minimum 8s. 0d. per truck, shall be imposed, not including loading or unloading. No additional charge shall be made in respect of consignments in truck loads carried direct to the wharf.
	—	6 0	12 0	12 0	—	For placing trucks at the Wharf Siding *a/c* T. H. Jackett and Sons, Permewan Wright and Co., and State Rivers and Water Supply Commission.
Swan Hill Shire's Oil Discharging Site ...	214½	6 0	12 0	12 0	—	To be imposed for placing trucks at the discharging point.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
SWAN HILL—*continued.*						
Postal Department's Pole Depot ...	214½	6 0	12 0	—	—	
Shell Co. of Australia and Vacuum Oil Co's. Siding	214½	6 0	12 0	12 0	24 0	
Lessees of Manure Shed	214½	6 0	12 0	—	—	To be imposed whenever it is necessary to specially sort out trucks for lessees so that the trucks may be placed at the shed for delivery.
Cattle Siding	216	—	—	—	—	
Swan Hill Borough Storage Site ...	214½	6 0	12 0	12 0	24 0	
K. A. Cook's Firewood Cutting Site ...	214½	6 0	12 0	12 0	—	
Swan Milling Co.	‡	6 0	12 0	24 0	48 0	‡ Freight to be computed on the mileage to Swan Hill, plus ½ mile.
Caltex Oil (Aust.) Pty. Ltd.	‡	6 0	12 0	12 0	24 0	‡ 1. Freight charges shall be computed on the mileage to or from Swan Hill, plus ½ mile. 2. If trucks are placed on the Siding by an engine which makes a Special trip to the Siding, the charges shewn in footnote (*a*) shall be imposed ; time to be computed from when engine leaves Swan Hill until its return thereto.
SYDENHAM—						
Slaney and Roberts, trading as Butler and Moss	15	6 0	12 0	—	—	This shunting charge shall be imposed only when trucks are placed on the Siding by the engine of an "Up" Goods train.

TABILK—						
J. and G. Ponting's Firewood Stacking Site	74	6 0	12 0	—	—	
TATURA—						
Rosella Preserving and Manufacturing Co. Ltd.	‡	—	—	—	—	‡ 1. Freight to be computed on the mileage to Tatura, plus ½ mile. 2. In addition to freight, the charges shewn in footnote (*a*) shall be imposed for placing trucks on the Siding. No charge is to be imposed for clearing the trucks from the Siding.
TERANG—						
Glenormiston Butter and Cheese Factory's Site	137¼	6 0	12 0	12 0	24 0	
Trufood of Australia Ltd. Site	137¼	6 0	12 0	—	—	Shunting charges shall not be imposed in instances where delivery of trucks is accepted at any place on the Siding suitable to the Department and the Company hand or horse-shunt therefrom.
THORNBURY—						
Glen Iris Brick and Tile Co. Siding ...	7	—	—	—	—	
TIMBOON—						
P. R. LeCouteur Timber Milling Site ...	149½	6 0	12 0	12 0	24 0	
G. C. Sullivan's Wheat Storage Site ...	149½	—	—	—	—	A charge of 24s. 0d. shall be imposed for each placing of a truck or trucks at the site by engine power.
TOCUMWAL—						
Mechanical Transporter Siding ...	156¼	6 0	12 0	24 0	72 0	For any traffic consigned to Tocumwal and placed at the mechanical transporters for unloading, these shunting charges shall be imposed for placing trucks, plus 2s. 9d. per ton for unloading.
Department of Army Spur Siding ...	156¼	6 0	12 0	24 0	72 0	
Department of the Interior Storage Sites ...	156¼	6 0	12 0	24 0	72 0	
Wise Bros.' Siding	156¼	6 0	12 0	—	—	For consignments *a/c* Wise Bros. or T. Boyle.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
TOCUMWAL—*continued.*						
Goods re-directed at Tocumwal ...	—	—	—	—	—	(i) Goods consigned from Victorian stations to Tocumwal, placed on the Goods Shed Road at that station for local delivery, and subsequently consigned to New South Wales stations and hauled by the Department from the Tocumwal Goods Shed Platform to the Transhipping Roads for transhipment into New South Wales trucks, shall be subject to a haulage charge of 4s. 0d. per ton, minimum 20s. 0d. per consignment, in addition to the usual transhipping charges. (ii) Goods consigned from New South Wales stations to Tocumwal and reloaded into Victorian trucks and hauled by the Department to the Goods Sheds or to the Crane for unloading, shall be charged a haulage charge of 4s. 0d. per ton, minimum 20s. 0d. per consignment, in addition to the usual transhipping charges. (iii) Goods consigned from New South Wales stations to Tocumwal, and after arrival there reconsigned to Victorian stations, and which can be transhipped at the parallel Victorian and New South Wales roads, shall be transhipped by the Department at a charge of 6s. 8d. per ton. A signature for the contents of the New South Wales truck must be obtained and a consignment note lodged for the journey from Tocumwal to the Victorian station. If such consignments *ex* New South Wales be divided at Tocumwal and consigned in different quantities to Victorian destination stations, thereby involving extra shunting by engine at Tocumwal, an additional charge of 4s. 0d. per ton, minimum 20s. 0d. per consignment, shall be imposed.

Shell Co. of Aust. Ltd's. Site	156¼	6 0	12 0	24 0	—	
Firewood Cutting Site, a/c A. Eltis ...	156¼	6 0	12 0	12 0	—	To be imposed for the clearance of trucks from the site to which vehicles must be propelled by hand.
Vacuum Oil Co's. Site	156¼	6 0	12 0	12 0	—	
Commonwealth No. 1 Siding	‡	—	—	—	—	‡ 1. Freight charges shall be calculated on the mileage to or from Tocumwal, plus 2¼ miles. 2. The Siding shall be worked by a Pilot engine from Tocumwal at a time suitable to the Department. 3. In addition to freight, the charges shewn in footnote (*a*) shall be made for the time the engine is engaged in placing trucks at the Siding; time to be computed from when the engine leaves Tocumwal until its return thereto.
Commonwealth No. 2 Siding	‡	—	—	—	—	‡ 1. Freight charges shall be calculated on the mileage as to or from Tocumwal, plus 1¼ miles. 2. In addition to the freight charges, the charges shewn in footnote (*a*) shall be made for the time the engine is engaged in placing trucks at the Siding ; time to be computed from when the engine leaves Tocumwal until its return thereto.
TONGALA— *Tongala Milk Products Ltd.*	‡	—	—	—	—	‡ Freight charges shall be computed on the mileage to Tongala, plus ¼ mile. In addition to the freight charges, the charges shewn in footnote (*a*) shall be imposed. Time to be computed from when the engine leaves Tongala until its return thereto.
TOOLONDO— *S. N. Rodda Ptd. Ltd. Storage Site* ...	234¾	6 0	12 0	12 0	24 0	
TOORA— *British United Dairies Pty. Ltd's. Siding* ...	114½	—	—	—	—	In addition to the freight charges, the charges shewn in footnote (*a*) shall be made for each placing of trucks on the Siding ; time to be computed from when the engine commences the service in connection with the operation of the Siding until the completion of such service.
TOORAK— *Firewood Siding Frontages*	5¼	6 0	12 0	—	—	This charge shall be made for placing trucks (either for loading or unloading), for lessees opposite their sites on the Siding frontages in the Goods Yard.
Forests Commission of Victoria Firewood Cutting Site	5¼	6 0	12 0	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
TOORONGA—						
Malvern City Council Siding	6¼	—	—	—	—	
TRAFALGAR—						
Trafalgar Butter Co's. Siding	75	—	—	—	—	
Permewan Wright & Co's. Site ...	75	6 0	12 0	12 0	24 0	
Trafalgar Timbers Pty. Ltd. Crane Site ...	75	6 0	12 0	—	—	
W. A. Purvis Stores Pty. Ltd. Siding ...	75	—	—	—	—	The Company to accept delivery of Inwards trucks and give delivery of Outwards trucks on No. 3 Road.
TURIFF—						
J. R. Torpey's Siding	258½	6 0	12 0	12 0	—	
TYABB—						
Tyabb & District Co-op. Cool Stores Grain Shed Sites	37½	6 0	12 0	12 0	—	
UPPER FERNTREE GULLY—						
Departmental Siding a/c J. A. Jones ...	23½	6 0	12 0	—	—	
VICTORIA PARK—						
Stratton and Co. Pty. Ltd.	3½	—	—	—	—	
State Electricity Commission's Siding ...	3½	6 0	12 0	12 0	24 0	
VIOLET TOWN—						
Vacuum Oil Co's. Siding	104½	6 0	12 0	24 0	48 0	

WAHGUNYAH—						
Carriage Dock Siding	174	6 0	12 0	12 0	—	The shunting charge shall only be imposed when trucks are so placed at the request of consignors or consignees, and it is convenient for the Department to do so.
Commonwealth Fertilizers and Chemicals Ltd. Sheds	174	6 0	12 0	12 0	—	
Shell Co. of Aust. and Vacuum Oil Co's. siding	174	6 0	12 0	24 0	48 0	
Shell Co. of Aust. Ltd. and Vacuum Oil Co. Pty. Ltd. a/c Atlantic Union Oil Co. Ltd.	174	6 0	12 0	24 0	48 0	If trucks for the Atlantic Union Oil Coy. Ltd., Shell Coy. of Aust. Ltd. and/or Vacuum Oil Co. Pty. Ltd. are placed on the Siding during the one operation, the total charge shall be apportioned according to the number of trucks placed for each Company.
WALHALLA LINE—						
Forests Commission of Victoria Siding ...	99½	—	—	—	—	Freight charges on consignments shall be calculated on the actual Mileage to or from the Siding. Trucks will be placed and cleared by Ordinary Goods trains, and in the absence of regular Goods trains, arrangements will be made to clear the loaded trucks by Special engine, for a minimum of six trucks, and no charge beyond the ordinary freight charges will be made for the service. If a special service be required to clear less than six loaded trucks, an extra charge of 20s. 8d. for the first 15 minutes detention or part thereof, and 6s. 8d. for each 5 minutes' detention or part thereof beyond the first 15 minutes, shall be added to the freight ; time to be computed from when engine leaves Depot at Moe until its return thereto.
WALLAN—						
A. A. Cleve's Firewood Cutting Site ...	29½	6 0	12 0	12 0	—	
WANGARATTA—						
Alumatta Siding, a/c Bruck Mills (Aust.) Ltd. *Wangaratta Flour Mills Co. Pty. Ltd.'s Siding*	‡	—	—	—	—	‡ 1. Freight on traffic forwarded to or from the Siding shall be computed on the mileage to or from Wangaratta, plus 1 mile. 2. In addition to the freight charges, charges shewn in footnote (a) shall be imposed, time to be computed :— (i) *When the Siding is worked by a passing train*—From when the train arrives at the Main Line points giving access to the Siding until the journey is recommenced from that point. (ii) *When the Siding is worked by Pilot Engine from Wangaratta*—From when the engine leaves Wangaratta until its return thereto.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
WANGARATTA—*continued.*						3. When trucks are placed at the one time for Bruck Mills (Aust.) Ltd. and Wangaratta Flour Mills Co. Pty. Ltd., the charge shall be borne proportionately by each in accordance with the number of trucks placed for each.
Vacuum Oil Co. Pty. Ltd's. Oil Discharging Site	145½	6 0	12 0	12 0	24 0	
Wangaratta Flour Mill Co's. Shed Site ...	145½	6 0	12 0	12 0	24 0	
Caltex Oil (Aust.) Pty. Ltd's. Oil Discharging Site	145½	6 0	12 0	24 0	—	
Shell Co. of Australia's Oil Discharging Site	145½	6 0	12 0	12 0	24 0	
P. Smith's Shed Site	145½	6 0	12 0	12 0	24 0	
WARBURTON—						
Melbourne and Metropolitan Board of Works' Shed Site	48¾	6 0	12 0	12 0	24 0	
WARRACKNABEAL—						
Thomas, W. C., and Sons Pty. Ltd. ...	216¾	—	—	—	—	
Shell Co. of Australia's Site	216½	6 0	12 0	12 0	24 0	If trucks for the Shell Co. and Caltex Ltd. are placed on the Siding during the one operation, the charge shall be borne proportionately by each, in accordance with the number of trucks placed for each of them.
Shell Co. of Australia's Site a/c Caltex Oil (Aust.) Pty. Ltd.	216½	6 0	12 0	12 0	24 0	
John Darling and Son's Stacking Site ...	216½	6 0	12 0	12 0	24 0	These shunting charges shall be imposed only for trucks placed on the Dead-End Siding adjacent to Wheat Stacking Sites Nos. 31 to 36 inclusive.

Warrackside (Australian Wheat Board Siding)	‡	—	—	—	—	‡ Freight charges shall be computed at ordinary tariff rates on the mileage to or from the Main Line points giving access to the Siding (217¼ miles from Melbourne), plus 1 mile. In addition to the freight charges a shunting charge as set out hereunder shall be inposed :— (i) When the Siding is worked by a passing train, the charges shown in footnote (*a*) shall be imposed for the time the engine is engaged ; time to be computed from when the engine arrives at the Main Line points giving access to the Siding until the journey is recommenced from such points. (ii) When the Siding is worked by Pilot from Warracknabeal, the charges shewn in (i) shall be imposed ; time to be computed from when the engine departs from Warracknabeal until its return thereto.
Electric Supply Co's. Pipe Site ...	216½	6 0	12 0	12 0	24 0	The Siding is situated ½ mile on "Down" side of Warracknabeal, and mileage rates to or from the Siding shall be charged in addition to the charges shewn in footnote (*a*) for the time engaged by an engine in each trip made to the Siding ; time to be computed from when the engine leaves the Warracknabeal Yard until its return thereto.
Vacuum Oil Co's. Siding	217	—	—	—	—	
Commonwealth Oil Refineries Ltd's. Siding	217	—	—	—	—	This Siding is an extension of the Vacuum Oil Coy's. Siding and is situated ½ mile on the "Down" side of Warracknabeal. Mileage rates to or from the Siding shall be charged in addition to the charges shewn in footnote (*a*) for the time engaged by an engine on each trip made to the Siding ; time to be computed from when the engine leaves Warracknabeal Yard until its return thereto.
H. C. Sleigh Ltd's. Siding	217	—	—	—	—	This Company has been granted permission to also use the Siding operated by the Vacuum Oil Co. and Commonwealth Oil Refineries Ltd. Mileage rates to or from the Siding shall be charged in addition to the charge shewn in footnote (*a*) for the time engaged by an engine on each trip made to the Siding ; time to be computed from when the engine leaves Warracknabeal Yard until its return thereto. In the event of trucks for more than one Company being placed at the one time, the charge shall be apportioned according to the number of trucks placed for each Company.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
WARRAGUL—						
Holdensen and Nielson Siding ...	62¼	6 0	12 0	—	—	To be imposed only when the trucks that are placed in the Siding are not fully loaded.
Holdensen and Nielson's Oil Discharging Site	62¼	24 0	24 0	—	—	
Holdensen and Nielson Siding, a/c Shell Co. of Australia	62¼	6 0	12 0	24 0	48 0	
Vacuum Oil Co's. Site	62¼	6 0	12 0	12 0	24 0	
Caltex Oil (Aust.) Pty. Ltd. Siding ...	‡	—	—	—	—	‡ Freight shall be charged for the mileage to or from Warragul, plus ¼ of a mile. The Siding will be worked by an engine from Warragul and, in addition to freight, the charges shewn in footnote (*a*) shall be imposed for the time that the engine is engaged in placing trucks at or removing trucks from the Siding ; time to be computed from when the engine leaves Warragul until its return thereto.
Commonwealth Oil Refineries Oil Discharging Site	62¼	6 0	12 0	12 0	24 0	
WARRNAMBOOL—						
Pier (see Volume I, page 179)						
Vacuum Oil Co., Atlantic Union Co. and Commonwealth Oil Refineries Ltd's. Siding	166	—	—	—	—	The charges shewn in footnote (*a*) shall be made ; time to be computed from when the engine commences collecting the necessary trucks at the Station Yard until such trucks are returned to their original positions in the Yard.

						In the event of trucks for more than one of these Companies being placed at the one time, the charge to cover such placings shall be apportioned between the Companies concerned.
Briar Ltd. ... ... ,, ,, *a/c A.P.M. Forests Pty. Ltd.*	166¾	6 0	12 0	24 0	—	1. To be imposed when done in conjunction with the placing of trucks at the Woollen Mills Siding. 2. If trucks are placed on the Siding by an engine which does not require to place trucks at the Woollen Mills Siding, the charges shewn in footnote (*a*) shall be imposed for the time the engine is engaged for each placing of trucks at the Siding ; time to be computed from when the engine departs from Warrnambool Yard until its return thereto. 3. Traffic between this Siding and the Pier or Warrnambool Station shall be charged the rates applicable to traffic between the Pier and Warrnambool Station (see Volume 1, page 179), plus the shunting charges shewn in Clause 1.
Woollen Mills Siding	166½	6 0	12 0	—	—	Open for General Goods. Traffic between the Siding and the Pier, or Warrnambool Station, shall be charged the rates applicable to traffic between the Pier and Warrnambool Station (see Volume 1, page 179), plus shunting charges.
Saltau and Sons Grain Store, a/c Nestles Food Specialities (Aust.) Ltd.	166	6 0	12 0	18 0	24 0	
Dehydration Factory Siding on Pier Line ...	167	6 0	12 0	24 0	48 0	To be imposed in addition to freight which shall be computed on the mileage to the Siding, with the exception that traffic between the Siding and Warrnambool Pier or Warrnambool Station shall be charged the rates applicable to traffic between the Pier and Warrnambool Station (see Volume 1, page 179), plus shunting charges.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (mimimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
WARRNAMBOOL—*continued.*						
Caltex Oil (Aust.) Pty. Ltd. Siding ...	‡	6 0	12 0	12 0	24 0	1. ‡ Freight charges shall be computed on the mileage to or from Warrnambool, plus 1½ miles. 2. The charges shown shall be imposed when placing is done by a passing train. 3. If placing or clearing be done by Special engine from Warrnambool. the charges shewn in footnote (*a*) shall be imposed ; time to be computed from when the engine leaves Warrnambool until its return thereto.
Gas Supply Coy. Ltd.	166	6 0	12 0	24 0	—	
Gepps' Shed Site on Pier Line ...	167¼	6 0	12 0	24 0	48 0	The charge of 7s. 4d. per ton for haulage of traffic from the Pier to the Station shall not be imposed unless the traffic is railed to the Pier for shipment or to the Station for local delivery.
Commonwealth Oil Refineries Ltd. Site in Station Yard	166	6 0	12 0	18 0	24 0	
Cattle Siding	167	—	—	—	—	The freight on Live Stock forwarded to or from the Warrnambool Cattle Siding shall be computed on this mileage.
WAUBRA—						
R. Torney's Mill Site	95¼	12 0	—	—	—	
WERRIBEE—						
E. C. Robertson and Co's. Siding ...	20	6 0	12 0	12 0	24 0	
E. C. Robertson and Co's. Siding a/c R. M. Watson and Son	20	6 0	12 0	12 0	24 0	
Stockley & Coy. Pty. Ltd. Shed Site ...	19¾	6 0	12 0	12 0	24 0	
Defence Department Siding	19¾	6 0	12 0	12 0	24 0	

WESTALL—						
Martin & King's Pty. Ltd's. Siding ...	‡	—	—	—	—	‡ The Siding is situated between Clayton and Spring Vale and is distant 14½ miles from Spencer Street. Freight charges at Ordinary tariff rates shall be computed as follows :— (1) *Inwards Traffic*— (*a*) *From stations on the "Up" side of the Siding*—at the mileage to the Siding. (*b*) From stations on the "Down" side of the Siding—at the mileage to the Siding, plus 2½ miles. (2) *Outwards Traffic*— (*a*) To stations on the "Down" side of the Siding—at the mileage from the Siding. (*b*) To stations on the "Up" side of the Siding—at the mileage from the Siding, plus 2½ miles. In addition to the freight charges the charges shewn in footnote (*a*) shall be imposed for the time the engine is engaged in each placing of trucks on the Siding ; the time to be computed from when the train arrives at the points giving access to the Company's Siding until the train is ready to depart on completion of shunting operations.
WEST FOOTSCRAY—						
Schutt and Barrie Pty. Ltd. Siding ...	4½	6 0	12 0	12 0	24 0	
Footscray Council's Storage Site ...	4½	6 0	12 0	12 0	—	
Parkinson Stove Co. (Aust.) Ltd's. Siding	‡	6 0	12 0	12 0	24 0	‡ Freight charges shall be computed on the mileage to or from West Footscray, plus ½ mile.
Southern Can Co. (Aust.) Pty. Ltd. Siding	4½	6 0	12 0	12 0	24 0	
Munistone (Ministry of Munitions Sidings)	‡	—	—	—	—	‡ 1. Freight charges at Ordinary tariff rares shall be computed on the mileage to or from West Footscray, plus, Munistone 2 miles, Maidstone 1½ miles, with the following exceptions :— (i) Traffic between these Sidings and Melbourne Goods Sheds shall be charged 11s. 11d. per ton (see Note *). (ii) Traffic between these Sidings and Port Melbourne Pier shall be charged 16s. 8d. per ton (see Note *). Note *—This Special rate is subject to the following minimum tonnages for freight purposes :— 7 tons per 11-ton truck. 10 tons per 16, 22 or 27-ton truck. 14 tons per "QR" truck. 20 tons per "Q" or "E" truck.
Maidstone (Dept. of Army Sidings at Tottenham)						

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
WEST FOOTSCRAY—*continued.* *Dept. of Army Sidings—continued.*						2. In addition to the freight charges, the charges shewn in footnote (*a*) shall be imposed for the time an engine is engaged in placing trucks on or removing trucks from the Sidings; time to be computed from when the engine leaves Tottenham Gravitation Yard until its return thereto. 3. If the placing of trucks on or removing of trucks from the Sidings be done in conjunction with the placing of trucks on or removing of trucks from the Department of Army Sidings at Maidstone, or R.A.A.F. Stores Depot at Rafstone, the charge shall be borne proportionately by the Departments concerned.
Rafstone (R.A.A.F. Stores Depot Sidings at Tottenham)	‡	—	—	—	—	‡ 1. Freight charges shall be computed on the mileage to or from West Footscray, plus 2 miles. 2. In addition to the freight charges, the charges shewn in footnote (*a*) shall be imposed for the time the engine is engaged in placing trucks on or removing trucks from the Sidings; time to be computed from when the engine leaves Tottenham Gravitation Yard until its return thereto. 3. See Munistone and Maidstone (Clause 3) above. 4. The Department of Air has erected a gate across each end of this Siding and has arranged for these gates to be opened for the passage of trains on regular train days. If the gates are required to be opened at any other times, the necessary arrangements will be made by the R.A.A.F. Ordnance Officer ('phone MW. 3351), provided not less than 30 minutes' notice is given by the Officer-in-Charge, Tottenham Yard.

						On any occasion on which an engine is delayed at the gates owing to the above arrangements not being complied with by the Department of Air, a detention charge as shewn in footnote (*a*), for the time the engine is delayed, shall be imposed.
Creamoata Ltd's. Siding	‡	—	—	—	—	‡ 1. Freight shall be computed on the mileage to or from West Footscray, plus ½ mile. 2. In addition to the freight, the charges shewn in footnote (*a*) shall be imposed for each placing of trucks on the Siding,; time to be computed from when the engine commences the service until its return to the original point.
Goldsborough Mort and Co. Ltd. ...	‡	—	—	—	—	‡ 1. Freight shall be imposed on the mileage to or from West Footscray, plus ½ mile, with the exception that Wool from stations other than Melbourne shall be charged the same rate as from the forwarding station to the Melbourne Goods Sheds. 2. In addition to the freight, the charges shewn in footnote (*a*) shall be imposed ; time to be computed from when the engine leaves Tottenham Yard until its return thereto, or should the work be done by the regular West Footscray Pilot engine, time to be computed from when the engine leaves West Footscray Yard until its return thereto.
WILLIAMSTOWN PIER—						
Jas. Seymour and Co. Pty. Ltd. Shed Site	9¾	6 0	12 0	12 0	24 0	
Easton Miscamble and Co. Shed Site ...	9¾	6 0	12 0	12 0	24 0	
Thompsons (Castlemaine) Ltd's. Siding	9¾	6 0	12 0	—	—	
Thompsons (Castlemaine) Ltd's. Siding, a/c Dept. of Navy	9¾	6 0	12 0	—	—	When it is necessary for a shunter to obtain the keys of the gates from the Navy Department's Watchman and return them thereto in connection with the operation of traffic beyond the gate, a detention charge of 30s. 10d. shall be imposed.
Thompsons (Castlemaine) Ltd's. New Extension Siding	9¾	6 0	12 0	—	—	To be imposed for all trucks placed on the principal Siding at the entrance to New Siding.
Bunge (Aust.) Pty. Ltd. Shed Site a/c Jas. Seymour and Co. Pty. Ltd.	9¾	6 0	12 0	12 0	24 0	
Ship Building Yard's Siding	9¾	—	—	—	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
WILLIAMSTOWN PIER—*continued.*						
Nelson Pier	9¾	—	—	—	—	In addition to freight charges, a detention charge at the rates shewn in footnote (*a*) shall be imposed for the time that an engine engaged in the operation of traffic to Nelson Pier is delayed at the gates, and debited to the Naval Dock Yard.
WINDSOR—						
State Electricity Commission's Site ...	5	6 0	12 0	—	—	
Australasian Conference Association Ltd.	5	6 0	12 0	12 0	24 0	
WODONGA—						
Commonwealth Oil Refineries Ltd. Site ...	187	—	—	—	—	In addition to the freight charges, a detention charge at the rates shewn on footnote (*a*) shall be imposed on each occasion that an engine is employed placing trucks on the Siding ; time to be computed from when the engine leaves Wodonga until its return thereto.
Ampol (Alba) Petroleum Pty. Ltd. Siding	187	6 0	12 0	24 0	—	
Caltex Oil (Aust.) Pty. Ltd.	‡189½	6 0	12 0	12 0	24 0	‡ 1. Freight charges shall be computed as follows :— (i) On Inwards traffic from a Victorian station to Siding, actual mileage to the Siding at the Victorian classification rate (terminal charges to be allowed to New South Wales). (ii) On Outwards traffic from the Siding to Victoria, at tariff rates on the total mileage as from the Siding via Albury, at Victorian classification rates (ternimal charges to be allowed New South Wales). (iii) On Inwards traffic from stations in New South Wales to the Siding, at New South Wales tariff rates from such stations to Albury for New South Wales proportion, plus tariff rates at Victorian classification for 1 to 10 miles from Albury to

						the Siding (terminal charges to be allowed to New South Wales). (iv) On Outwards traffic from Siding to stations in New South Wales, including Albury, on the mileage from Siding to Albury ; 1 to 10 miles at Victorian classification rate, plus New South Wales tariff rate, Albury to destination station (terminal charges to be allowed to New South Wales).
Atlantic Union Oil Coy. Ltd. Siding ...	187	—	—	—	—	In addition to freight, the charges shewn in footnote (*a*) shall be imposed on each occasion an engine is employed placing trucks on the Siding ; time to be computed :— (i) When the placing is done by a Special trip from Wodonga— From when the engine leaves Wodonga until its return thereto. (ii) By a Pilot engine working at the Siding— From when the engine enters the Siding until its return to the Main line. (iii) If placing for the Company be done in conjunction with the placing for other Companies, the charge shall be borne proportionately by each, in accordance with the number of trucks placed on each Siding.
Shell Co. of Australia Ltd's. Storage Site at Albury	‡190½	6 0	12 0	12 0	—	‡ Freight shall be computed on the mileage as to or from Albury. The Stationmaster, Albury, shall collect the shunting charges and include the items in the Victorian Miscellaneous Debit Return. The Stationmaster, Wodonga, shall furnish T.R. 95 Return.
Vacuum Oil Co. Pty. Ltd. Siding ...	‡190½	6 0	12 0	24 0	—	
Department of Army Sidings—						
BANDIANA No. 1 BANDOLIER BANDIORD	See pages 111 to 113					
WOORINEN— *Woorinen Fruit Growers' Co-op. Co. Ltd. Siding*	222	—	—	—	—	The Siding Holder must accept delivery of Inward and give delivery of Outward trucks with the doors closed and secured, properly sheeted, and coupled together, ready for a straight pick-up, and clear of points at Up end of Loop Siding.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged.	Shunting Charges to be imposed in addition to freight. For each Truck whether loaded or empty, placed on the Siding. Per 4-wheeled Truck.	Per Bogie Truck.	For each placing. Minimum Charge.	Maximum Charge.	Remarks.
	Miles.	s. d.	s. d.	s. d.	s. d.	
WYCHEPROOF—						
Vacuum Oil Coy. Pty. Ltd's. Oil Discharging Site	190¼	6 0	12 0	12 0	—	
Vacuum Oil Coy. Pty. Ltd's. Oil Discharging Site a/c Atlantic Union Oil Coy. Ltd.	190¼	6 0	12 0	12 0	—	
YAAPEET—						
W. R. Westphalen Manure Shed ...	276¾	6 0	12 0	—	—	
H. J. Bowden's Gypsum Storage Site ...	276¾	6 0	12 0	24 0	48 0	These sites adjoin each other and when trucks are placed for each site holder at the same time, the minimum charge of 24s. 0d. is to be apportioned in accordance with the number of trucks placed for each site holder.
Edgar and Co. Pty. Ltd's. Oat Storage Site	276¾	6 0	12 0	24 0	48 0	
YALLOURN—						
State Electricity Commission of Victoria—Briquetting Siding	86¾	—	—	—	—	(i) No shunting charge shall be imposed when thirteen or more trucks are cleared from the Briquetting Siding by one engine operation.
	—	6 0	12 0	24 0	48 0	(ii) To be imposed for each engine operation in addition to freight when less then thirteen trucks are ready for clearance.
New Briquetting Siding	86¾	—	—	—	—	The charges shewn in footnote (*a*) shall be imposed for placing trucks in the Siding.
Pole Storage Site	86½	—	—	—	—	In addition to the freight charges, the charges shewn in footnote (*a*) shall be imposed for the time the engine is engaged in each placing of trucks on the Siding ; time to be computed from when the engine arrives at the points giving access to the Siding until its return thereto.

Commission's Store Siding (Cement, Woodwork and Sand Road)	86½	6 0	12 0	12 0	—	
Other Sidings (Turntable, Workshops, Shovel, and Dead-end Extension)	86½	6 0	12 0	12 0	—	
Brown Coal (Old Open Cut) ...	87½	—	—	—	—	i. No shunting charge shall be imposed when 13 or more trucks are cleared from the Siding by the one engine operation.
	—	6 0	12 0	24 0	48 0	(ii) To be imposed for each engine operation in addition to freight when less than 13 trucks are ready for clearance.
Use of Trucks at Local Sidings ...	—	—	—	—	—	*When trucks are utilized locally for the conveyance of any traffic to* the Commission's Sidings from the Public Siding in the Station Yard, or between any other points on the Commission's Sidings, a truck hire charge of 68s. 2d. per day for each 4-wheeled, and 136s. 4d. per day for each bogie truck, shall be made. These charges shall be imposed for each day until the trucks are released for Ordinary service.
Fuel Sales Depot Siding	87	6 0	12 0	12 0	—	
Metal Discharging Site	87¼	6 0	12 0	12 0	—	
YARRA JUNCTION—						
Victorian Hardwood Milling Co. Pty. Ltd's. Siding	43¼	—	—	—	—	
YARRAGON—						
King Bros. and Doig Pty. Ltd. Timber Storage Site	70	6 0	12 0	12 0	24 0	
YARRARA—						
State Rivers and Water Supply Commission's Oil Site	388	6 0	12 0	12 0	—	
YARRAWONGA—*Sites as under*—						
B. C. Keenan, Grain; Shire of Yarrawonga, Metal; W. Christie, Firewood; McPherson & Milton, Firewood; J. J. Clarke, Firewood; A. H. Foster, Firewood; J. Pigdon, Manure	161½	6 0	12 0	12 0	24 0	Empty trucks placed for Outward wheat are exempt from these shunting charges.
Grant and Wilson's Flour Mill Siding ...	161½	6 0	12 0	24 0	—	

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

Station and Siding or Site.	Mileage from Melbourne to be charged	Shunting Charges to be imposed in addition to freight.				Remarks.
		For each Truck whether loaded or empty, placed on the Siding.		For each placing.		
		Per 4-wheeled Truck.	Per Bogie Truck.	Minimum Charge.	Maximum Charge.	
	Miles.	s. d.	s. d.	s. d.	s. d.	
YELTA—						
Department of Air Siding	364	6 0	12 0	24 0	48 0	When a Special service is required to place and/or remove trucks from the Siding by Special Pilot engine from Mildura or Merbein, the charges shewn in footnote (*a*) shall be imposed for the time the engine is so engaged, the time to be computed from when the engine commences the Special service until its return to its originating place.
Water Conservation and Irrigation Commission Oil Discharging Site	364	6 0	12 0	24 0	48 0	If placing of trucks is done by ordinary Pilot engine. When a Special service is required to place and/or remove trucks from the Siding by Special Pilot engine from Mildura or Merbein, the charges shewn in footnote (*a*) shall be imposed for the time the engine is so engaged, the time to be computed from when the engine commences the Special service until its return to its originating place.
Yelta Stockyards	364	—	—	—	—	Freight charges shall be computed on the mileage to or from the Siding. In addition to freight, if a Special train is required to place or clear less than 15 trucks of Live Stock, a charge at the rate of 107s. 4d. per hour shall be imposed ; the time to be computed from when the train is scheduled to leave Mildura or Merbein, as the case may be, and its return thereto. No charge, other than the freight charges, shall be imposed if 15 or more trucks are placed or cleared at the one time.

(a) In addition to the freight a charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.) for the time that the engine is engaged on each trip to and from the siding.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

BANDIANA No. 1, BANDOLIER, BANDIORD.

Department of Army Sidings on Tallangatta line for the receipt and despatch of Defence traffic on both Victorian and New South Wales gauges and transhipment of certain Intersystem Defence traffic.

BANDIANA, No. 1.

1. Freight charges shall be computed as follows :—

(*a*) **On Local Traffic forwarded between Victorian Stations and Bandiana, *i.e.*, originating or terminating at Bandiana No. 1.**

(i) Between Bandiana No. 1 and Melbourne or Suburban stations :—at the Victorian mileage rates for the mileage to the Marshalling Yard, 189¼ miles from Melbourne, or at the District rates and conditions shewn in Volume 1 to Wodonga, plus mileage rates for 2¼ miles.

(ii) Between Bandiana No. 1 and other Victorian stations :—at the Victorian mileage rates to the Marshalling Yard, 189¼ miles distant from Melbourne.

(iii) The revenue from traffic in clauses (i) and (ii) will be all Victorian.

(*b*) **On Through Traffic forwarded between Victoria and New South Wales stations which is transhipped at Bandiana No. 1 en route.**

(i) Ammunition or other traffic forwarded direct between a Victorian station and a station in New South Wales or Queensland *via* Bandiana No. 1 for transhipment, shall be charged at Tariff Rate and conditions as follows :—

Victoria. For 187½ miles for the Victorian proportion for the journey Melbourne to Albury *via* Bandiana No. 1.

New South Wales. For 3 miles in addition to the mileage to or from Albury and the New South Wales station concerned.

(ii) Each State's proportion of the charges must be shown on waybills.

(iii) **Transhipping.**—The same rates as those prescribed for transhipping at Albury shall be imposed and dealt with as instructed in respect of this service at Albury. Such charges are to be raised on all traffic including that for Dept. of the Army, Dept. of Munitions or other Departments, even though the consignment note is marked "for transfer by the Army."

(*c*) **On Through Traffic forwarded between New South Wales or Queensland Stations and Bandiana No. 1.**

(i) At the respective mileage rates for the systems concerned, which shall be credited with the revenue so computed to New South Wales, or Queensland.
(Bandiana No. 1 is 408 miles from Darling Harbour.)

(*d*) **On Traffic forwarded between Albury and Bandiana No. 1.**

(i) At the New South Wales rates and conditions for 1–10 miles. The revenue therefrom will be all New South Wales.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

BANDIANA NO. 1—*continued.*

2. SHUNTING CHARGES. In addition to the freight charges, the following shunting charges shall be imposed :—

(i) **Victorian.**

(*a*) Traffic between Victorian stations and Bandiana No. 1 will generally be placed, and the shunting of trucks between the Sidings within the depot performed, by special engine service from Wodonga.

Except as indicated in clause (*b*) hereof, shunting charges as follows, in addition to freight charges, shall be imposed for the time the Victorian engine is engaged in such services ;

30s. 10d. for the first fifteen (15) minutes ;

9s. 4d. for each subsequent five (5) minutes or part thereof, minimum charge 30s. 10d.; time to be computed from when the engine leaves Wodonga until its return thereto.

(*b*) No shunting charges shall apply to traffic in train loads, minimum 15 trucks, carried by special train to any one siding in the Bandiana No. 1 Army Depot.

(ii) **New South Wales.**

New South Wales staff will raise debit for N.S.W. shunting charges where due as prescribed in the New South Wales Merchandise Rates Book.

BANDOLIER—

Mileage from Melbourne—191¼‡.

" " Darling Harbour—410‡.

BANDIORD—

Mileage from Melbourne—190½‡.

" " Darling Harbour—408‡.

‡ Freight charges shall be computed as follows :—

(i) Between Bandolier or Bandiord and Melbourne or Suburban stations—

At the mileage rates to and from the siding, unless it be cheaper to charge the District rates and conditions provided in Volume 1 to Wodonga, plus the mileage rates for 4½ miles for Bandolier, or 3½ miles for Bandiord.

(ii) Between Bandolier or Bandiord and other Victorian stations—

At the mileage rates to or from the Siding.

The revenue for traffic forwarded between Albury and Bandolier and Bandiord will all be New South Wales.

Shunting Charges, Etc., Applicable at the Undermentioned Stations—continued.

BANDOLIER and BANDIORD—*continued.*

In addition to the freight charges, the following shunting charges shall be imposed :—

(i) **Victorian.**

(*a*) Traffic between Victorian stations and Bandolier or Bandiord will generally be placed, and the shunting of trucks between the Siding within the depot performed, by special engine service from Wodonga.
Except as indicated in clause (*b*) hereof, shunting charges as follows, shall be imposed for the time the Victorian engine is engaged in such services :—

30s. 10d.for the first 15 minutes ;

9s. 4d. for each subsequent five minutes or part thereof, minimum charge 30s. 10d. ; time to be computed from when the engine leaves Wodonga until its return thereto.

(*b*) No shunting charges shall apply to traffic in train loads, minimum 15 ordinary 4-wheeled trucks, or 13 flat bogie trucks, carried by special train to Bandolier or Bandiord.

(ii) **New South Wales.**

New South Wales staff will raise debit for N.S.W. shunting charges as prescribed in the New South Wales Merchandise Rates Book.

Rocla Limited Siding (Bandiana).
State Electricity Commission Siding of Victoria (Bandiana).

Freight charges shall be computed as follows :—

(*a*) Between the Siding and Melbourne and Suburban stations. On the mileage as to or from Bandiana, *i.e.*, 189¼ miles from Melbourne or at the District Rates to Wodonga, plus mileage rates for 2¼ miles.

(*b*) Between the Siding and other Victorian stations. At the mileage rates to or from the Siding.
In addition to the freight charges, a shunting charge shall be imposed of 30s. 10d. for the first 15 minutes and 9s. 4d. for each 5 minutes or part thereof beyond the first 15 minutes (minimum charge 30s. 10d.), for the time the engine is engaged for each placing of trucks on the Siding ; time to be computed from when the engine leaves Wodonga until its return thereto. These shunting charges will not apply to a train load, that is, not less than 15 fully loaded 4-wheeled trucks, hauled by special train from Wodonga to the Siding.
When trucks are placed for Rocla Ltd. and/or State Electricity Commission, at the same time as trucks are placed for the Department of Army on this Siding, charges shall be apportioned according to the number of trucks placed for each.

Newport—Sunshine Loop Line Sidings (Mileages).

The Newport–Sunshine Loop is not to be used in the computation of the mileages for freight purposes except in respect of traffic to or from the undermentioned Sidings on the Loop, the rates for which shall be computed on the shortest mileage *via* Newport or *via* Sunshine.

The respective Sidings and mileages from Newport, Sunshine and Broadmeadows are as shown hereunder :—

NAME OF SIDING HOLDER	From Newport	From Sunshine	From Broadmeadows (via Sunshine)
	Miles	Miles	Miles
W. C. Thomas & Sons Pty. Ltd.	½	4¼	13¾
Caltex Oil (Aust.) Pty. Ltd.	½	4¼	13¾
McKenzie and Holland (Aust.) Pty. Ltd. ...	1	3¾	13¼
Brookwood (Forests Commission Firewood Storage Site)	1½	3¼	12¾
Brooklyn Quarries *a/c* Sulphates Pty. Ltd.	1½	3¼	12¾
Wright, Stephenson & Co. (Aust.) Pty. Ltd.	1½	3¼	12¾
James Hardie and Co.	2½	2½	12
South Brooklyn (Borthwick Freezing Co.)	2½	2½	12
Little Brooklyn (Livestock only) ...	2½	2¼	11¾
Melbourne Iron & Steel Mills Pty. Ltd. ...	2½	2¼	11¾
Ready Mixed Concrete Limited ...	2¾	2	11½

NAME OF SIDING HOLDER	From Newport	From Sunshine	From Broadmeadows (via Sunshine)
	Miles	Miles	Miles
Prossor (Western & Murray Co-op. Bacon Co.)	2¾	2	11½
North Brooklyn—(N.S. Smorgan & Sons)	2¾	2	11½
S. J. Willis Pty. Ltd.	3¼	1½	11
Braybrook Quarries	3¼	1½	11
Melbourne Machinery Co. Pty. Ltd. ...	3¼	1½	11
Melbourne Machinery Co. Pty. Ltd. *a/c* W. M. Butterfield Pty. Ltd.	3¼	1½	11
Highfield's Quarry	3¼	1½	11
Armbrook	3¼	1½	11
Western Metropolitan Market Trusts Siding (Melbourne Pig and Calf Market)	4¼	1½	11
Stanley Quarries Pty. Ltd.	4¼	1½	11
Stanley Quarries Pty. Ltd. *a/c* Consolidated Concrete Co.	4¼	1½	11
Monsanto (Aust.) Pty. Ltd.	4¼	1½	11
Commonwealth Quarry	4	¾	10¼

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In witness whereof the Common Seal of the Victorian Railways Commissioners was affixed hereto this 29th day of September.

In the year of our Lord, One thousand nine hundred and fifty four in the presence of

(Seal) R. G. Wishart, O. G. Meyer, E. H. Brownbill — Victorian Railways Commissioners

Confirmed by the Governor in Council the 5th October, 1954

A. Mahlstedt
Clerk of the Executive Council

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GOVERNMENT GAZETTE.

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No. 929] **TUESDAY, OCTOBER 12.** **[1954**

Prices Regulation Acts.

PRICES REGULATION ORDER No. 653.

PAINTS—SALES BY RETAIL.

IN pursuance of the powers conferred upon me by the Prices Regulation Acts, I, John Francis Waldron, Prices Commissioner, hereby make the following Order :—

Citation.

1. This Order may be cited as Prices Regulation Order No. 653.

Partial Revocation of Previous Orders.

2. Nothing contained in Prices Regulation Order No. 199 or in No. 267 or in any notice in writing issued thereunder shall apply to any paints the maximum price for which is fixed by or under this Order.

Definitions.

3. In this Order, unless the contrary intention appears—

"Frankston Area" means all that area comprised within a radius of one mile from the Post Office, at Frankston.

"Free Delivery Area" means the Metropolitan Area, the Geelong Area, the Seaford Area, the Frankston Area, and all that part of the City of Chelsea which is situated outside the Metropolitan Area.

"Geelong Area" means all that Area comprised within a radius of five miles from the principal Post Office at Geelong, Victoria.

"Metropolitan Area" means all that area comprised within a radius of twenty miles from the General Post Office, Melbourne.

"Point of Delivery" means, in relation to the sale of paint, the place at which liability for payment of transport charges in conveying such paint to his place of business passed to the seller from the person from whom he purchased it.

"Seaford Area" means all that area comprised within a radius of one mile from the Post Office at Seaford.

Maximum Prices—Sales in Free Delivery Area.

4. I fix and declare the maximum price at which paints of any of the descriptions specified in the Schedule to this Order may be sold by retail in the Free Delivery Area to be the appropriate prices specified in that Schedule.

Maximum Prices—Sales outside Free Delivery Area.

5. I fix and declare the maximum price at which paints of any of the descriptions specified in the Schedule to this Order may be sold by retail outside the Free Delivery Area to be the appropriate price specified in that Schedule plus the cost actually incurred in respect of and properly attributable to the transport of such paints from the point of delivery to the retailer's place of business and which is clearly recorded in his books of account or records.

Exhibition of Price List.

6. Every person who sells or has for sale by retail in Victoria any paints, the maximum price of which is fixed by or under the provisions of this Order, shall exhibit and keep exhibited in a prominent position in his place of business or, if he has more than one place of business, in each of his places of business, in the form of a price list and in such manner as to be easily legible to persons contemplating making any purchase or conducting any business at his place or places of business particulars of the maximum prices fixed by or under the provisions of this Order for the sale by him of these paints.

Fixation of Maximum Prices by Notice.

7. Notwithstanding the foregoing provisions of this Order, I declare the maximum prices at which any paints which are specified in a notice in writing given in pursuance of this clause may be sold by any person to whom such notice is given to be such price as is fixed by the Commissioner by notice in writing to that person.

THE SCHEDULE.

Description.	Maximum Retail Prices.						
	4 or 5 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	⅛ Gallon Tin per Tin.	1/16 Gallon Tin per Tin.	1/32 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Products of any of the following Manufacturers, viz.:—British Australian Lead Manufacturers Pty. Ltd., Australasian United Paint Company Pty. Ltd., The Muralo Company (Australia) Pty. Ltd.—							
Dulux Hi-Gloss—All colours other than Bright Red or Chinese Red	..	66 6	35 0	18 5	9 8	5 6	..
Bright Red or Chinese Red	..	74 0	38 9	20 3	10 8	5 11	..
Dulux Hi-Gloss Undercoat	..	54 6	29 0	15 5	8 2	4 9	..
Dulux Super Enamel Finish—All colours other than black ..	..	77 0	40 3	21 0	11 0	6 1	3 9
Black	..	73 4	38 5	20 1	10 7	5 11	3 7
Dulux Super Satin Finish—All colours ..	..	64 3	33 11	17 10	9 5	5 4	..
Dulux Super Matt Finish —All colours ..	..	61 6	32 6	17 2	9 1	5 2	..
Dulux Super Seal ..	..	49 6	26 6	14 2	7 7	4 5	..
Dulux Undercoats—							
Grey, 366–26 ..	..	53 6	28 6	15 2	8 1	4 8	3 0
White, 366–33 ..	..	53 6	28 6	15 2	8 1	4 8	3 0
Ivory, 366–215 ..	..	53 6	28 6	15 2	8 1	4 8	3 0
Pure Prepared Paint—							
All colours other than Vermilion ..	..	56 6	30 0	15 11	8 5	4 10	..
Vermilion	..	63 6	33 6	17 8	9 4	5 3	..
Undercoats or Pink Primer	..	54 6	29 0	15 5	8 2	4 9	..
Roof Paint—							
Reds, other than Super Anti-corrosive ..	..	53 0	28 3	15 0	8 0	..	..
Super Anti-corrosive Red, Aluminium, Grey	..	57 0	30 3	16 0	8 6	..	..
Chromium Greens ..	..	61 0	32 3	17 0	9 0	..	..
Interior Muraltone Plastic	41 0	42 6	23 0	12 5	6 8	..	..

THE SCHEDULE—*continued.*

Description.	Maximum Retail Prices.						
	4 or 5 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	⅛ Gallon Tin per Tin.	1/16 Gallon Tin per Tin.	1/32 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Products of any of the following Manufacturers, viz.:—British Paints (Australia) Pty. Ltd., Taylor's Paints Pty. Ltd.							
"Supalusta," "Superlative," Super Enamelized Colours—Gloss White, Off White, Old Ivory, Cream, Sunshine Yellow, Black, Chocolate Brown, Eastern Green, Biscuit, Timor Blue, Dove Grey, Primrose, Windsor Green, Barrier Coral, Morning Grey, Blue Mist, Pastel Green, Soft Primrose ..	62 6	64 0	33 9	17 10	9 5	5 5	..
Fadeless Green, Jade Green	70 0	71 6	37 6	19 8	10 5	5 11	..
Burgundy and Royal Blue	67 9	69 3	36 4	19 1	10 0	5 8	..
Clear	54 3	55 9	29 8	15 8	8 5	4 11	2 0
Bright Red, Carmen Red	72 6	74 0	38 0	20 3	10 9	6 0	..
Prepared Paints—"Taylors," "British" Gloss White, Flat White, Ivory, Black, and Standard Colours Nos. 19, 20, 21, 22, 23, 25, 28, 29, 32, 33, 39, and 40 ..	54 0	55 6	29 6	15 8	8 4	4 10	..
Bright Green No. 34, Mid-Green No. 35, Permanent Eau de Nil No. 37, Fast Light-blue No. 42 ..	56 8	58 2	30 10	16 4	8 8	4 11	..
Fadeless Green No. 43, Signal Red No. 31, Orange No. 44, Yellow	61 11	63 5	33 6	17 7	9 4	5 3	..
Roof and Structural Paint—							
Bright Red or Tile Red	50 8	52 2	27 10	14 10	7 11	..	..
White and Mid Grey..	59 0	60 6	32 0	16 11	9 0	..	..
Fadeless Green ..	68 4	69 10	36 8	19 3	10 2	..	..
Aluminium	62 6	64 0	33 9	17 10	9 5	..	..
Undercoats	51 0	52 6	28 0	14 11	8 0	4 8	..
"Supa-Flat" — Flat Enamel Wall Finish—All Shades ..	58 2	59 8	31 7	16 8	8 10	..	..
"Satin - Tone - Satin Finish Enamel—All Shades ..	58 2	59 8	31 7	16 8	8 10	..	..
"Walmasta" Interior Resinous Emulsion—All Shades ..	42 6	44 0	23 9	12 9	..	..	..
Products of any of the following Manufacturers, viz.:—Glazebrooks Paints (Australia) Pty. Ltd., Glazebrooks Paint and Chemicals Ltd.							
Tanox Super Gloss Paint—							
White, Creams, Stones, Greys, Blues ..	65 0	66 6	35 0	18 5	9 8	5 6	..
Greens, Browns, Reds	..	69 0	36 3	19 0	10 0	5 7	..
Scarlet, Orange, and Yellow	..	74 6	39 0	20 5	10 8	5 11	..
Tanox Undercoat—White or Colours ..	55 6	57 0	30 3	16 0	8 6	4 11	..
Tanox Primers—White or Pink ..	55 6	57 0	30 3	16 0	8 6	4 11	..
Aluminium Wood Primer	53 0	54 6	29 0	15 5	8 2	..	..

THE SCHEDULE—*continued.*

Description.	Maximum Retail Prices.						
	4 or 5 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	⅛ Gallon Tin per Tin.	1/16 Gallon Tin per Tin.	1/32 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Rusta Resta Roof Paint—							
Ferric Red ..	52 6	54 0	28 10	15 3	..	..	..
Light Grey ..	63 6	65 0	34 3	18 0	..	..	..
Permanent Green ..	75 6	77 0	40 3	21 0	..	..	..
Aluminium ..	59 6	61 0	32 3	17 0	..	..	..
Spred Satin—All colours	..	66 6	35 0	18 5	9 8	..	..
Permoglaze—							
Interior Gloss Finish—							
All colours ..	74 0	75 6	39 6	20 8	10 10	6 0	3 8
Clear Gloss Finish ..	..	54 6	29 0	15 5	8 3	4 9	3 0
Satin Finish—All Colours	..	62 6	33 0	17 5	9 2	5 3	..
Matt Finish—All Colours	..	59 0	31 3	16 6	8 9	5 0	..
Primer No. 1—All Colours	56 6	58 0	30 9	16 3	8 8	5 0	..
Undercoat No. 2 ..	56 6	58 0	30 9	16 3	8 8	5 0	..
Products of any of the following Manufacturers, viz.:—Goodlass, Wall and Co. Pty. Ltd., Alexander Ferguson Pty. Ltd.							
"Goodlass" Valspar Exterior and Ricksha Magicolour—							
All colours other than Bright Red ..	..	66 6	34 11	18 5	9 8	5 6	..
Bright Red ..	..	73 0	38 2	20 1	10 6	5 10	..
"Ricksha" Magicolour—							
Undercoats General Colours	53 0	54 6	29 0	15 4	8 2	4 9	..
Metal Primer ..	..	64 9	34 2	17 11	9 5	5 4	..
"Goodlass" and "Elephant" Paint—							
All colours other than Red, Yellow, Permanent Greens ..	56 9	58 3	30 10	16 4	8 8	5 0	..
Red, Yellow ..	..	73 9	38 7	20 2	10 7	6 0	..
Permanent Greens ..	..	69 9	36 7	19 2	10 1	5 9	..
Roof Paints—							
Red	51 3	52 9	28 1	15 0	7 11	..	..
Permanent Green ..	..	70 3	36 10	19 4	10 2	..	..
White or Grey ..	59 9	61 3	32 4	17 1	9 0	..	..
Bituminous Aluminium ..	41 0	42 6	23 0	12 5	6 8	..	..
Aluminium ..	55 3	56 9	30 1	16 0	8 5	4 11	..
Combinol, "Ricksha"—							
Flat Finish—							
All colours other than Midnight Blue, Wattle Yellow, Flame, Royal Red, Terra-cotta, Forest Green, Ebony Black ..	57 9	59 3	31 4	16 7	8 10	5 0	..
Midnight Blue, Wattle Yellow, Flame, Royal Red, Terra-cotta, Forest Green, Ebony Black	..	62 3	32 10	17 4	9 2	5 2	..
Satin Finish, Satinkote ..	..	62 3	32 10	17 4	9 2	5 2	..
Enamel, Glosskote ..	..	66 6	34 11	18 5	9 8	5 6	..
"Valspar" Plastic Matt—							
Flat—All colours other than Midnight Blue, Wattle Yellow, Flame, Royal Red, Terra-cotta, Forest Green, Ebony Black	..	54 0	28 9	15 3	8 2	..	..
Satin—All colours other than Midnight Blue, Wattle Yellow, Flame, Royal Red, Terra-cotta, Forest Green, Ebony Black	..	62 9	33 2	17 6	9 2	..	..
Midnight Blue, Wattle Yellow, Flame, Royal Red, Terra-cotta, Forest Green, Ebony Black	..	72 0	37 9	19 9	10 5	5 10	..

THE SCHEDULE—*continued.*

Description.	Maximum Retail Prices.						
	4 or 6 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	⅛ Gallon Tin per Tin.	1/16 Gallon Tin per Tin.	1/32 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Products of any of the following Manufacturers, viz.:—Major Bros. and Co. (Vic.) Pty. Ltd., Major Bros. and Co. Pty. Ltd.							
Majora Pure Mixed Paint—							
Nos. 801, 802, 803, 805, 806, 807, 808, 809, 812, 814, 817, 825, 826, 827, 828, 829	..	56 6	30 0	15 11	8 6	4 10	..
Interior Flat White ..	..	52 6	28 0	14 11	8 0	4 7	..
818–819	..	62 9	33 2	17 6	9 3	5 3	..
Permanent Green ..	..	65 9	34 8	18 3	9 7	5 5	..
Permanent Red ..	..	72 6	38 0	19 11	10 6	5 10	..
Majora Undercoat—							
White, Cream, Light Grey	..	52 6	28 0	14 11	8 0	4 7	..
Green	..	55 0	29 3	15 7	8 3	4 9	..
Majora Pink Primer ..	..	52 6	28 0	14 11	8 0	4 7	..
"Majora" Majorgloss—							
Enamelized colours—							
All colours other than Jasmine Yellow, Brilliant Green, Cypress Green, Fresco Blue, Sunproof Red ..	..	64 6	34 0	17 11	9 6	5 4	..
Jasmine Yellow, Brilliant Green, Cypress Green, Fresco Blue ..	..	67 3	35 5	18 7	9 10	5 6	..
Sunproof Red ..	..	72 6	38 0	19 11	10 6	5 10	...
"Majora" Undercoating for Majorgloss—							
White, Cream, Light Grey, Dark Grey	..	54 6	29 0	15 5	8 3	4 9	..
Green	..	56 6	30 0	15 11	8 6	4 10	..
"Majora" Plastic Paint—							
Interior Flat—							
All colours other than Mistletoe Green, Fresco Blue, or Terra-cotta ..	..	62 0	32 9	17 4	9 2	..	..
Mistletoe Green, Fresco Blue, Terra-cotta ..	..	66 0	34 9	18 4	9 8	5 5	..
"Majora" Plastic Paint—							
Glaze Finish—							
All colours other than Mistletoe Green, Fresco Blue, or Terra-cotta ..	..	77 0	40 3	21 1	11 1	..	..
Mistletoe Green, Fresco Blue, and Terra-cotta ..	..	79 6	41 6	21 8	11 4	6 6	..
Products of any of the following Manufacturers, viz.:—Lewis Berger and Sons (Australia) Pty. Ltd., Lewis Berger and Sons (Victoria) Pty. Ltd., Sherwin-Williams Co. (Australia) Pty. Ltd., Rogers Paint and Varnish Co.							
Bergermaster, Rogermaster, Shermaster—							
All colours other than Mail Red or Poinsettia ..	..	66 6	35 0	18 4	9 8	5 6	...
Mail Red, Poinsettia	..	74 0	38 9	20 3	10 8	5 11	...
"B.P.," "S.W.P.," "Roger Three Star" ..	55 0	56 6	30 0	15 11	8 5	..	..
Undercoat	52 0	53 6	28 6	15 2	8 1	4 8	..
Pink Primer	52 0	53 6	28 6	15 2	8 1	4 8	..

THE SCHEDULE—*continued.*

Description.	Maximum Retail Prices.						
	4 or 5 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	¾ Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	1/8 Gallon Tin per Tin.	1/16 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Kemtone Interior Finish—							
All colours other than Wedgwood Blue, Sunshine Yellow, Pinehurst Green, Monterey Red ..	..	44 6	24 0	12 11	...	...	..
Wedgwood Blue, Sunshine Yellow, Pinehurst Green, Monterey Red ..	..	48 6	26 0	13 10	7 5	..	..
Kem-Glo Full Gloss—All Colours	..	65 0	34 3	18 0	9 6	5 4	3 4
Kem-Glo Interior Varnish	..	42 0	22 9	12 3	6 8	3 11	2 8
Kem-Glo Semi-Lustre ..	..	61 6	32 6	17 3	9 0	5 3	..
Kem-Glo Surfacer Undercoat	..	51 0	27 3	14 6	7 9	4 6	..
Roofmaster Roof Paint—							
All colours other than Chromium Green ..	57 0	58 6	31 0	16 4	..	..	..
Chromium Green ..	67 6	69 0	36 3	19 0	..	..	..
Products of the following Manufacturer, viz.:— Smith and Walton (Aust.) Pty. Ltd.							
Synflat—							
All colours other than Flame, Chrome Green, Blue ..	59 6	61 0	32 3	17 0	9 0	..	..
Flame, Chrome Green, Blue	66 3	67 9	35 9	18 9	9 9	..	..
Synglaze—All colours ..	59 0	60 6	32 0	16 9	9 0	5 0	..
Hadrian Full Bodied Gloss	63 6	65 0	34 3	18 0	9 6	5 3	..
Hadrian Undercoat ..	51 0	52 6	28 0	14 9	8 0	4 6	..
Products of any of the following Manufacturers, viz.:— Spartan Paints Pty. Ltd., Nason Paint and Color Pty. Ltd., Hardie Trading Ltd.							
Hardex Gloss Paint—							
All colours other than Reds or Greens ..	59 6	61 0	32 3	17 0	9 0	5 2	..
Reds or Greens ..	63 0	64 6	34 0	17 11	9 5	5 5	..
Hardex Undercoats ...	52 0	53 6	28 6	15 2	8 0	4 8	..
Hardex Pink Primer ..	48 0	49 6	26 6	14 2	7 7	4 5	..
Spartan Plastic Paint—							
All colours other than machine tints and full-tone colours ..	46 0	49 3	26 6	13 9	..	..	..
Machine Tints ..	52 3	54 3	28 11	15 1	..	..	..
Full-tone Colours ..	63 3	66 6	35 0	18 3	..	..	..
Products of any of the following Manufacturers, viz.:— Tip Top Paints (Vic.) Pty. Ltd., Tip Top Paints (Australia) Ltd.							
Coloramic Gloss—							
All colours other than Cerise, Flame, Citron, Forest, Swiss Blue, Ebony, Nutmeg ..	62 6	64 0	33 9	17 9	9 5	5 4	..
Cerise, Flame, Citron, Forest, Swiss Blue, Ebony, Nutmeg ..	..	71 0	37 3	19 6	10 3	5 9	..
Undercoats—Exterior	51 0	52 6	28 0	14 9	8 0	4 6	..
Undercoats—Interior	48 6	50 0	26 9	14 3	7 8	4 5	..
Coloramic Matt—							
All colours other than Cerise, Flame, Citron, Forest, Swiss Blue, Ebony, Nutmeg, Clear	57 6	59 0	31 3	16 3	8 9	5 0	..
Cerise, Flame, Citron, Forest, Swiss Blue, Ebony, Nutmeg ...	..	62 0	32 9	17 3	9 2	5 3	3 3
Clear	..	49 0	26 3	14 0	7 6	4 5	..
Pink Primer ...	51 0	52 6	28 0	14 9	8 0	4 6	..

THE SCHEDULE—*continued.*

Description.	Maximum Retail Prices.						
	4 or 5 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	⅛ Gallon Tin per Tin.	1/16 Gallon Tin per Tin.	1/32 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Products of any of the following Manufacturers, viz.:—Sterling Paint and Varnish (Vic.) Pty. Ltd., Sterling Paint and Varnish Co. Pty. Ltd.							
Rayflex Long Gloss—							
All colours other than Green, Blue, Orange, Red	..	61 6	32 6	17 2	9 1	5 3	..
Green, Blue, Orange, Red	..	66 9	35 2	18 5	9 9	5 6	..
Rayflex Long Gloss Undercoat	..	52 0	27 9	14 9	7 9	4 6	..
Sterling Prepared Paint—							
All colours other than Signal and Carriage Red, Mid Weather, Dark Green ..	..	55 6	29 6	15 6	8 2	4 8	..
Signal and Carriage Red, Mid Weather, Dark Green ..	..	57 6	30 6	16 0	8 6	4 10	..
Pink Primer ..	..	52 0	27 9	14 9	7 9	4 6	..
Products of any of the following Manufacturers, viz.:—Taubmans (Vic.) Pty. Ltd., Taubmans (S.A.) Pty. Ltd. T.I.L. Services Pty., Ltd., Taubmans Industries Ltd.							
Butex—							
All colours other than Bristol Green, Mecca Green, Royal Blue, Turquoise, Orange, Lhasa, Burgundy, Durbar	..	64 0	33 9	17 9	9 5	5 4	..
Bristol Green, Mecca Green, Royal Blue, Turquoise, Orange, Lhasa, Burgundy ..	..	68 0	35 9	18 9	9 11	5 7	..
Durbar	..	74 0	38 9	20 3	10 8	5 11	..
Super Paint—	..						
All colours other than Greens, Orange, Mid Chrome, Brilliant Red ..	..	55 6	29 6	15 8	8 4	4 9	..
Greens	..	57 6	30 6	16 1	8 7	4 11	..
Orange, Mid Chrome	..	59 6	31 6	16 8	8 10	5 0	..
Brilliant Red ..	..	69 6	36 6	19 1	10 1	5 8	..
Exterior Undercoats—							
All colours other than Red, Green ..	..	52 6	28 0	14 11	7 11	4 7	..
Red	..	58 0	30 9	16 3	8 8	4 11	..
Green	..	54 6	29 0	15 5	8 2	4 9	..
Pink Primer ..	..	52 6	28 0	14 11	7 11	4 7	..
Waldex Emulsion Paint—							
All colours other than Evergreen ..	..	46 6	25 0	13 4	7 2	..	..
Evergreen	..	61 3	32 4	17 1	9 0	..	..
Roof Paint—							
Ferric Red	..	52 9	28 2	14 11	8 0	..	..
Light Slate, Terra-tile	..	59 4	31 5	16 7	8 9	..	..
Green, Tangotile, Aluminium ..	..	63 0	33 3	17 6	9 3	..	..
Revelite Full Gloss Enamel—							
All colours other than Coral Rose ..	..	72 6	38 0	19 11	10 5	5 10	3 7
Coral Rose ..	..	100 0	51 9	26 9	13 11	7 7	4 5
Revelite Semi-Gloss Wall Enamel—							
All colours other than Coral Rose ..	..	63 6	33 6	17 7	9 4	5 3	..
Coral Rose ..	..	100 0	51 9	26 9	13 11	7 7	4 5

THE SCHEDULE—*continued.*

Description.	Maximum Retail Prices.						
	4 or 5 Gallon Tins per Gallon.	1 Gallon Tin per Tin.	½ Gallon Tin per Tin.	¼ Gallon Tin per Tin.	⅛ Gallon Tin per Tin.	1/16 Gallon Tin per Tin.	1/32 Gallon Tin per Tin.
	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.	s. d.
Revelite Flat Wall Enamel—							
All colours other than Coral Rose ..	..	61 0	32 3	17 0	9 0	..	..
Coral Rose ..	..	100 0	51 9	26 9	13 11	7 7	4 5
Undercoat—Interior ..	..	52 0	27 9	14 9	7 10	4 7	2 11
Dynamel	..	56 2	29 10	15 10	8 5	4 10	3 0
Solpah Paving Paint—							
All colours other than Ferric Red or Black	..	56 4	29 11	15 10	8 5	4 10	..
Ferric Red or Black ..	..	45 8	24 7	13 2	7 1	4 2	..
Products of the following Manufacturer, viz.:— The W.C. Davis Paint Co. Pty. Ltd.							
"Alsatian"—							
All colours other than Apple Green, Eau de Nil, Sea Green, Purple Brown, Oak Chestnut Brown, Jarrah, Azure Blue, Oxford Blue, Indian Red, Greens, Reds, Orange, Lemon, Yellow	56 6	58 0	30 9	16 4	8 8	4 10	..
Apple Green, Eau de Nil, Sea Green, Purple Brown, Oak, Chestnut Brown, Jarrah, Azure Blue, Oxford Blue, Indian Red	..	59 6	31 6	16 8	8 10	5 0	..
Greens	..	66 6	35 0	18 5	9 7	5 6	..
Reds, Orange, Lemon Yellow	..	70 0	36 9	19 3	10 2	5 8	..
"Alsatian" Undercoats or Primers	52 6	54 0	28 9	15 3	8 1	4 8	..
Products of the following Manufacturer, viz.:— Wesco (Australia) Pty. Ltd.							
Wesco-tone	42 6	44 0	23 9	12 9	..	..	..

Dated this 27th day of September, 1954.

J. F. WALDRON,
Prices Commissioner.

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GEELONG WATERWORKS AND SEWERAGE TRUST.

REGULATIONS RELATING TO THE ISSUE OF INSCRIBED STOCK AND DEBENTURES.

1. *Interpretation.*—In these Regulations, unless there be something in the subject-matter or context inconsistent therewith, the following terms shall have the meanings set against them respectively, that is to say:—

"The Trust."—Geelong Waterworks and Sewerage Trust.

"The Secretary."—The Secretary or Acting Secretary for the time being of the Trust.

"The Registrar."—The officer of the Trust for the time being appointed by the Commissioners to be Registrar of Stock.

"The Office."—The offices of the Trust at Geelong.

"Stock."—Inscribed Stock created or issued by the Trust under the provisions of the Geelong Waterworks and Sewerage Acts.

"Stock Ledger."—The ledger or ledgers of the Trust in which the stock is recorded or inscribed.

"Debentures."—Debentures payable to bearer with interest coupons attached issued by the Trust under the provisions of the Geelong Waterworks and Sewerage Acts.

Words importing the singular shall include the plural and vice versa.

Words importing the masculine shall include the feminine.

"Schedule."—The Schedule to these Regulations.

DEBENTURES.

2. *Application.*—An application to purchase debentures shall be in accordance with Form 1 and on receipt of purchase money the Commissioners of the Trust shall proceed to allot the debentures applied for in accordance with the application; save that where a person tenders for debentures in accordance with a prospectus calling for subscription by public tender the Trust may if it deems fit allot the debentures from such tender without any further or other application and the price paid shall be in manner provided by such prospectus.

3. *Denomination.*—Debentures shall be issued in denominations of £100 and £500.

4. *Delivery.*—The Registrar may, at the request of the owner deliver debentures to any bank nominated by the owner and the receipt of such bank shall constitute proof of effective delivery of such debentures.

Debentures may be sent by registered post to the owner at the address disclosed in the application for such debentures and such posting shall constitute effective delivery of the debentures to the owner.

In all other cases a receipt for debentures issued shall be given to the Registrar by the purchaser.

5. *Conversion to Stock.*—Where any person desires to convert debentures of the Trust held by him into stock he shall transmit to the Registrar an application in Form 2 and shall lodge therewith the debentures held by him, and thereupon the Trust will allot stock in accordance with such application and issue a certificate of inscription.

The debentures lodged with the application together with all coupons in respect of interest not due shall be cancelled and shall be destroyed in the presence of one of the Trust's Auditors who shall certify to the Chairman accordingly. For the purpose of conversion as aforesaid the value of the stock and debentures shall be taken as par and the conversion effected on that basis and on every such conversion the appropriate entries shall be made in the Stock Ledger and duly attested under Clause 12 of these Regulations.

6. *Where Payable.*—Debentures may, at the Trust's discretion, be made payable at the offices of the Commonwealth Bank of Australia at Geelong or at Melbourne.

INSCRIPTION OF STOCK.

7. *Minimum Holding.*—Stock shall not be inscribed, issued, or transferred except in sums of not less than £50 or a multiple thereof.

8. *Applications for Stock.*—An application to purchase stock shall be in accordance with Form 1 and shall be accompanied by the purchase money for the amount of stock applied for, or for the balance of such price where portion thereof has already been paid by way of deposit save that where a person tenders for stock in accordance with a prospectus calling for subscription by public tender the Trust may if it deems fit, inscribe and issue the stock from such tender without any further or other application and the purchase money paid shall be in manner provided by such prospectus. In a joint holding there shall be set forth in the application or tender the names of the applicants in the order in which they are to appear in the Stock Ledger.

9. *Allotment and Certificate of Inscription.*—On receipt of the application or tender and purchase money the Trust shall proceed to allot the stock applied for as set out therein and the Registrar shall cause to be inscribed in the Stock Ledger the name of the purchaser, together with the amount of stock allotted and such other matters and particulars as may be deemed necessary. After such inscription a certificate of inscription in Form 3 shall be transmitted by the Registrar to the allottee or his agent or attorney.

10. *Conversion of Stock into Debentures.*—Upon receipt of an application in accordance with Form 4 for the issue of debentures in exchange for stock the Registrar shall debit the appropriate account in the Stock Ledger and issue debentures of the same currency and rate of interest and for the amount of the stock concerned, after having removed and cancelled the overdue interest coupons.

In the case of a joint holding an application for conversion of stock to debentures shall be signed by all persons inscribed in the Stock Ledger.

Debentures shall not be issued in exchange for stock in cases where the stock is inscribed in the name of an infant or in the name of a person of unsound mind jointly with another or others not under legal disability, except in pursuance of an order of a Justice of the High Court or of a Judge of the Supreme Court of a State or Territory.

11. *Stock Certificate Lost or Destroyed may be Replaced.*—If any certificate of inscription is lost, mislaid, or destroyed the Registrar shall, on being applied to, issue a duplicate certificate in place of the certificate so lost, mislaid, or destroyed, and such duplicate shall be delivered only to the person whose name is inscribed in the Stock Ledger as the holder of the stock or to his attorney or agent. The Registrar may charge a fee not exceeding 1s. for the issue of a duplicate certificate.

12. *Verification of Ledger Entries.*—Every entry in the Stock Ledger shall be initialled by the Secretary to the Trust or such other officer of the Trust as may be appointed, and such ledger shall be audited from time to time by the Trust's Auditors.

13. *Joint Owners, Companies, Corporations, Firms, &c. Stock not to be inscribed in more than four names—inscription in name of corporation, friendly society, or trade union.*—Stock may be inscribed in the name or names of one or more persons, not exceeding four, or in the name of a friendly society or trade union, or in the name of a registered company or other corporation but in the case of a registered company or other corporation the Registrar must first be furnished with a copy of the certificate of incorporation or special Act of Parliament under which such corporation is established, together with a certified specimen of the seal of the corporation, and a duly verified copy of any Regulations governing the mode of affixing the seal.

An application by a friendly society, trade union, or branch thereof for inscribed stock shall be in accordance with Form 1, or in the form accompanying the prospectus of a loan in respect of which the application is made, and shall contain the particulars specified in that form, and shall be accompanied by a certificate in accordance with Form 5 and the Registrar must be furnished with a copy of the rules of the society or union.

Stock will not be inscribed in the name of a firm but in the names of the individual partners.

14. *Minors and Persons of Unsound Mind.*—(*a*) Stock shall not be inscribed in the name of a minor unless jointly with one or more persons who have attained the age of 21 years and when so inscribed it shall not be transferred without the consent in writing of the minor's guardian until the minor has attained the age of fourteen years or is deceased. In cases where stock so inscribed matures the Trust may, if it thinks fit, on the application of the holders, convert such stock into a new loan or the redemption money may, if the minor has attained the age of fourteen years, be paid to him jointly with the other person or persons in whose name the stock is inscribed or, if the minor has not attained the age of fourteen years, be paid to the minor's parent or guardian jointly with the other person or persons in whose name the stock is inscribed.

(*b*) Where stock is inscribed in the name of an infant or persons of unsound mind jointly with any person or persons not under legal disability, a power of attorney for the receipt of the interest on such stock shall be sufficient authority in that behalf if given under the hand and seal of the person or persons not under disability and duly attested. The Registrar before acting on any such power of attorney may require proof to his satisfaction of the alleged infancy or unsoundness of mind by statutory declaration of competent persons or otherwise in such manner as he may reasonably require, but the Registrar shall be under no obligation to require such proof.

15. *Change of Address.*—Any change of address of a stockholder shall be at once notified in writing to the Registrar.

16. *Persons Whose Names are Inscribed Alone Recognized as Owners.*—The Registrar shall, for all purposes, be entitled to regard the person or persons whose name or names are inscribed in the

Stock Ledger as the true and lawful owners and holders of the stock in relation to which such name or names is or are so inscribed, and all receipts, acquittances, discharges, releases, and documents whatsoever executed by such person or persons in relation to such stock, or the interest thereon, shall be deemed for all purposes to be documents duly executed by the person or persons entitled to such stock or interest, as the case may be.

17. *Trusts not Recognized.*—Save as herein or by statute provided no entry of any trust shall be made in the Stock Ledger, and the Trust shall be entitled to treat the person or persons whose name or names are inscribed in the Stock Ledger in respect of any stock as the absolute owner or owners thereof, and accordingly shall not, except as ordered by a court of competent jurisdiction, or as by Statute required, be bound to recognize any equitable or other claim to or interest in such stock on the part of any other person.

18. *Receipts of Joint Holders.*—The receipt of any one stockholder or of his attorney or agent in the case of stock inscribed in more than one name for any interest or for any document relating to such stock shall be sufficient discharge to the Trust in respect thereof, but a discharge in respect of repayment of principal must be given by all stockholders in a joint holding.

19. *Transfers of Stock.*—

(*a*) *Transfer Books, Form* 6.—There shall be kept at the office of the Trust books to be called " Transfer Books " wherein all transfers of stock, or any part thereof, shall be entered and registered, which entries shall be numbered consecutively beginning on the first day of July in each year and corresponding numbers shall be put on the forms from which the entries are made.

All transfers shall be in the form 6 contained in the schedule and shall be fully signed, and in the case of a corporation properly sealed, by the parties making and accepting such transfers, or if any such party or parties be absent by his or their attorney or attorneys thereunto, lawfully authorized as hereinafter mentioned, and every such transfer shall pass the right to all interest becoming due and payable after the date of registration thereof, so that the Trust shall not be under any necessity to apportion any such interest as between the transferror and transferee.

(*b*) As far as practicable the signing of transfers by owners of stock shall take place in the presence of the Registrar or other officer of the Trust duly authorized for that purpose otherwise the Registrar must be satisfied as to the genuiness of signatures before allowing registration to be made. Signatures to transfers shall be attested by one of the witnesses indicated on Form 6.

(*c*) Registration will not be made of any transfer nor application therefor received one calendar month immediately preceding the date of maturity of stock, nor within fourteen days prior to the date on which interest is due.

(*d*) Upon receipt of a form of transfer the Registrar shall inspect the Stock Ledger and shall mark such form of transfer with a notice that the stock transferred is registered in the name of the transferror if such be the case. The form of transfer may be registered at any time within fourteen days of being so marked, notwithstanding that the transferror, after executing the transfer had died or become bankrupt, but if after the expiration of the said period of fourteen days, and before registration, the transferror has died or become bankrupt, no such transfer of stock shall be registered if the Registrar has knowledge of the death or bankruptcy.

(*e*) On a transfer being registered the Registrar shall issue a certificate of inscription to the transferee for the amount of the stock transferred and a fresh certificate to the transferror for the balance of stock (if any) remaining to his credit.

(*f*) Where debentures or stock have been issued for a period which may be determined whether at any time or at one of several dates by the Trust by notice, such notice may

in default of other provisions in the relevant prospectus be given by advertisement appearing at least six months before the intended date of determination in a daily newspaper published in each of the capital cities of the Commonwealth and in Geelong and no transfer of the stock after the publication of such advertisement shall affect the right of the Trust to redeem the stock at the date so notified.

20. *Transfers to be Lodged on a Transfer Day.*—Every transfer of stock shall be lodged at the office on a transfer day. The transfer days shall be Monday, Tuesday, Wednesday, Thursday, and Friday in each week from 9.30 a.m. to 4 p.m. except when any of such days falls on a public holiday..

21. *Repayment of Principal.*—At least six weeks before the maturity of any stock the Secretary shall cause a letter to be sent to all the holders of such stock (in the case of joint accounts to the person whose name is first inscribed) reminding them of such maturity and asking for their instructions regarding disposal of the principal.

Upon receipt of instructions in writing from the stockholder the principal sum may be paid into any bank account nominated, or forwarded by prepaid post to whatever person designated, or collected at the Office by the owner or his attorney.

Sums held in a maturing loan may be converted into any loan being floated by the Trust at the time of such maturity.

The Secretary shall cause all reasonable steps to be taken for obtaining instructions from owners of maturing stock as to disposal of the principal. Where no written instructions have been received from an owner or his attorney the principal sum due shall be retained in the office until payment can properly be made and shall not carry interest.

22. *Stock Ledgers, &c., to be Closed.*—The Registrar will cause the Stock Ledger and transfer books to be closed for a period of fourteen clear days next before either of the days on which interest on the stock is payable, and shall cause the amount of the stock respectively standing to the credit of the several holders thereof to be ascertained, and the balances to be struck in the proper ledger, and the persons who, on the days such books are closed, are inscribed as holders of stock, shall be entitled to receive the interest next payable thereon.

23. *Entries in Stock Ledgers.*—As soon as possible after the registration of any transfer or transmission of stock the amount of stock transferred or transmitted shall be debited to the proper account in the Stock Ledger, and shall be forthwith credited in the same ledger to the account of the person or persons to whom it shall have been so transferred or transmitted, and the Registrar shall cause the said Stock Ledger to be compared with the transfers and transmissions so as to ascertain that all the business transacted has been duly and properly entered and recorded.

In the event of any stock being inscribed incorrectly owing to a mistake in any document the account may be amended by a letter or declaration or in such other manner as the Registrar may reasonably require.

24. *Inspection of Stock Ledger.*—Any stockholder or his attorney, on production of authority in writing, shall be at liberty to inspect his account in the Stock Ledger at all reasonable times on payment of the prescribed fee of 1s.

TRANSMISSION OF STOCK.

25. *Executors or Administrators Alone Recognized.*—(*a*) The executors or administrators of a deceased stockholder (not being one of several joint holders) shall be the only persons recognized by the Trust as having any title to the stock inscribed in the name of such deceased stockholder.

(*b*) (i) On the decease of any one of the joint holders of any stock the death may be proved by production of probate of will, letters of administration or certificate of death, and, if required by the Registrar, a declaration by some disinterested person as to the identity of deceased as the owner. On completion of the proof of death in a joint account the stock and interest thereon shall be registered in the name of the survivor or survivors.

(ii) On the decease of the last survivor in a joint account, the death must be proved as in the case of a sole owner and steps taken to inscribe the stock in the name of the executor or administrator of the last survivor.

26. *Transmission—Evidence of.*—(i) If any stock has been transmitted in consequence of the death or bankruptcy or insolvency of any stockholder or by any lawful means other than by a transfer under these Regulations such transmission shall be made in Form 7 and shall be authenticated by a statutory declaration and in such other manner, if any, as the Registrar may require.

(ii) Every declaration made pursuant to this section shall state the manner in which and the name of the said party to whom such stock shall have been transmitted, and shall be made and signed by some credible person before a Justice of the Peace of the State of Victoria, or a Commissioner of Affidavits or a Commissioner for Declarations and when made and signed outside the said State before a Notary Public or British Consul or Vice-Consul or other British Authority.

(iii) Every application for transmission together with the declaration, probate of the will, letters of administration, adjudication order of sequestration vesting or other order as the case may be or an office copy thereof shall be delivered to the Registrar and such application shall be numbered by him consecutively as though it were a transfer.

27. *Persons Entitled to be Recognized on Transmission.*—(*a*) The person becoming entitled on transmission in manner aforesaid may either himself be registered as the holder of the stock so transmitted or instead of being himself registered may make such transfer of the stock as the stockholder from whom the same was transmitted could have made, and any such transfer shall be accompanied by such evidence in proof of title of the transferror as the Registrar may reasonably require, and the person becoming entitled pursuant to this section shall be entitled to receive the interest on such stock and to all other advantages accruing to the registered holder of the stock.

(*b*) The Registrar may require the signatures of persons who claim transmission of stock by virtue of their appointment as executors, administrators, or trustees to be verified to his satisfaction by a person eligible to witness signatures to a transfer as set out in Form 6.

(*c*) Where the Registrar is satisfied that stock is inscribed in the name of a person whose property is required by law to be placed in the hands of a Public Trustee or any other officer charged with administering the estates of deceased or missing persons or persons under disability the Registrar may, upon receipt of such indemnity from the Public Trustee or other officer as he thinks necessary, transfer the stock accordingly.

28. *Marriage of Female Stockholder.*—On the marriage of a female stockholder such stockholder may apply, in writing in Form 8, to have such stock inscribed in her married name, or jointly with any other name, and shall lodge a copy of the certificate of registration of her marriage under the hand of the proper State officer, together with such other proofs as the Registrar may reasonably require, and the Registrar shall thereupon have such stock inscribed as required, and shall issue a new certificate of inscription in that name or names.

29. *Power of Attorney.*—Any attorney appointed by any stockholder for all or any of the purposes authorized by these Regulations or approved by the Trust, shall be thereunto authorized by writing under hand and seal, attested by two or more credible witnesses, and except with the Registrar's approval to the contrary every such power shall be in Form 9 contained in the Schedule hereto, and shall be executed and attested in accordance with the instructions printed at the foot thereof or endorsed thereon and duly stamped and shall be deposited and kept at the office, and shall be as valid and effectual to all intents and purposes as if the same had been filed in the office of the Registrar-General, and the death, bankruptcy, or (if a female) the marriage of the principal or the revocation of such power had not been registered according to law, and the same shall continue in force until actual written notice of the death of the grantor of such power or of the revocation thereof shall have been received by the Registrar, and the burden of proof of such receipt shall be upon the

person seeking to impeach any act of the Registrar or the Trust purporting to have been done by virtue or reason of such power. In case of a joint account such power of attorney shall be signed by all the owners.

30. *Power of Attorney, &c.—When to be Lodged.*—All powers of attorney, probates, and letters of administration, certificates of adjudication, or orders of sequestration, or declarations, must be lodged at the office at least two clear working days before the same can be acted upon, and neither the Trust nor the Registrar shall be answerable for any claim made or loss arising by reason of any such document not being acted upon during a period of two clear working days after the same has been so lodged.

31. *Interest—When Due.*—Interest is due on the first day of January and the first day of July in every year during the currency of the stock for the half year ended with the day previous to those dates respectively, and is payable on and after those days (holidays excepted).

32. *Payment of Interest.*—(*a*) Interest will be paid in one of the following modes:—

(i) Unless otherwise instructed, payment will be made by cheque sent by ordinary prepaid letter through the post addressed to the stockholder, or the owner whose name in a joint account is first inscribed in the Stock Ledger at his address last notified to the Registrar before the closing of the Stock Ledger.

(ii) By cheque sent through the post to any person nominated in writing by the stockholder or in the case of a joint holding by all or the first-named stockholder. A specimen signature of the person so nominated shall be supplied to the Registrar.

(iii) To the stockholder or his attorney or in the case of a joint holding to the first-named stockholder or his attorney personally at the office by cheque after due notification in writing has been received that such is to be the mode of payment.

(iv) By payment into an account in a bank, including a savings bank, within the Commonwealth at the risk of the stockholder under the following conditions:—

(*a*) Any stockholder who desires to have his interest cheques paid to the credit of a bank account must forward to the Registrar a written, signed authority to that effect. In the case of a joint account all the owners, or the owner whose name is first inscribed may sign the form.

(*b*) Applications for payment into a bank account must be lodged at the office fourteen days before the interest is due, and the instructions therein will remain in force, although the stock may have been added to or partly transferred.

(*c*) Any change of address of the person to whom interest payments are to be sent should be notified at once to the Registrar. When any such notification reaches the Registrar less than fourteen clear days before the next interest is due, the Registrar cannot undertake to record it until after the payment of the next interest.

(*d*) In the absence of any notification to the contrary, it will be assumed that interest payments sent by post have duly reached their destination. When they are not received on the correct day, notice should be sent to the Registrar without delay.

33. *Forms.*—The Registrar shall cause to be printed and kept at the office a sufficient supply of the several forms contained in the Schedule, and no forms other than those printed as aforesaid shall be used except with his approval.

GENERAL.

34. *Specimen Signatures, &c.*—The genuineness of signatures may be tested by comparison with signatures on application or other forms. Specimen signatures obtained by post must be witnessed by a credible person, and their genuineness tested as provided in this Regulation.

If in cases where the owner is not personally known to the Registrar, or where the Registrar is doubtful as to the genuineness of a signature, or if for any other reason he is not satisfied as to the genuineness of an application or signature, a notice of dealing, in the Form 10 contained in the Schedule hereto, should be forwarded to the owner by the Registrar, who shall not register the transaction until a sufficient time has elapsed for a reply to be received.

All possible precautions shall be taken by the Registrar for the security of the stock to its owner and to guard against fraud or improper transactions; erasures must not be made in the Stock Ledger, errors must be ruled through, and the correct entries made.

In all cases of proposed dealings in stock, the attendance of the owner at the Trust's office should, when practicable, be arranged for.

Inscribed Stock Regulations—

FORM No. 1.

No.
Fo.

GEELONG WATERWORKS AND SEWERAGE TRUST.

APPLICATION FOR STOCK OR DEBENTURES.

Loan No. per cent. Maturing

I/We hereby apply for £ (pounds) of Geelong Waterworks and Sewerage Trust

Inscribed Stock / Debentures at par and tender herewith the sum of £ (pounds)

being payment in full / 10 per cent. of the face value of the amount applied for.

I/We agree to pay the balance subject to the conditions contained in the prospectus.

Dated this day of 19

NOTE.—
The amount applied for must be a multiple of £50 for stock and £100 for debentures.

Signature of Applicant(s) .....

Surname. (State whether Mr., Mrs., or Miss.)	Christian Name.	Occupation.	Address in Full.

If Inscribed Stock is Required.—

I/We desire that the interest payments as they fall due be (*a*) paid to the credit of at the Bank; or (*b*) posted to

Signature

NOTE.—If Savings Bank give account number as well as Branch.

Specimen Signature of Applicant

Witness

Inscribed Stock Regulations—

FORM No. 2.

GEELONG WATERWORKS AND SEWERAGE TRUST.

APPLICATION FOR INSCRIPTION OF STOCK IN EXCHANGE FOR DEBENTURES.

I/We hereby apply for exchange into pounds Inscribed Stock of the Geelong Waterworks and Sewerage Trust (Loan No. per cent. due 19) of the attached Debentures for pounds as detailed below and request that such stock be inscribed in the name(s) of—

Surname.	Christian Names.	Address in Full.	Occupation.

PARTICULARS OF DEBENTURES TO BE EXCHANGED.

Loan.	Due Date.	Rate per Cent.	Serial Numbers.	Amount.	Total.

Dated this day of 19

Signature of Applicant(s)

Specimen Signature of Applicants

Witness

Inscribed Stock Regulations—

FORM No. 3.

GEELONG WATERWORKS AND SEWERAGE TRUST.

65-67 Ryrie-street,
Geelong.

INSCRIBED STOCK.

CERTIFICATE OF INSCRIPTION.

Loan No. Interest Rate Maturity Date

This is to certify that under the provisions of the Geelong Waterworks and Sewerage Acts pounds (£) Geelong Waterworks and Sewerage Trust Stock is inscribed in the Stock Ledger and books of record in the Trust's Offices at Geelong in the name(s) of

as the owner(s) of the said stock; such stock being subject to the said Acts and the Regulations which now are or may hereafter be in force thereunder.

This Certificate is valueless for purposes of transfer or negotiation and does not confer any title to the stock. It is issued only as evidence of inscription of the stock at the date hereof.

Dated at Geelong this day of 19

REGISTRAR.

Ledger Folio

Name(s) Certificate of Inscription Issued by

Transfer Book No.
Application No.
Conversion No.

Ledger Folio Date

Inscribed Stock Regulations—

FORM No. 4.

GEELONG WATERWORKS AND SEWERAGE TRUST.

Loan per cent. Maturing 19

APPLICATION FOR DEBENTURES IN EXCHANGE FOR STOCK.

I/We request that £ (pounds) of Geelong Waterworks and Sewerage Trust Inscribed Stock standing in my/our name(s) be exchanged for Geelong Waterworks and Sewerage Trust Debentures of like amount (Denomination: £100 £500).

It is requested that the Debentures be delivered at to whose signature appears in the margin.

* Witness

Signature of Stockholder(s)

Received as shown hereunder

OFFICE USE ONLY.	Geelong Waterworks and Sewerage Trust
Transfer Book	Debentures at £100 Nos.
Ledger	at £500 Nos.
Deb. Reg.	Signature of Recipient
Date Delivered	

* Witness must be a Justice of the Peace, Commissioner for Affidavits, Commissioner for Declarations, Notary Public, Solicitor, member of a recognized Stock Exchange, the Secretary or other authorized officer of the Trust, or a bank manager, who shall sign as such and add the bank stamp.

Inscribed Stock Regulations—

FORM No. 5.

Certificate No.

GEELONG WATERWORKS AND SEWERAGE TRUST.

FRIENDLY SOCIETIES TRADE UNIONS AND BRANCHES THEREOF FORM OF CERTIFICATE.

This is to certify that [1] and and have been duly appointed by the [2] in accordance with the constitution and rules of the said [3] to sign applications on behalf of the [2] for Inscribed Stock and that [1] and and have been duly appointed by the said [2] in accordance with its constitution and rules to sign any documents relating to stock inscribed in the name of the said [2] and that has been duly appointed by the [2] in accordance with its constitution and rules to receive certificates, receipts, and other documents relating to such stock.

A specimen signature of each of the persons appointed as above appears at foot hereof.

Signature of President (or other principal executive officer).

Signature of Secretary

Date

Specimen Signatures of persons appointed as above—

Specimen Signature Specimen Signature

1. Here fill in full names of persons authorized to sign.
2. Here fill in full registered name of the Society or Union.
3. Here fill in the word "Society" or "Union" as the case may be.

NOTE.—Fresh certificate in the above form must be furnished to the Secretary whenever any change is made in any of the persons appointed as above.

Inscribed Stock Regulations—

FORM No. 6.

Stamp Duty to be affixed here.

GEELONG WATERWORKS AND SEWERAGE TRUST.

GEELONG WATERWORKS AND SEWERAGE TRUST INSCRIBED STOCK.

Loan per cent. maturing

TRANSFER AND ACCEPTANCE OF STOCK.

STAMPS ACT.
TRANSFER OF MARKETABLE SECURITIES.

Upon the transfer of any marketable security or right in respect of shares of any corporation company or society which has a register in Victoria, in which such marketable securities or rights are registered—

(a) where such transfer is made on a sale thereof for a consideration in money or money's worth of not less than the unencumbered value of the marketable security or the right in respect of shares comprised in the transfer—
"Where the amount or value of the consideration for the transfer—does not exceed £10—a stamp duty of Six-pence; exceeds £10—a stamp duty of one-quarter per centum of such amount or value."

(b) where such transfer is made without consideration in money or money's worth or upon a consideration in money or money's worth of less than the unencumbered value of the marketable security or the right in respect of shares comprised in the transfer—
The same duty assessed in the same manner and with the same exemptions as on a deed of settlement or gift of property.

No.
£

I/We

(Name, address and occupation must be given in full.)

for the consideration of £ paid to me/us by

hereby assign and transfer unto

(Name, address and occupation must be given in full.)

executors administrators or assigns the sum of £ Geelong Waterworks and Sewerage Trust (Loan per cent.) Stock and all property and interest in and right to the same and to the interest accrued thereon.

Stock Ledger Fol.
Stock Ledger Fol.
Entered
Examined
Stock-holders identified by
Registered at the office of the Geelong Waterworks and Sewerage Trust.
Secretary to the Trust.

Date of sale to Transferee / /

Signed by the said Transferor* this day of 195

in the presence of

* Witness * Transferor

* Witness * Transferor

* Witness * Transferor

* Witness * Transferor

I/We hereby accept the stock above mentioned transferred into my or our name.

Signed by the said Transferee* this day of 195

in the presence of

* Witness * Transferee

* Witness * Transferee

* Witness * Transferee

* Witness * Transferee

In the case of a Corporation the Transfer or Acceptance shall be properly sealed.

The witness *must* be a Justice of the Peace, Commissioner for Affidavits, Commissioner for Declarations, Notary Public, Solicitor, member of a recognized Stock Exchange, the secretary or other authorized officer of the Trust, or a bank manager, who shall sign as such and add the bank stamp.

Inscribed Stock Regulations—

FORM No. 7.

GEELONG WATERWORKS AND SEWERAGE TRUST.

MANDATE FOR TRANSMISSION.

Transmission No.

To the Secretary,
Geelong Waterworks and Sewerage Trust.

Please have the undermentioned Transfer by Transmission effected.

From who
lately resided at but
who is now (1)

Pounds
of the Geelong Waterworks and Sewerage Trust (Loan per centum) stock to the undermentioned person(s)

Surname.	Christian Names.	Occupation.	Address.

who is/are (2) and claim by transmission.

I/We submit the following documents in support of this claim in accordance with Section 26 of the Regulations.

Documents:

Witness Signature
Qualification Date
Witness Signature
Qualification Date

(1) Dead, bankrupt, or insolvent.

(2) Executor, administrator, guardian, trustee as the case may be.

NOTES.—The witness should be a Judge of the Supreme Court or County Court or Police Magistrate, Solicitor, Justice of the Peace of the State, Commissioner for Affidavits or Declarations, member of a recognized Stock Exchange, the Secretary or other authorized officer of the Trust, or a bank manager who shall sign as such and add the bank stamp, and when made and signed outside the State a Notary Public or British Consul or Vice-Consul.

Inscribed Stock Regulations—

FORM No. 8.

GEELONG WATERWORKS AND SEWERAGE TRUST.

MARRIAGE OF FEMALE STOCK-HOLDER.

INSCRIBED STOCK.

Ledger Fol.
Sig. Book Fo.

APPLICATION OF STOCK-HOLDER FOR ALTERATION OF NAME ON MARRIAGE.

To the Secretary,
Geelong Waterworks and Sewerage Trust.

In consequence of my marriage I desire that stock described in schedule below and inscribed in the name of
(my former name) be inscribed in my present name of
My husband's full name is
I attach my Marriage Certificate for noting.
My address in future will be

Amount of Stock.	Loan.	Rate per Cent.	Due Date.

Witness New Signature
Occupation (Married Name)
Address Signature
(Former Name)
Date

Inscribed Stock Regulations—

FORM No. 9.

GEELONG WATERWORKS AND SEWERAGE TRUST.

POWER OF ATTORNEY.

Know All Men by These Presents that I
have constituted and appointed by these presents do constitute and appoint
true and lawful attorney for and in name and on behalf—

1. To purchase and accept transfer of any part of the Geelong Waterworks and Sewerage Trust Stock.
2. To receive and give receipts for all interest now due or that may hereafter may become due on all Geelong Waterworks and Sewerage Trust Stock now or at any time hereafter during the currency of this Power of Attorney standing in name; and
3. To sell and transfer all or any part of the Geelong Waterworks and Sewerage Trust Stock now or at any time herafter during the currency of this Power of Attorney standing in name and the interest thereon; and
4. To do and perform all acts and things and to sign all documents and receipts necessary and proper to be done, performed, or signed by virtue hereof.

In Witness Whereof have hereunto set hand and seal this
day of One thousand nine hundred and

Signed Sealed and Delivered SEAL
by the said in the presence of—

Witness
Occupation
Address
Witness
Occupation
Address

NOTES.—Special attention is requested to the instructions for executing powers of Attorney which are printed on the other side.

Insert "my" or "our", "me" or "us" as the case may be.

INSTRUCTIONS FOR EXECUTING POWERS OF ATTORNEY.

1. The date must be inserted in words and not in figures.
2. Each execution must be under seal and be attested by two or more credible witnesses, who must state their full addresses and occupation.
3. If clerks or servants are witnesses, they should give the name and address of their employers.
4. When a power of attorney is executed out of the State of Victoria, in addition to two witnesses, the signature must be attested by a British Minister, Consul, Vice-Consul, or other British Authority, or by a notary public.
5. If it should be necessary for a stockholder to execute a power of attorney by a mark instead of by signing his name, each witness must be a person of known position, such as a Minister of Religion, Magistrate, Justice of the Peace, Solicitor, or Medical Practitioner; and the witness must declare in writing that the document has been read over and fully explained to and understood by the stockholder.
6. Any alteration, interlineation, or erasure made in a power of attorney must be particularly mentioned in the attestation subscribed by the witnesses, and it must be stated to have been done previous to execution.
7. The Power of Attorney must be duly stamped as prescribed by the Stamps Acts.

Inscribed Stock Regulations—

FORM No. 10.

GEELONG WATERWORKS AND SEWERAGE TRUST.

INSCRIBED STOCK.

NOTICE OF DEALING.

To

I have to inform you that application has been made to deal with stock maturing and standing in your name in the manner stated hereunder, and if no objection in writing is received from you within days, the transaction referred to will be allowed by me.

REGISTRAR.

Proposed Transaction—

The foregoing Regulations, with Schedules thereto, were made and passed by the Geelong Waterworks and Sewerage Trust, and the common seal of the said Trust was hereunto affixed the 27th day of August, 1954, in the presence of—

J. W. CARR, Chairman.

SEAL N. M. FREEMAN, Commissioner.

B. C. HENSHAW, Secretary.

Approved by the Governor in Council, the 21st day of September, 1954.

A. MAHLSTEDT,
Clerk of the Executive Council.

By Authority: W. M. HOUSTON, Government Printer, Melbourne.